

# LOCAL PLANNING POLICY: HERITAGE AREA GUIDELINES



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|--|--|
| <b>Legislation / local law requirements</b>                      | <i>Heritage Act 2018</i><br><i>Planning and Development Act 2005</i><br>This policy has been prepared under the provisions of Schedule 2, Part 2 and 3 of the <i>Planning and Development (Local Planning Schemes) Regulations 2015</i>  |
| <b>Relevant delegations</b>                                      | 16.1.1 Determination of various applications for development approval under the City's Local Planning Scheme   |
| <b>Related policies, procedures and supporting documentation</b> | State Planning Policy 7.3 Residential Design Codes<br>City of Vincent Local Planning Scheme No. 2<br>Local Planning Policy 7.1.1 Built Form<br>Local Planning Policy 7.6.1 Heritage Management - Development Guidelines for Heritage and Adjacent Properties<br>Local Planning Policy 7.6.2 Heritage Management - Assessment<br>Local Planning Policy: Character Area Guidelines |

## PART 1 - PRELIMINARY

### PURPOSE

The purpose of Local Planning Policy: Heritage Area Guidelines (Policy) is to:

- establish a framework to identify areas within the City of Vincent (City) that contain characteristics that are valued by the community; and
- provide guidance on the assessment of proposals for works in identified heritage areas.

### OBJECTIVES

The objectives of this Policy are to:

1. identify areas within the City that contain characteristics valued by the community to be known as heritage areas;
2. establish a planning and design framework to protect the streetscape located within heritage areas;
3. ensure that new buildings and additions to existing buildings will be sympathetic to the unique character of the streetscape; and
4. distinguish between buildings within each heritage area that contribute to the integrity of the streetscape, and those which do not.

### SCOPE

This Policy applies to:

1. the process used to identify and formally recognise heritage areas.

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2. the identification and listing of development standards which will be used in assessing all development applications for properties within those heritage areas.
3. applications for development approval in the following areas:
  - Harley Street Heritage Area;
  - Janet Street Heritage Area; and
  - Brookman and Moir Streets Heritage Area

## RELATIONSHIP TO OTHER DOCUMENTS

This Policy forms part of the City's local planning policy framework. Where this Policy is inconsistent with the City's operative Local Planning Scheme (Scheme), the Scheme prevails. Where this Policy is inconsistent with an adopted local development plan, activity centre plan or structure plan, the adopted local development plan, activity centre plan or structure plan prevails.

Where this Policy is permitted to amend or replace the Deemed to Comply provisions under Clause 3.2.3(a) of the State Planning Policy 7.3 Residential Design Codes Volume 1 (R-Codes), the provisions of this Policy prevail. Where this Policy augments the Design Principles under Clause 3.2.3(c) of the R-Codes by providing Local Housing Objectives, both the Design Principles of the R-Codes and the Local Housing Objectives of this Policy apply.

Where this Policy is inconsistent with the provisions of another local planning policy, the provisions of this Policy prevail.

## PART 2 - POLICY PROVISIONS

### DEFINITIONS

All terms used in this Policy are as defined in the *Planning and Development Act 2005*, *Planning and Development (Local Planning Schemes) Regulations 2015* (Regulations), R-Codes and Scheme, unless stated otherwise below.

**character** means the defining features of a place, including form and scale, materiality, style or repetition.

**character area** means a group of properties that are recognised to collectively form a streetscape with defining features and characteristics, which may include form and scale, materiality, style or repetition.

**complementary** or **sympathetic** means a design outcome that respects its context. It would not be identical to historic neighbours, nor would it intrude on their presence in the streetscape. It would be of a similar or lesser scale.

**heritage area** means a group of properties which are recognised to collectively form a streetscape, townscape or cultural environment with significant heritage characteristics, which may include architectural style, town planning or urban design excellence, landscape qualities, or strong historic associations.

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**heritage assessment** is a systematic assessment that describes a place and its setting and states the cultural heritage significance of the place based on the criteria outlined in the Burra Charter, and in accordance with the HCWA Guidelines for the Assessment of Local Heritage Places 2022, Guidelines for Local Heritage Surveys 2022 and Guidelines for Establishing a Heritage List 2023.

**heritage impact statement** means a document that evaluates the likely impact of proposed development on the significance of a heritage place and its setting, and any conservation areas within which it is situated. The heritage impact statement should outline measures proposed to minimise any identified impact and any conservation benefits associated with the proposal. It should be prepared in accordance with the HCWA Heritage Impact Statement: A Guide.

**Heritage List** means the City of Vincent's Heritage List. It is a list of places within the City of Vincent, considered by the City to be of identified cultural heritage significance and worthy of built heritage conservation pursuant to the Regulations.

**public domain view** means the sightline from the front property line on the opposite side of the road with a viewing height of 1.65 metres above the level of the pavement.

**statement of significance** means a concise and distilled summary of the cultural significance of the place. It is a statement summarising each aspect of significance, following an assessment of significance. It underpins decisions about statutory protection and conservation.

**streetscape** means the collective elements that contribute to a street, including architectural styles, front yards, car parking structures and access, infrastructure, footpaths, signage, street trees and landscaping and fencing and front boundary treatments.

## POLICY

### 1. Development Standards

- 1.1 Applications for development that comply with the relevant Deemed to Comply provisions and Local Housing Objectives are considered to meet the objectives of this Policy.
- 1.2 Applications for development that seek departure from the Deemed to Comply provisions may be deemed to be acceptable where:
  - 1.2.1 the applicant applies for assessment, and provides adequate justification, against the relevant Design Principles of the R-Codes, Design Objectives, Local Housing Objectives, objectives of this Policy and the Scheme; and
  - 1.2.2 the application obtains the support of the City's Design Review Panel where the City determines it to be necessary.
- 1.3 The City will assess and determine at its discretion that the same or better outcome will be achieved than would have been provided by the Deemed to Comply criteria.
- 1.4 Applications for development shall include a contextual elevation showing the elevation of the proposed development and the existing development on the adjoining properties either side pursuant to Schedule 2, Part 8, Clause 63(1)(d) of the Regulations.

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## 2. Designating Heritage Areas

- 2.1 The City may consider designating a heritage area in accordance with Part 3 of Schedule 2 of the Regulations in the following instances:
- 2.1.1 Where a nomination has been received for a character area in accordance with Local Planning Policy: Character Area Guidelines and the City is satisfied the area is suitably qualified to be designated as a heritage area following a heritage assessment.
  - 2.1.2 Where the City has undertaken a heritage assessment of any area and is satisfied it is suitably qualified to be designated as a heritage area following a heritage assessment.
  - 2.1.3 If an area qualifies as a heritage area, the City will consult with affected landowners on the designation prior to making a recommendation to Council on whether to proceed in accordance with Part 3 of Schedule 2 of the Regulations.

| OFFICE USE ONLY          |  |
|--------------------------|--|
| Responsible Officer      | Executive Manager Urban Design and Strategic Projects                  |
| Initial Council Adoption | 22/09/2015   |
| Previous Title           | Local Planning Policy: Character Areas and Heritage Areas              |
| Reviewed / Amended       | 12/12/2017; 01/05/2018; 23/03/2021; 14/12/2021; 26/07/2022; 25/07/2023 |
| Next Review Date         | 07/2027  |

## APPENDIX 1: HARLEY STREET HERITAGE AREA GUIDELINES

These guidelines are to be read in conjunction with the City’s Local Planning Policy: Heritage Area Guidelines.



Figure 1: Harley Street Heritage Area

### 1. PURPOSE

These guidelines have been adopted for the purpose of preserving and enhancing the established neighbourhood character and amenity of Harley Street, while allowing for new development that meets the changing needs of the community.

The guidelines:

1. Provide a comprehensive basis for the control of all development within Harley Street including new development and additions/alterations to existing development;
2. Replace the deemed to comply requirements and/or provide additional deemed to comply requirements in relation to certain elements of the R-Codes.

## 2. STATEMENT OF HERITAGE SIGNIFICANCE

Harley Street has moderate aesthetic value through its dominant display of single storey residences constructed in the Federation style of architecture. Harley Street has moderate historic value as an almost complete example of a 19th century streetscape, with the residences that were predominantly built in the late 1890s - 1910s. It provides a good and rare historic record of the accommodation of people in this period.

Harley Street demonstrates a unified and cohesive physical form in the public realm. The overall form, style, height, setback and selection of materials of the historic dwellings contribute to the uniformity of the streetscapes, providing a coherent character with varied levels of intactness.

Harley Street contains a good representation of the wide variety of building design applied to the inner city residential areas during the Gold Rush period of 1891 to 1919. This includes substantial single residential dwellings with individual architectural detail, modest single residential dwellings, semi-detached and terraced dwellings. All these styles are represented in Harley Street, and although alterations have been undertaken to some of the dwellings, and some new replacement development has occurred in the 1980s, the streetscape is largely consistent with the styles of the Gold Boom period and construction applied during this time.

Overall, Harley Street is a highly intact example of late nineteenth to early twentieth century residential development comprising fine examples of modest single storey dwellings constructed in the Federation period of architecture.

## 3. RECORD OF PLACES OF HERITAGE SIGNIFICANCE

| PROPERTY ADDRESS      | CONSTRUCTED (CIRCA) | ARCHITECTURAL STYLE <sup>1</sup>     | LEVEL OF SIGNIFICANCE |
|-----------------------|---------------------|--------------------------------------|-----------------------|
| 1A - 1C Harley        | c.1988              | Post-war Conventional Suburban Style | No contribution       |
| 2 Harley              | c.2017              | Contemporary                         | No contribution       |
| 3 Harley              | c.1898              | Federation Bungalow                  | Moderate contribution |
| 4 Harley              | c.1914              | Federation Bungalow                  | Moderate contribution |
| 6 Harley              | c.1900              | Federation Queen Anne                | Moderate contribution |
| 7, 9, 11, & 13 Harley | c.1898              | Federation Italianate                | Moderate contribution |
| 10 Harley             | c.1905              | Federation Queen Anne                | Moderate contribution |
| 14 Harley             | c.2017              | Contemporary                         | No contribution       |
| 15A & 15B Harley      | c.1982              | Reproduction of a Federation Terrace | No contribution       |
| 16 Harley             | c.1908              | Federation Queen Anne                | Moderate contribution |
| 17 Harley             | c.1900              | Federation Bungalow                  | Moderate contribution |
| 19 Harley             | c.1900              | Federation Bungalow                  | Moderate contribution |
| 20 Harley             | c.1898              | Federation Bungalow                  | Moderate contribution |

|           |        |   |                       |
|-----------|--------|---|-----------------------|
| 21 Harley | c.1898 | Federation Queen Anne   | Moderate contribution |
| 22 Harley | c.1899 | Federation Bungalow   | Moderate contribution |
| 23 Harley | c.1910 | Federation Queen Anne   | Moderate contribution |
| 24 Harley | c.1898 | Federation Bungalow   | Moderate contribution |
| 25 Harley | c.1903 | Federation Queen Anne   | Moderate contribution |
| 27 Harley | c.1905 | Federation Bungalow   | Moderate contribution |
| 28 Harley | c.1898 | Federation Queen Anne   | Moderate contribution |
| 30 Harley | c.1988 | Post-war Conventional Suburban Style                            | No contribution       |
| 31 Harley | c.1908 | Federation Bungalow   | Moderate contribution |
| 32 Harley | c.1900 | Federation Bungalow   | Moderate contribution |
| 33 Harley | c.1920 | Federation Bungalow with Inter War California Bungalow elements | Moderate contribution |
| 36 Harley | c.1905 | Federation Bungalow   | Moderate contribution |
| 38 Harley | c.1905 | Federation Bungalow   | Moderate contribution |
| 39 Harley | c.1898 | Federation Bungalow   | Moderate contribution |

1 The architectural style applied to dwellings below are based on those outlined in Identifying Australian Architecture by Appery, Irving and Reynolds and the City of Vincent Housing Style Guide.

#### 4. HARLEY STREET LOCAL HOUSING OBJECTIVES

- Retention and conservation of the original Federation style dwellings;
- Protection and enhancement of existing streetscape character;
- Retention of the visual character of late nineteenth to early twentieth century residential development;
- Encourage a high standard of architectural and sustainable design solutions for additions to existing development and new development;
- Additions to existing development and new development is appropriately set back so as to maintain the visual prominence of the single storey streetscape when viewed from Harley Street;
- New development is to respond appropriately to the surrounding street context through:
  - consistent scale and bulk in relation to the original street pattern, maintaining front and side setback patterns;
  - built form that incorporates consistent physical built form elements without the requirement to mimic the style of the original character dwellings;
- The reinstatement and reconstruction of missing design elements to original building facades such as a timber verandahs, decorative timber gables and the removal of render from original brickwork; and site features such as landscaping and front fences;
- Maintain the absence of and promote the removal of visually intrusive car storage structures and spaces and vehicular crossovers to the primary streetscape; and
- Maintain an open atmosphere of street frontage by ensuring front boundary fences are low and/or visually permeable.

**5. DEVELOPMENT GUIDELINES FOR ALL DEVELOPMENT**

| <b>Local Housing Objectives</b>  |                     | <b>Deemed to Comply</b>   |                          |                       |   |
|--|---------------------|---|--------------------------|-----------------------|---|
| Pursuant to clause 7.3.1 (b) of the R-Codes, the following Local Housing Objectives augment or add to the listed sections of the R-Codes. For all development that is not subject to the R- Codes, the following apply as Design Principles. |                     | Pursuant to clause 7.3.1 (a) of the R-Codes, the following provisions replace the Deemed to Comply requirements in the listed sections of the R-Codes. For all development that is not subject to the R-Codes, the following apply as Deemed-to- Comply provisions. |                          |                       |   |
| <b>Street Setbacks</b>   |                     |   |                          |                       |   |
| Augment<br>5.1.2 P2.1  | Augment<br>6.1.3 P3 | Dwellings should not detract from the streetscape by being set back too close or too far from the front boundary when compared with adjacent properties or the predominant streetscape character.   | Replace<br>5.1.2<br>C2.1 | Replace<br>6.1.3 C3.1 | Ground floor street setbacks to be an average of the two directly adjoining properties on either side of the proposed development.  |
| New 5.1.2<br>P2.3  | New 6.1.3<br>P3.1   | First floor development is to be adequately setback to maintain the predominant single storey appearance of the streetscape.  | Replace<br>5.1.2<br>C2.1 | Replace<br>6.1.3 C3.1 | <p>Ground floor street setbacks to be an average of the two directly adjoining properties on either side of the proposed development.</p> <p>First floor development shall not be located within 10 meters of the primary street boundary; or</p> <p>First floor development shall not be visible from Harley Street when measured in accordance with 'Figure 2 – Line of Sight'.</p> |



| Lot Boundary Setbacks    |                          |  |                      |                      |   |
|--------------------------|--------------------------|--|----------------------|----------------------|---|
| Augment<br>5.1.3<br>P3.1 | Augment<br>6.1.4<br>P4.1 | Reduce impacts of building bulk on adjoining properties by maintaining a setback similar to any adjoining properties or the predominant development context while also complying with relevant overshadowing and visual privacy requirements.  |                      |                      |   |
| Building Height          |                          |  |                      |                      |   |
| Augment<br>5.1.6 P6      | Augment<br>6.1.2 P2      | <p>Building height that creates no adverse impact on the amenity of adjoining properties or the streetscape by maintaining similar features of adjoining properties or the streetscape in terms of scale and proportions.</p> <p>The plate height of new dwellings, particularly the portions in view from the street, should approximate the street's typical wall plate height to avoid an under scaled squat appearance.</p> <p><i>Note: The plate heights on Harley Street are typically around 40 courses, usually starting above limestone footings equal to the internal floor level.</i></p> | Replace<br>5.1.6 C6  | Replace<br>6.1.2 C2  | <p>Minimum building height fronting primary street: 3m</p> <p>Maximum building height:</p> <p>Maximum height of a dwelling is 2 storeys and comply with the following measurements</p> <p>Top of external wall (roof above) : 7m</p> <p>Top of external wall (concealed roof): 8m</p> <p>Top of pitched roof: 10m</p> |
|                          |                          |  | New<br>5.1.6<br>C6.1 | New<br>6.1.2<br>C2.1 | <p>Loft additions will be contained wholly within the roof space and where no visible change is made to the dwelling's appearance from the street.</p> <p>The addition of front facing dormer windows and gables to lofts will cause lofts to be assessed as a storey for the purpose of this policy.</p>             |

| <b>Vehicular Access</b>                |                          |   |  |                          |   |
|--|--------------------------|---|--|--------------------------|---|
| Augment<br>5.3.5<br>P5.1               | Augment<br>6.3.5 P5      | No crossovers to Harley Street unless the property has no other vehicular access opportunities.   |  |                          |   |
| <b>Setback of Garages and Carports</b> |                          |   |  |                          |   |
| Augment<br>5.2.1                       | Augment<br>6.3.4         | Maintain the absence of visually intrusive car storage facilities within the streetscape.   | Replace<br>5.2.1<br>C1.1,<br>C1.2<br>and<br>C1.5 |                          | Carports, garages or hardstand car bays are to be located at the rear of the property where ROW access is available.  |
| <b>Street Surveillance</b>             |                          |   |  |                          |   |
| Augment<br>5.2.3 P3                    | Augment<br>6.2.1<br>P1.1 | Buildings to be designed to encourage active use and actual surveillance of the front yard by including verandahs, porches or Outdoor Living Areas and by including habitable rooms at the front of the dwelling. | Replace<br>5.2.3<br>C3.1                         | Replace<br>6.2.1<br>C1.1 | Ensure a front verandah is incorporated into the façade of new dwellings (at a minimum of 50% of the width of the front facade) with a useable minimum depth of 1.8 metres. |
|  |                          |   | Replace<br>5.2.3<br>C3.2                         | Replace<br>6.2.1<br>C1.2 | Incorporate habitable rooms to the front of the development with generous openings to provide street engagement and passive surveillance to the street.                     |

| Street Walls and Fences |                     |  |                     |                     |   |
|-------------------------|---------------------|--|---------------------|---------------------|---|
| Augment<br>5.2.3 P3     | Augment<br>6.2.2 P2 | Ensure front boundary fences enhance the streetscape by being designed in the predominant style of the street and are low enough to retain views to dwellings and front gardens. | Replace<br>5.2.4 C4 | Replace<br>6.2.2 C2 | <ul style="list-style-type: none"> <li>a. The maximum height of new fences facing the street is to be 1.2 metres above the adjacent footpath level;</li> <li>b. The maximum height of any solid portion of a new fence is to be 500mm above the adjacent footpath level. The remaining portion is to be a minimum of 40% visually permeable;</li> <li>c. The maximum height of fence piers with decorative capping to be 1.8 metres above the adjacent footpath level; and</li> <li>d. Maximum width of piers to be 470mm;</li> </ul> |

|  |  |  |  |  |
|--|--|--|--|--|
|  |  |  |  | <p><b>OR</b></p> <ul style="list-style-type: none"> <li>e. The maximum height of new fences facing the street is to be 1.8 metres above the adjacent footpath level;</li> <li>f. The maximum height of any solid portion of a new fence is to be 750mm above the adjacent footpath level. The remaining portion is to be a minimum of 80% visually permeable; and</li> <li>g. The maximum height of fence piers with decorative capping to be 2.0 metres above the adjacent footpath level;</li> </ul> <p><b>OR</b></p> <ul style="list-style-type: none"> <li>h. Fencing types as illustrated in the diagrams below are permitted. Variations to the examples indicated below may be supported where it can be demonstrated that they are compatible with the existing fence styles in the street.</li> </ul> |
|--|--|--|--|--|

| General Building Design  |                |  |  |                      |   |
|--|----------------|--|--|----------------------|---|
| New 5.4.6 P6.1   | New 6.2.4 P4.3 | The roof of a building is to be designed so that it is similar to the existing streetscape character.  | New 5.4.6 C6.1   | Pursuant to 6.2.4 C4 | Roof pitch of ground and upper storeys of between 30 and 40 degrees, with 35 to 45 degree pitches to gables where it can be viewed from the street.   |
| New 5.4.6 P6.2   | New 6.2.4 P4.4 | New buildings must respond to the prevailing surrounding character and ensure unsympathetic contrasts in scale, mass and materials are avoided.                                | New 5.4.6 C6.2   | Pursuant to 6.2.4 C4 | New dwellings' floor levels match, or mediate between, properties on both sides of the proposed dwelling.<br><i>Floor levels of the original dwellings in Harley Street are raised approximately 300 - 500mm above footpath level, which assists to define the verandah area and provide additional privacy as views into front rooms are obscured.</i> |
| New 5.4.6 P6.3   | New 6.2.4 P4.5 | Each street facing window should have a style that suits the predominant character of the street and the architectural style of the proposed dwelling.                         | New 5.4.6 C6.3   | Pursuant to 6.2.4 C4 | Street facing windows have a height of at least 1.6 times their width.  |
| New 5.4.6 P6.4   | New 6.2.4 P4.6 | Additions visible from the street should be compatible with and respectful of the existing dwelling and the predominant character of the existing dwellings within the street. | New 5.4.6 C6.4   | Pursuant to 6.2.4 C4 | Additions to existing buildings will only be supported where they are located behind the original facade and set back so that the existing front building and roof form remains the dominant feature to the street.   |
| Demolition   |                |  |  |                      |   |
| Applications for development approval should maintain the front façade of a contributing dwelling. |                |  | Applications for development approval that retain the original dwelling are considered acceptable. |                      |   |

## 6. RECOMMENDED FENCING STYLES FOR HARLEY STREET

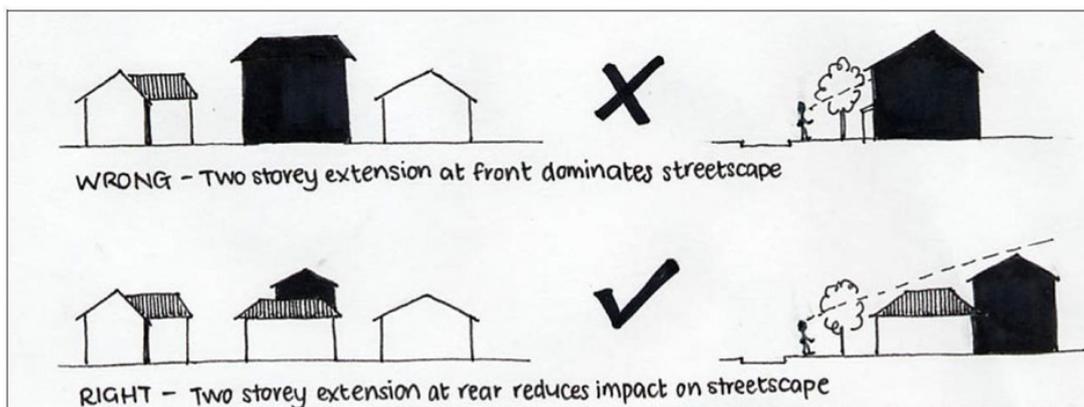


Figure 2 - Line of Sight

## APPENDIX 2: JANET STREET HERITAGE AREA GUIDELINES

These guidelines are to be read in conjunction with the City's Local Planning Policy: Heritage Area Guidelines.

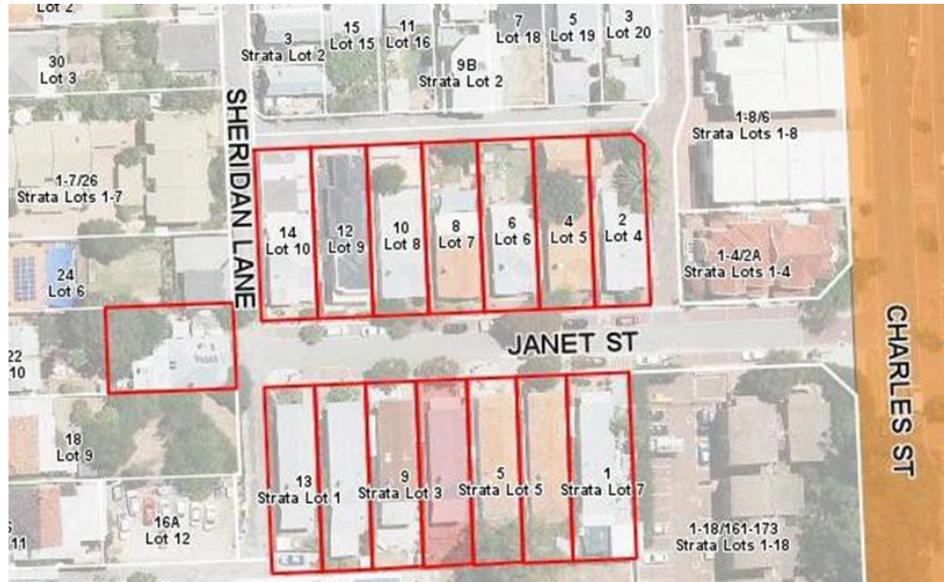


Figure 1: Janet Street Heritage Area

### 1. PURPOSE

These guidelines have been adopted for the purpose of preserving and enhancing the established neighbourhood character and amenity of Janet Street, while allowing for new development that meets the changing needs of the community.

The guidelines:

1. Provide a comprehensive basis for the control of all development within Janet Street Heritage Area including new development and additions/alterations to existing development;
2. Replace the deemed to comply requirements and/or provide additional deemed to comply requirements in relation to certain elements of the R Codes.

### 2. STATEMENT OF HERITAGE SIGNIFICANCE

Janet Street has moderate aesthetic value through its dominant display of single storey residences constructed in the Federation and Interwar style of architecture.

Janet Street has moderate historic value as an almost intact and cohesive example of a 20th century streetscape, with the residences that were predominantly built in the 1920's. It provides a good and rare historic record of the accommodation of people in this period.

Janet Street demonstrates a cohesive physical form in the public realm. The overall form, style, height, setback and selection of materials of the historic dwellings contribute to the uniformity of the streetscapes, providing a coherent character with varied levels of intactness

### 3. RECORD OF PLACES OF HERITAGE SIGNIFICANCE

| PROPERTY ADDRESS | CONSTRUCTED (CIRCA) | ARCHITECTURAL STYLE <sup>1</sup> | LEVEL OF SIGNIFICANCE |
|------------------|---------------------|----------------------------------|-----------------------|
| 1 Janet Street   | c.1922              | Interwar Bungalow                | Moderate contribution |
| 2 Janet Street   | c.1924              | Federation Bungalow              | Moderate contribution |
| 3 Janet Street   | c.1922              | Interwar Bungalow                | Moderate contribution |
| 4 Janet Street   | c.1923              | Federation Bungalow              | Moderate contribution |
| 5 Janet Street   | c.1922              | Interwar Bungalow                | Moderate contribution |
| 6 Janet Street   | c.1923              | Federation Bungalow              | Moderate contribution |
| 7 Janet Street   | c.1922              | Interwar Bungalow                | Moderate contribution |
| 8 Janet Street   | c.1923              | Interwar Bungalow                | Moderate contribution |
| 9 Janet Street   | c.1921              | Interwar Bungalow                | Moderate contribution |
| 10 Janet Street  | c.1923              | Federation Bungalow              | Moderate contribution |
| 11 Janet Street  | c.1921              | Interwar Bungalow                | Moderate contribution |
| 12 Janet Street  | c.2009              | Contemporary dwelling            | No contribution       |
| 13 Janet Street  | c.1921              | Interwar Bungalow                | Moderate contribution |
| 14 Janet Street  | c.1923              | Federation Bungalow              | Moderate contribution |
| 15 Janet Street  | c.1923              | Interwar Bungalow                | Moderate contribution |

### 4. JANET STREET LOCAL HOUSING OBJECTIVES

- Retention and conservation of the original Federation style dwellings;
- Protection and enhancement of the existing streetscape character;
- Retention of the visual character of late nineteenth to early twentieth century residential development;
- Encouragement of a high standard of architectural and sustainable design solutions for additions to existing buildings and the development of new buildings;
- Ensuring additions to existing buildings and new dwellings are appropriately setback so as to maintain the visual prominence of the single storey streetscape when viewed from Janet Street;
- New development is appropriate in scale and bulk in relation to the original street pattern, maintaining front and side setback patterns;
- The reinstatement and reconstruction of missing design elements to original building facades such as a timber verandah, decorative timber gables and the removal of render from original brickwork; and site features such as landscaping and front fences;
- Maintain the absence of and promote the removal of visually intrusive car storage structures and spaces, and vehicular crossovers to the primary streetscape; and
- Maintain an open atmosphere of street frontage by ensuring front boundary fences are low and/or visually permeable.

## 5. DEVELOPMENT GUIDELINES FOR ALL BUILDINGS

| Local Housing Objectives   |                       |   | Deemed to Comply  |                       |  |
|--|-----------------------|---|---|-----------------------|--|
| Pursuant to clause 7.3.1 (b) of the R-Codes, the following Local Housing Objectives augment or add to the listed sections of the R-Codes. For all development that is not subject to the R- Codes, the following apply as Design Principles. |                       |   | Pursuant to clause 7.3.1 (a) of the R-Codes, the following provisions replace the Deemed to Comply requirements in the listed sections of the R-Codes. For all development that is not subject to the R-Codes, the following apply as Deemed-to- Comply provisions. |                       |  |
| Street Setbacks  |                       |   |   |                       |  |
| Augment<br>5.1.2 P2.1  | Augment<br>6.1.3 P3   | Dwellings should not detract from the streetscape by being set back too close or too far from the front boundary when compared with adjacent properties or the predominant streetscape character.   | Replace<br>5.1.2 C2.1   | Replace<br>6.1.3 C3.1 | Ground floor street setbacks to be an average of the two directly adjoining properties on either side of the proposed development.<br><br>First floor development shall not be located within 4 meters of the primary street boundary; or<br><br>First floor development shall not be visible from Janet Street when measured in accordance with 'Figure 2 – Line of Sight'. |
| New 5.1.2 P2.3   | New 6.1.3 P3.1        | First floor development is to be adequately setback to maintain the predominant single storey appearance of the streetscape.  |   |                       |  |
| Lot Boundary Setbacks  |                       |   |   |                       |  |
| Augment<br>5.1.3 P3.1  | Augment<br>6.1.4 P4.1 | Reduce impacts of building bulk on adjoining properties by maintaining a setback similar to any adjoining properties or the predominant development context while also complying with relevant overshadowing and visual privacy requirements. |   |                       |  |



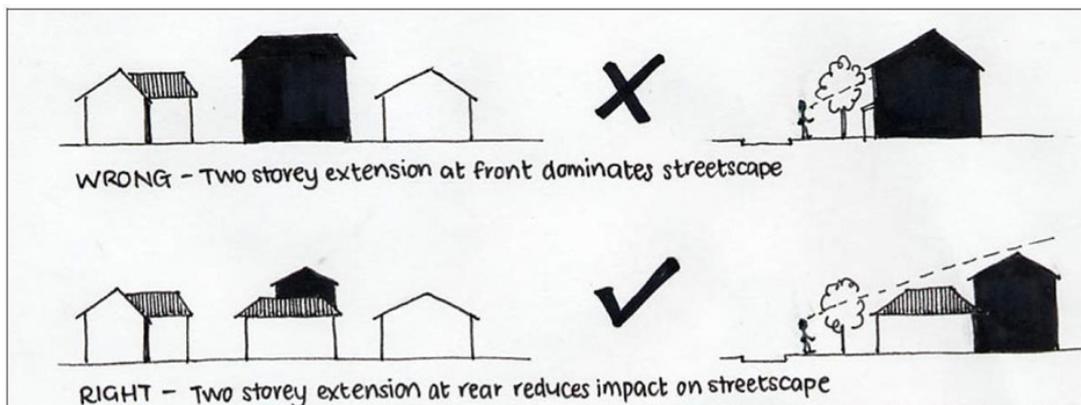
| Building Height     |                     |   |   |   |  |
|---------------------|---------------------|---|---|---|--|
| Augment<br>5.1.6 P6 | Augment<br>6.1.2 P2 | <p>Building height that creates no adverse impact on the amenity of adjoining properties or the streetscape by maintaining similar features of adjoining properties or the streetscape in terms of scale and proportions.</p> <p>The plate height of new dwellings, particularly the portions in view from the street, should approximate the street's typical wall plate height to avoid an under scaled squat appearance.</p> | <p>Replace<br/>5.1.6 C6</p> <p>New<br/>5.1.6 C6.1</p> | <p>Replace<br/>6.1.2 C2</p> <p>New<br/>6.1.2 C2.1</p> | <p>Minimum building height fronting primary street: 3m</p> <p>Maximum height of a dwelling is 2 storeys and comply with the following measurements</p> <p>Top of external wall (roof above) : 7m</p> <p>Top of external wall (concealed roof): 8m</p> <p>Top of pitched roof: 10m<br/>Loft additions will be contained wholly within the roof space and where no visible change is made to the dwelling's appearance from the street.</p> <p>The addition of front facing dormer windows and gables to lofts will cause lofts to be assessed as a storey for the purpose of this policy.</p> |
| New<br>5.1.6 P6.1   |                     | <p>Windows and gables are consistent with the existing dwellings in the street.</p>   | <p>New<br/>C6.1</p>                                   | <p>New<br/>6.1.2 C2.1</p>                             | <p>Loft additions where the development is contained wholly within the roof space and where there is no visible change to the dwellings appearance from the street.</p>  |

| <b>Setback of Garages and Carports</b> |                          |   |  |                          |  |
|--|--------------------------|---|--|--------------------------|--|
| Augment<br>5.2.1                       | Augment<br>6.3.4         | Maintain the absence of visually intrusive car storage facilities within the streetscape.   | Replace<br>5.2.1<br>C1.1,<br>C1.2<br>and<br>C1.5 |                          | Carports, garages or hardstand car bays are to be located at the rear of the property where ROW access is available.<br><br>Carports setback 500mm behind the front setback line where access is from the Primary Street |
| <b>Street Surveillance</b>             |                          |   |  |                          |  |
| Augment<br>5.2.3 P3                    | Augment<br>6.2.1<br>P1.1 | Buildings to be designed to encourage active use and actual surveillance of the front yard by including verandahs, porches or Outdoor Living Areas and by including habitable rooms at the front of the dwelling. | Replace<br>5.2.3<br>C3.1                         | Replace<br>6.2.1<br>C1.1 | Ensure a front verandah is incorporated into the façade of new dwellings (at a minimum of 50% of the width of the front facade) with a useable minimum depth of 1.8 metres.  |
|  |                          |   | Replace<br>5.2.3<br>C3.2                         | Replace<br>6.2.1<br>C1.2 | Incorporate habitable rooms to the front of the development with generous openings to provide street engagement and passive surveillance to the street.  |

| Street Walls and Fences |                     |  |                     |                     |   |
|-------------------------|---------------------|--|---------------------|---------------------|---|
| Augment<br>5.2.3 P3     | Augment<br>6.2.2 P2 | Ensure front boundary fences enhance the streetscape by being designed in the predominant style of the street and are low enough to retain views to dwellings and front gardens. | Replace<br>5.2.4 C4 | Replace<br>6.2.2 C2 | <ul style="list-style-type: none"> <li>a. The maximum height of new fences facing the street is to be 1.2 metres above the adjacent footpath level;</li> <li>b. The maximum height of any solid portion of a new fence is to be 500mm above the adjacent footpath level. The remaining portion is to be a minimum of 40% visually permeable;</li> <li>c. The maximum height of fence piers with decorative capping to be 1.8 metres above the adjacent footpath level; and</li> <li>d. Maximum width of piers to be 470mm;</li> </ul> |

|                                |                      |   |                      |                            |   |
|--------------------------------|----------------------|---|----------------------|----------------------------|---|
|                                |                      |   |                      |                            | <p><b>OR</b></p> <p>e. The maximum height of new fences facing the street is to be 1.8 metres above the adjacent footpath level;</p> <p>f. The maximum height of any solid portion of a new fence is to be 750mm above the adjacent footpath level. The remaining portion is to be a minimum of 80% visually permeable; and</p> <p>g. The maximum height of fence piers with decorative capping to be 2.0 metres above the adjacent footpath level;</p> |
| <b>General Building Design</b> |                      |   |                      |                            |   |
| New<br>5.4.6<br>P6.1           | New<br>6.2.4<br>P4.3 | The roof of a building is to be designed so that it is similar to the existing streetscape character.   | New<br>5.4.6<br>C6.1 | Pursuant<br>to 6.2.4<br>C4 | Roof pitch of ground and upper storeys of between 30 and 40 degrees, with 35 to 45 degree pitches to gables where it can be viewed from the street.   |
| New<br>5.4.6<br>P6.2           | New<br>6.2.4<br>P4.4 | New buildings must respond to the prevailing surrounding character and ensure unsympathetic contrasts in scale, mass and materials are avoided. | New<br>5.4.6<br>C6.2 | Pursuant<br>to 6.2.4<br>C4 | New dwellings' floor levels match, or mediate between, properties on both sides of the proposed dwelling.<br><i>Note: Floor levels of the original dwellings in Janet Street are raised approximately 300mm above footpath level, which assists to define the verandah area and provide additional privacy as views into front rooms are obscured.</i>  |

|  |                   |  |  |                         |   |
|--|-------------------|--|--|-------------------------|---|
| New 5.4.6<br>P6.3  | New 6.2.4<br>P4.5 | Each street facing window should have a style that suits the predominant character of the street and the architectural style of the proposed dwelling.                         | New 5.4.6<br>C6.3  | Pursuant to 6.2.4<br>C4 | Street facing windows have a height of at least 1.6 times their width.  |
| New 5.4.6<br>P6.4  | New 6.2.4<br>P4.6 | Additions visible from the street should be compatible with and respectful of the existing dwelling and the predominant character of the existing dwellings within the street. | New 5.4.6<br>C6.4  | Pursuant to 6.2.4<br>C4 | Additions to existing buildings will only be supported where they are located behind the original facade and set back so that the existing front building and roof form remains the dominant feature to the street. |
| <b>Demolition</b>  |                   |  |  |                         |   |
| Applications for development approval should maintain the front façade of a contributing dwelling. |                   |  | Applications for development approval that retain the original dwelling are considered acceptable. |                         |   |



**Figure 2: Line of Sight**

# BROOKMAN AND MOIR STREETS - HERITAGE AREA GUIDELINES

## PART 1: PRELIMINARY

### STATUTORY BACKGROUND

The Brookman and Moir Streets Heritage Area is designated as a heritage area in accordance with Schedule 2, Part 3, Clause 9 (deemed provisions) of the *Planning and Development (Local Planning Schemes) Regulations 2015* (Regulations).

This local planning policy is adopted consistent with Schedule 2, Part 3, Clause 9 (2) of the deemed provisions of the Regulations, which requires a local planning policy for each heritage area. In addition to the minimum requirements outlined in the deemed provisions, this policy identifies the contribution, or otherwise, of all places within the heritage area; and sets out planning controls that support conservation of the identified heritage values of the area.

Brookman and Moir Streets Heritage Area is also included on the State Register of Heritage Places as it has cultural heritage value at a state level. Under Section 73(1) of the *Heritage Act 2018* the City must elect to refer any development proposal to the Heritage Council for advice if it is considered that the proposal has the potential to impact the significance of the place.

### STATEMENT OF SIGNIFICANCE

The following statement of significance relates to the extent of the heritage area is identified in Map 1.

Brookman and Moir Streets Precinct, two streets in Perth comprising 58 semidetached residences and one detached residence in two types of the Federation Queen Anne style, constructed of limestone and brick with corrugated-iron roofs in 1897-98, and a shop at the corner of Moir Street and Forbes Road built in 1940, has cultural heritage significance for the following reasons:

- The historic precinct is an almost-complete example of two late 19th century streets of modestly scaled residential buildings in the Federation Queen Anne style of architecture, built between 1897-1898 in the wake of the rapid population expansion following the Western Australian gold boom.
- The historic precinct is a substantial section of the residential estate developed by the Colonial Finance Corporation in 1897-1898. This estate, comprising the historic precinct in Brookman and



# BROOKMAN AND MOIR STREETS - HERITAGE AREA GUIDELINES

Moir Streets, and Baker's Terrace in Lake Street, was the largest estate of its type developed in Western Australia.

- The historic precinct is rare in Western Australia as two streets in which a single basic design was utilised for all the residences in a large estate, with the exception of Numbers 2 and 4 Brookman Street, which are grander variations of the same pattern used throughout the precinct, that is relatively intact.
- The buildings contained within the precinct are representative of what was considered to be 'working class' rental accommodation from the late 19<sup>th</sup> and early 20<sup>th</sup> centuries.
- The one-way thoroughfares and modest lot sizes of the semi-detached dwellings contained within the precinct give it a particular character and sense of enclosure.
- The homogeneity of the modestly scaled, semi-detached residential buildings creates a visually striking precinct in an inner-city residential area.
- The historic precinct was developed by the Colonial Finance Corporation who named Brookman and Moir Streets after two of the principal investors in the company who were prominent Western Australians.

Generally, the present property fencing and most plantings are of little significance.

Recent additions and modifications are of little significance, e.g. replacements of original details.

Parking areas in the front of houses, and carports in the front setbacks, are intrusive. A small number of high masonry construction fences in the precinct are intrusive.

## PURPOSE

The primary purpose of these guidelines are to conserve and enhance the heritage significance and cohesive streetscape character of the Brookman and Moir Streets Heritage Area. These guidelines identify those contributory buildings that are to be retained, and that alterations and additions to these buildings are carried out in a way which respects the integrity and aesthetic value of the streetscape, whilst also illustrating innovative architectural and sustainable design excellence.

# BROOKMAN AND MOIR STREETS - HERITAGE AREA GUIDELINES

## OBJECTIVES

The objectives of these guidelines are to:

- retain, conserve, and protect the cultural heritage significance of the Brookman and Moir Streets Heritage Area as identified by its entry on the State Register of Heritage Places and as a designated heritage area protected under the City's Local Planning Scheme No. 2.
- ensure that additions to existing heritage places do not adversely impact the significance of the area, the contributory buildings, or neighbouring heritage places;
- ensure that future development is sympathetic to the existing built form, context of the streetscape, roof form, and public domain in all elements of design;
- maintain and improve existing street vegetation and front gardens in a manner that conserves the significance;
- ensure front fences, if required, are low height or open style and are consistent with the precinct in terms of materiality and colour; and
- encourage a high standard of architectural and sustainable building design for alterations to contributory buildings.



# BROOKMAN AND MOIR STREETS - HERITAGE AREA GUIDELINES

## SCOPE AND CONTRIBUTION OF INDIVIDUAL PLACES

These guidelines apply to all proposals for development within the Brookman and Moir Streets Heritage Area, as shown on Map 1.

| Level of Significance      | Description   | Desired Outcome   |
|----------------------------|---|---|
| <b>Contributes</b>         | Contributes to the significance of the heritage area          | Conservation of the place is desirable. Any external alterations or extensions should reinforce the significance of the area, in accordance with these heritage guidelines. |
| <b>Does not contribute</b> | Does not contribute to the significance of the heritage area. | Existing fabric does not need to be retained. Any new (replacement) development on the site should reinforce the significance of the area, in these heritage guidelines.    |



**Map 1:** Brookman and Moir Streets Heritage Area

# BROOKMAN AND MOIR STREETS - HERITAGE AREA GUIDELINES

## RELATIONSHIP TO THE PLANNING FRAMEWORK

These guidelines are made pursuant to Schedule 2, Part 2 of the Planning and Development (Local Planning Schemes) Regulations 2015 (Regulations) and Clause 3.2.3(a) of State Planning Policy 7.3 Residential Design Codes Volume 1 (R-Codes).

These guideline forms part of the City of Vincent's (City) local planning policy framework. Including but not limited to, the City's Local Planning Policy 7.1.1 Built Form (Built Form Policy) and Local Planning Policy 7.6.1 Heritage Management – Development Guidelines for Heritage and Adjacent Properties (LPP 7.6.1). Where these guidelines are inconsistent with the City's Local Planning Scheme No.2 (Scheme), the Scheme prevails.

Where inconsistency exists between this policy and a state planning policy or another local planning policy, this policy prevails to the extent of the inconsistency.

This is a performance-based policy. Applications for development approval need to demonstrate that the design achieves the objectives of each element. While addressing the deemed-to-comply criteria is likely to achieve the objectives of these guidelines, they do not provide automatic approval and the proposal will be assessed in the context of the entire design solution to ensure the objectives are achieved. Any application will require assessment against LPP 7.6.1 and the objectives of these guidelines. Should development not satisfy the deemed-to-comply requirements, it will require a performance assessment against the relevant local housing objectives of these guidelines, the local housing objectives of the Built Form Policy, design principles of the R-Codes as well as the policy objectives of LPP 7.6.1.

## WORKS NOT REQUIRING DEVELOPMENT APPROVAL

In accordance with Regulation 41 of the *Heritage Regulations 2019*, the following works do not require development approval:

1. Building maintenance that does not involve:
  - i. the removal of, or damage to, the existing fabric of the building; or
  - ii. the use of new materials.
2. Gardening or landscape maintenance that does not involve a major alteration of the layout, contours, structures, significant plant species or other significant features on the land.
3. Repairs, including replacing missing or deteriorated fabric with like for like fabric, that does not involve the removal of, or damage to, the significant fabric of the building.

# BROOKMAN AND MOIR STREETS - HERITAGE AREA GUIDELINES

4. Repainting of the surface of a building:
  - i. in the same colour scheme and paint type if they are appropriate to the substrate and do not endanger the survival of earlier paint layers; and
  - ii. without disturbing or removing an earlier paint layer unless it is chalking, flaking or peeling.
5. Signage that:
  - i. is temporary and does not have a deleterious effect on the fabric of a building; and
  - ii. advertises that a place is for sale or lease but does not remain on the place for more than 10 days after the place is sold or leased.
6. Replacement of utility services using existing routes or voids that does not involve the removal of, or damage to, the fabric of the building.

Applicants are encouraged to speak to the City's Development and Design team should they believe that their works are exempt from requiring development approval.

## PART 2: GUIDELINES PROVISIONS

### DEFINITIONS

Unless otherwise noted, terms used in this policy have common meanings and include those defined in the Planning and Development Act 2005, Regulations, the Heritage Act 2018 and Local Planning Policy: Heritage Area Guidelines.

**Burra Charter** means the document providing guidance for the conservation and management of places of cultural heritage significance and sets standards of practice for those who provide advice, make decisions about, or undertake works to places of cultural heritage significance, including owners, managers and custodians. The charter is adopted by the Australian International Council on Monuments and Sites (ICOMOS).

**contributory place** means places and structures that make a positive contribution to the cultural heritage significance of the Brookman and Moir Heritage Area. Refer to the heritage assessment of the Brookman and Moir precinct on [Inherit](#) for further information.

**non-contributory place** means buildings and structures which do not contribute to the cultural heritage significance of the Brookman and Moir Heritage Area.

# BROOKMAN AND MOIR STREETS - HERITAGE AREA GUIDELINES

**fabric** means all the physical material of a contributory place, including components, fixtures, contents and objects.

**public domain view** means sightline from the front property line on the opposite side of the road of Brookman Street and Moir Street with a viewing height of 1.65 metres above the level of the pavement. The rear right-of-way behind Moir Street and Wellman Street are not included in this requirement. Refer to **Figure 4** for further guidance.

**sympathetic** or **complementary** means a design outcome that respects its context. It would not be identical to historic neighbours, nor would it intrude on their presence in the streetscape. It would be of a similar or lesser scale. See **Explanatory Notes** for further detail.

## ADDITIONAL REQUIREMENTS FOR DEVELOPMENT APPLICATIONS

Applications for development approval within the Brookman and Moir Streets Heritage Area must be accompanied by the following documentation. The City may also elect to obtain its own independent advice on any of the following items to assist in the development application process.

### 1. Heritage Impact Statement

The heritage impact statement (HIS) undertaken by a qualified heritage professional in accordance with the Department of Planning, Lands and Heritage publication 'Heritage Impact Statement: A Guide' will be required for the following types of development proposals:

- i. Partial or full demolition of a contributory place including proposed new development.
- ii. Second storey additions.
- iii. Any other alterations or additions which may be visible from the public domain or could impact the heritage fabric of an existing dwelling.

The HIS is to address the following:

- i. How will the proposed works affect the cultural significance of the place and the Heritage Area?
- ii. What measures (if any) are proposed to ameliorate any adverse impacts?
- iii. Will the proposal result in any heritage conservation benefits that might offset any adverse impacts?

**Note:** Some applications may not require that a HIS be undertaken by a qualified heritage professional and may also not require a HIS. Please contact the City's Development and Design team for further information.

2. A contextual street elevation drawn on one continuous scale no smaller than 1:100 showing the proposed development and the whole of the existing development on each lot immediately adjoining the land subject to the application.

# BROOKMAN AND MOIR STREETS - HERITAGE AREA GUIDELINES

3. Detailed schedule of all finishes, materials and colours of the proposed development and how these relate to the adjoining developments.
4. Sightline drawings of any additions when viewed from the public realm. Diagrams/images from multiple viewpoints from the street will be required.
5. Structural Condition Assessment in case of demolition.

If structural failure is cited as a justification for demolition, evidence in the form of a structural condition assessment is required, provided by a practicing structural engineer with experience in heritage buildings, in line with Building Act requirements. The assessment must demonstrate that the structural integrity of the building has failed to the point where it cannot be rectified without removal of a majority of its significant fabric. Financial considerations are secondary to heritage and structural considerations.

An application for development approval may be referred to the City's Design Review Panel comprising of suitably experienced and qualified members as appointed by the City for advice regarding the proposal.

A condition of development approval will require the submission of a construction and demolition plan prior to the commencement of development. This plan is to be in accordance with the City's Local Planning Policy 7.5.23 Construction Management Plans.

## EXPLANATORY NOTES

Demolition of any building in the heritage area requires approval by Council with advice from the Heritage Council of Western Australia (HCWA).

Contributory places are those identified on **Map 1**.

The rear water closets are an integral part of the original development of the estate. Any demolition, particularly those which are visible from the public domain, needs to ensure that it will not adversely impact the heritage fabric of the place.

The plan of contributory buildings (refer to **Figure 1**) has five principal rooms under the pitched roof and then a series of spaces under a skillion roof of the rear verandah. The rear skillion addition is not intended to be retained or conserved.

Decorative detail includes the treatment of the gables, with timber barges, barge caps, finials, pierced timber fretwork, verge closing boards, modillions, console brackets to gutters, and decorative cast-iron lacework. Refer to **Figure 2** for further guidance.

# BROOKMAN AND MOIR STREETS - HERITAGE AREA GUIDELINES

The original colours of the dwellings were drawn from a very limited palette. Houses that retain their original features sometimes retain strong evidence of the original colours. Paint scraping can reveal original colours and assist with making new colour choices.

The original house plan has five principal rooms under the pitched roof and then a series of spaces under a skillion roof of the rear verandah. Many houses retain the essential elements of this plan and many of the features. The five front rooms and corridor of the house are an integral historical form under the original pitched roof.

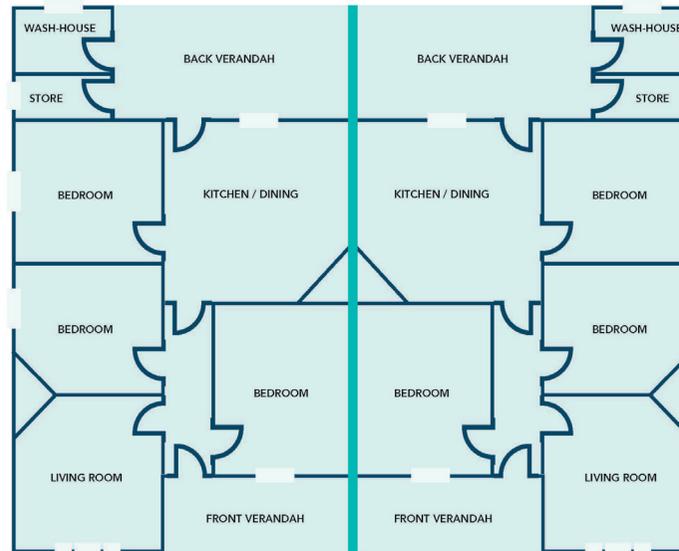
New development should be sympathetic to the existing contributory places and be based upon Burra Charter principles. Design approaches for new development should:

- incorporate subtle architectural style using simple forms and limited material palette;
- use a sympathetic architectural approach that enhances the existing character of the place; and
- use design principles that derive from the architectural language of the existing heritage fabric without imitation.

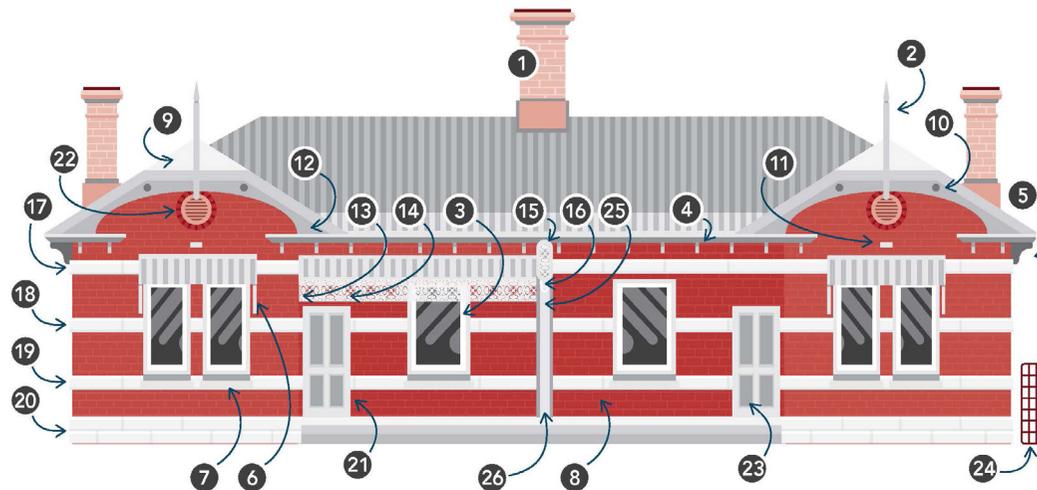
A list of heritage professionals can be found on the HCWA's [inContact](#) website.

# BROOKMAN AND MOIR STREETS - HERITAGE AREA GUIDELINES

Figure 1: Typical mirrored house plan.



1. Tuck pointed brick chimney with rendered decoration
2. Timber finial
3. Flat brick arch - curved soffit (over window)
4. Ogee gutter
5. Timber bracket
6. Timber awning bracket
7. Rendered sill
8. Stretcher bond brickwork
9. Pierced timberwork
10. Applied timber roundels
11. Cast iron roundels
12. Curved bargeboard
13. Cast iron bracket



14. Cast iron frieze
15. Cast stone - vermiculated finish
16. Cast stone bracket
17. Stucco Band
18. Stucco string course
19. Stucco sill course
20. Stucco plinth
21. Closer brick
22. Circular vent
23. Four panelled door
24. Early wire fence
25. Blind niche
26. Fin wall

Figure 2: Typical front elevation.

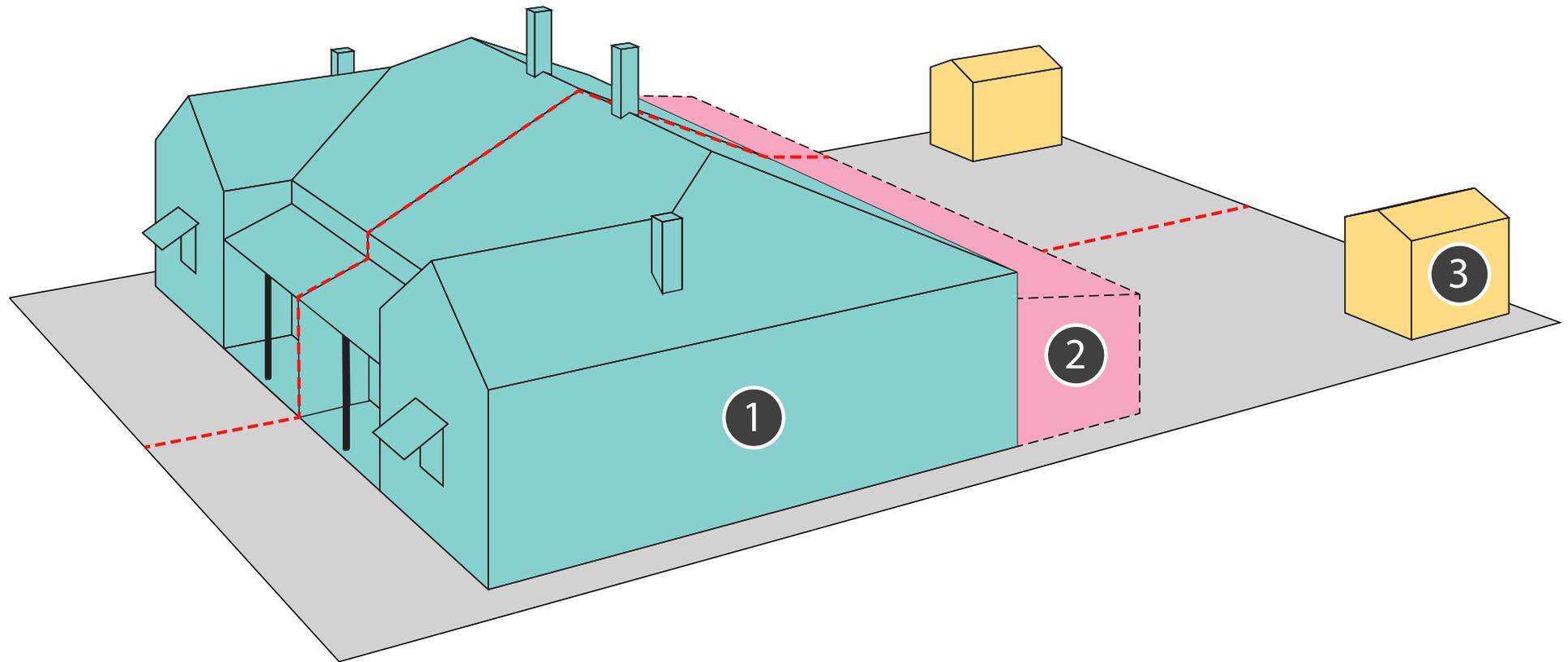
# BROOKMAN AND MOIR STREETS - HERITAGE AREA GUIDELINES

## 1. DEMOLITION

**Intent:** The loss of a contributory place negatively impacts on the cultural significance and character of the Brookman and Moir Streets Heritage Area. Demolition of a contributory building is not appropriate, and demolition should not occur to contributory buildings located in state registered heritage places.

| <p style="text-align: center;"><b>Local Housing Objectives</b></p> <p style="text-align: center;"><i>Pursuant to Clause 3.2.3(c) of the Residential Design Codes Volume 1, the following Local Housing Objectives augment the Design Principles of the Residential Design Codes as specified.</i></p>   | <p style="text-align: center;"><b>Deemed to Comply</b></p> <p style="text-align: center;"><i>Pursuant to Clause 3.2.3(a) of the Residential Design Codes Volume 1, the following provisions replace the Deemed to Comply requirements of the Residential Design Codes as specified.</i></p>  |
|---|--|
| <p>LO1.1 Demolition of the <b>Contributory Place</b> and associated structures to be avoided. In exceptional circumstances where demolition approval is sought the onus rests with the applicant to provide a compelling justification via an assessment by a qualified structural engineer with heritage experience supported by a <b>Heritage Impact Statement</b> prepared by a qualified heritage consultant.</p> <p>LO1.2 Demolition of non-contributory or intrusive elements to accommodate conservation and enhancement of the Brookman and Moir Heritage Area is encouraged.</p> | <p>C1.1 All <b>Contributory Buildings</b> are retained and conserved in accordance with this policy. Refer to Figure 3 for guidance.</p> <p>C1.2 Demolition of the <b>Contributory Place</b> is not permitted. This includes circumstances where there has been a demonstrable period of neglect leading to deterioration in the building's condition.</p> |

# BROOKMAN AND MOIR STREETS - HERITAGE AREA GUIDELINES



**Figure 3:** Retention of Contributory Buildings.

1. Retain the contributory building.
2. Demolition of skillion extension is possible.
3. Retention of the original wash closet

# BROOKMAN AND MOIR STREETS - HERITAGE AREA GUIDELINES

## 2. CONSERVATION OF CONTRIBUTORY PLACES

**Intent:** Conservation works are essential for ensuring long-term survival and contribution of a property to the significance of the Brookman and Moir Streets Heritage Area. Conservation works can include repair, maintenance, restoration and reconstruction. The appropriate conservation approach for individual buildings should be based on an understanding of the significance of that place and in consultation with a qualified heritage professional.

### 2.1 Roofs

LO2.1.1 Roof forms that incorporate proportions, materials and design elements that respect and reference the character of the precinct.

C2.1.1 Replacement roofs on contributory places shall match the original roof form.

C2.1.2 Rolled-top ridges, timber barge caps (not metal), with ogee gutters and circular down pipes are included when replacing roof details.

C2.1.3 Television aerials, air conditioning and other fittings may be roof mounted but are not visible from the public domain.

### 2.2 External Walls

LO2.2.1 Building façades that incorporate proportions, materials and design elements that respect and reference the character of the precinct.

C2.2.1 Original features are retained and conserved. Previously unpainted surfaces must not be painted.

C2.2.2 The streetscape presentation of contributory places reflects their original appearance either by preservation or restoration back to the original aesthetic.

### 2.3 Front Verandahs

LO2.3.1 Front verandahs are retained and conserved to match the original appearance.

C2.3.1 The open verandahs and decorative features must be retained and conserved in their original form where they still exist. Material that is damaged beyond conservation is reconstructed correctly to original detail.

# BROOKMAN AND MOIR STREETS - HERITAGE AREA GUIDELINES

|  |  |
|--|--|
| <b>2.4 Windows</b>   |  |
| LO2.4.1 Windows are retained and conserved to match the original appearance.   | <p>C2.4.1 All original timber window features, including single pane double hung sashes and sun hoods are retained.</p> <p>C2.4.2 Development does not propose enlargement of openings and the use of aluminium window frames.</p> |
| <b>2.5 Front Doors and Hopper Lights</b>   |  |
| LO2.2.1 Building façades that incorporate proportions, materials and design elements that respect and reference the character of the precinct. | C2.5.1 Development does not propose enlargement of openings and the use of aluminium window frames   |
| <b>2.6 Chimneys</b>  |  |
| LO2.6.1 Chimneys are retained and conserved to match the original appearance.  | C2.6.1 All original brick chimneys are retained except those located on the rear skillion addition.  |
| <b>2.3 External Decorative Details</b>   |  |
| LO2.7.1 Decorative details are retained and conserved to match the original appearance.  | C2.7.1 All original decorative details are retained or reconstructed correctly to detail.  |

# BROOKMAN AND MOIR STREETS - HERITAGE AREA GUIDELINES

## 3. BUILT FORM

**Intent:** Most existing buildings can accept some level of new additions without having a negative impact on the cultural significance and character of the Brookman and Moir Streets Heritage Area. New additions do not visually intrude on contributory places or the overall streetscape and are consistent with the character of the area in which they are located taking into account style, scale, materiality, form, function and siting.

*Augments Clause 5.1.2 P2.1 and P2.2*

*Replaces C2.1 - C2.3*

LO3.1 Additions respect the predominant scale (height and bulk) of the contributory building and the heritage area and do not have an adverse visual impact on it.

LO3.2 Additions preserve and enhance the visual character of existing cultural heritage streetscape by considering the form, colours and materiality of the contributory building.

C3.1 Additions should be of a contemporary design that do not mimic the heritage fabric of the contributory building and wider heritage area.

C3.2 Additions shall be setback from main roofline of the contributing building as so they are not visible from the **public domain view** of Brookman and Moir Streets. Refer to **Figure 4**.

C3.3 The wall height of ground floor addition shall not exceed the wall height of the contributory place. Refer to **Figure 4**.

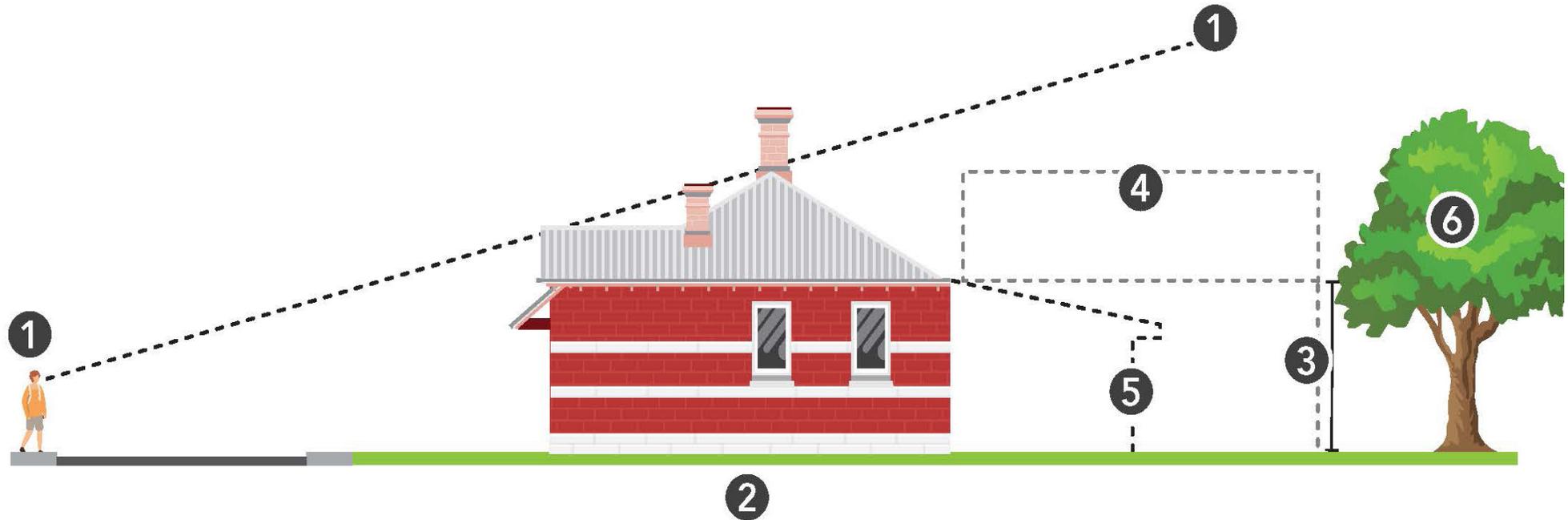
C3.4 The height of second storey additions shall not exceed the maximum ridge height of the contributory place. Refer to **Figure 4**.

C3.5 Side setbacks shall not encroach into the setback established by the contributory building. Refer to **Figure 5**.

C3.6 Additions to corner lots are to be visually and physically separated from the contributory building as to reduce visual impact to the heritage significance of the place.

# BROOKMAN AND MOIR STREETS - HERITAGE AREA GUIDELINES

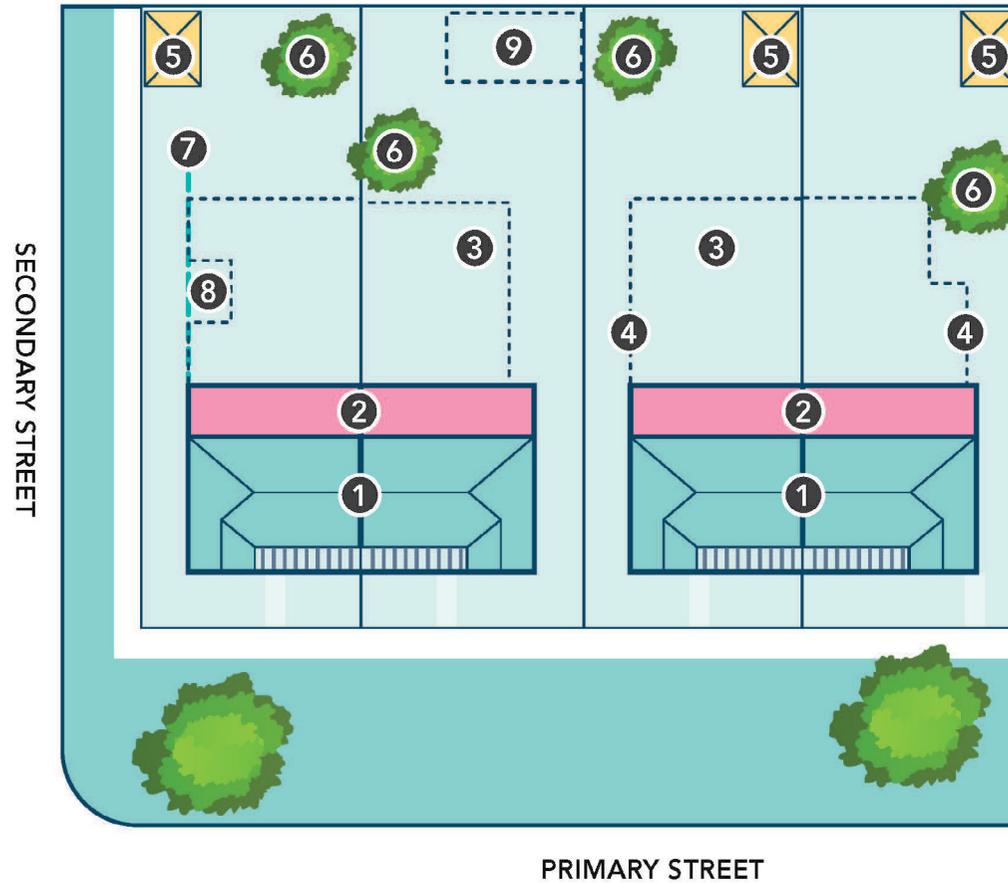
Figure 4: Public domain view



1. Additions are located behind the primary building to areas not visible from the public realm.
2. Retain contributory built form.
3. Ground floor wall height shall not exceed the existing ceiling height of the contributory building.
4. Additions not to exceed the maximum ridge height of contributory building and are to be complementary or sympathetic to the existing built form.
5. Demolition of skillion extension is possible.
6. Retention of mature trees is encouraged.

# BROOKMAN AND MOIR STREETS - HERITAGE AREA GUIDELINES

Figure 5: Potential form and scale of new development



1. Retain the contributory building structure under the primary roof structure.
2. Demolition of skillion extension is possible.
3. Additions to extend behind the primary built form.
4. Facades are in line with the contributory building envelope.
5. Retention of the original water closet which relate to the heritage significance of the place.
6. Retention of any mature vegetation is encouraged.
7. Side setbacks shall not encroach into the setback established by the contributory building.
8. Large expanses of walls visible from neighbouring properties broken up to reduce scale and bulk impact.
9. Structures can be located at the rear, such as garages servicing Wellman Street.

# BROOKMAN AND MOIR STREETS - HERITAGE AREA GUIDELINES

## 4. Front Fences, Secondary Street Fences and the Front Garden

**Intent:** The treatment of front setback area has a significant impact on the streetscape. Retaining uniformity in the scale and proportions of front fences and gates and walls will allow visual transparency between the heritage buildings and the streetscape which is critical in maintaining the character of the Brookman and Moir Streets Heritage Area.

The landscape character of the small front gardens reflects their heritage as workers cottages.

*Augments Clause 5.2.4 P4*

LO4.1 Unless documentary or physical evidence is available, fences should be identifiable as new but also sympathetic to the cultural heritage significance of the precinct.

LO4.2 Fences in the front setback which maintain views of the heritage dwellings.

LO4.3 Any landscaping in the front setback area should not obscure the facade of the contributory building.

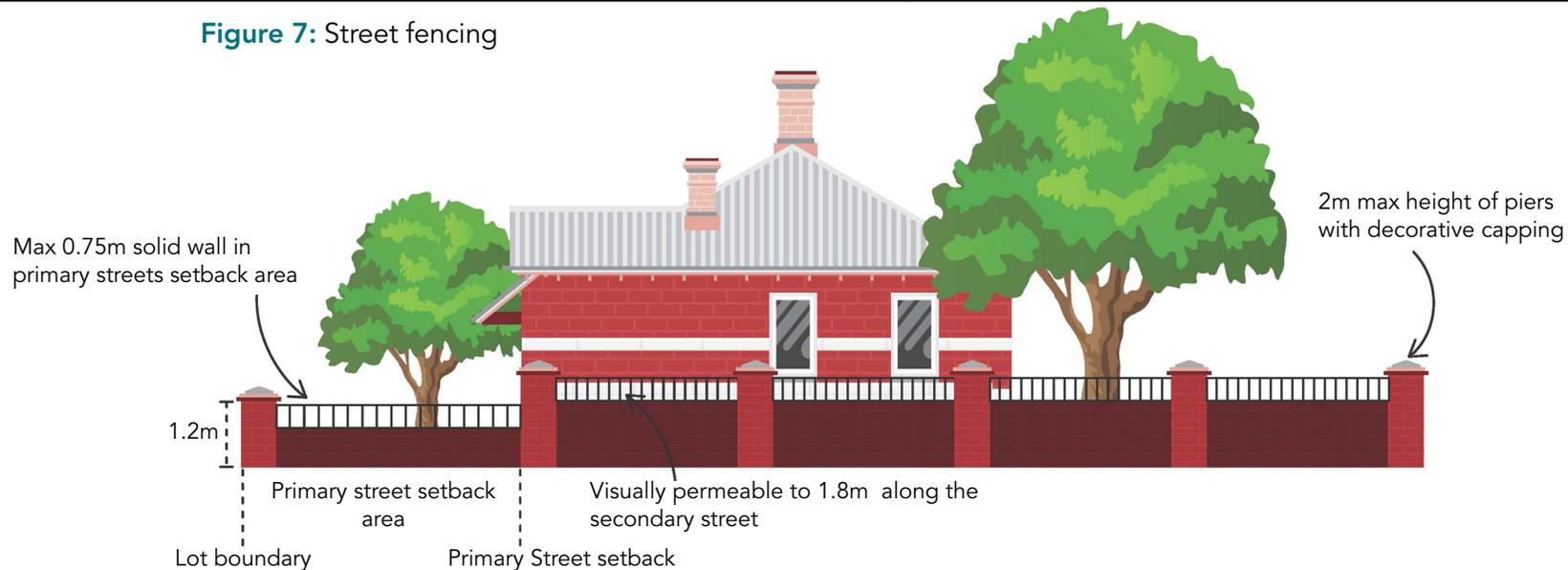
*Replaces C4.1 C4.2 remains*

C4.1. Front fences shall be solid up to 0.75m and visually permeable above to a maximum fence height of 1.2m.

C4.2 Visually permeable secondary street (Forbes Street and Robinson Avenue) fences are to have a maximum height of 1.8m.

C4.3 Street fencing fronting Wellman Street can be solid to a height of 1.8m.

**Figure 7:** Street fencing



# BROOKMAN AND MOIR STREETS - HERITAGE AREA GUIDELINES

## 5. Car Parking

**Intent:** The precinct was developed prior to cars coming into common usage. As a result, the homes within the Brookman and Moir Streets Heritage Area have not been designed to accommodate the parking of vehicles. Garages, carports and open parking areas are an obtrusive element within the streetscape as such they must be carefully sited and designed to avoid negative impacts to the cultural heritage significance of the precinct.

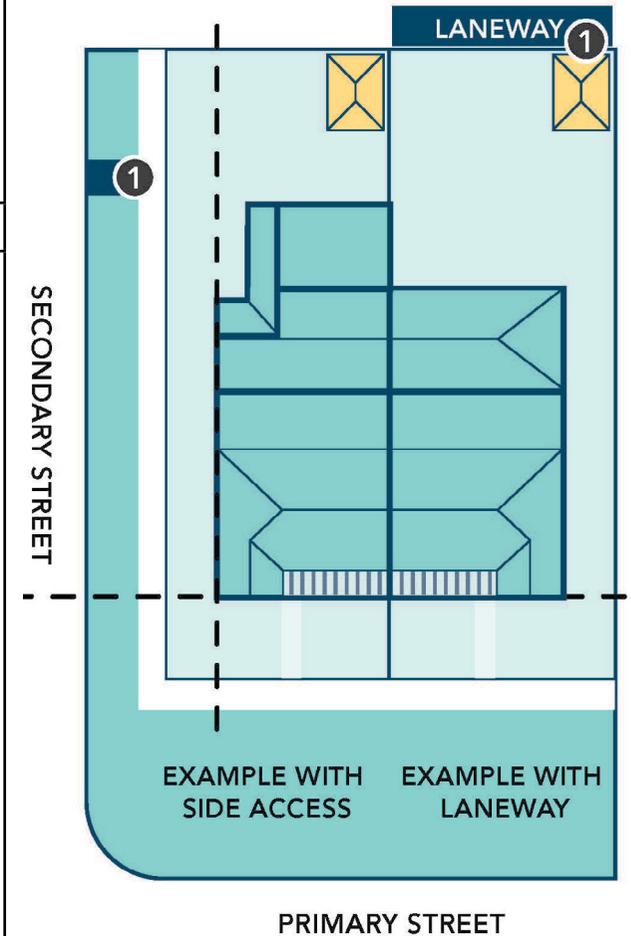
*Augments Clause 5.2.1 P1.1 and P1.2*

*Replaces Clause C1.1 - C1.4*

LO5.1 To ensure that car parking infrastructure, does not have an adverse impact on the cultural significance of the Brookman and Moir Streets Heritage Area.

- C5.1 There is no minimum number of on-site car parking spaces required to be provided.
- C5.2 Car parking and associated structures are not permitted in the front setback area.
- C5.3 Vehicle access shall be located as follows:
- for the street block between Brookman Street and Wellman Street, vehicle access to a garage or carport shall be obtained from Wellman street;
  - for the portion of the heritage area bounded by Moir Street and Lake Street, vehicle access to a garage or carport shall be obtained by the rear right of way;
  - for the street block between Brookman Street and Moir Street, corner lots shall obtain vehicle access to a carport from Forbes Road or Robinson Avenue.
- C5.4 Car parking infrastructure shall not involve the demolition of the original water closet.

Figure 8: Garage and Carport locations



- Access to car parking areas shall be provided from the rear or side only.

# BROOKMAN AND MOIR STREETS - HERITAGE AREA GUIDELINES

## 6. Materials and Colours

**Intent:** The materials, textures, colours and decorative treatments of contributory places are important elements to the cultural heritage significance of the Brookman and Moir Streets Heritage Area and should be used to inform the palette used in conservation works and to guide the selection of materials and colours on new developments.

| Local Housing Objectives   | Deemed to Comply   |
|--|--|
| <p>LO6.1 Materials, colours and finishes reflects the original materials and colours used in the heritage area.</p> <p>LO6.2 For conservation work consideration should be given to original material, colours and finishes of the contributory place, having regard to documentary evidence.</p> <p>LO6.3 Colour choice for new additions may be varied, but consideration should be given to the impact of a colour choice on Brookman and Moir Streets, as a whole.</p> | <p>C6.1 Materials and colours are to be selected with reference to the prevailing contributory character of the heritage area. Houses that retain a high proportion of original fabric, or have been conserved and restored, should have traditional colour schemes, preferably based on the evidence of paint scrapes. Refer HCWA 'Policy and Practice Note for Painting Heritage Places'</p> |



**Figure 9:** The homogeneity of the modestly scaled, semi-detached residential buildings create a visually striking precinct in an inner-city residential area.

# BROOKMAN AND MOIR STREETS - HERITAGE AREA GUIDELINES

## APPENDIX 1: MODIFICATION TO STATE PLANNING POLICY 7.3 RESIDENTIAL DESIGN CODES

| SPP7.3 VOLUME 1 DESIGN ELEMENT           | R-CODE DEEMED-TO-COMPLY REQUIREMENT   | BROOKMAN AND MOIR STREETS HERITAGE AREA GUIDELINES   |
|--|---|--|
| <p><b>5.1.2 Street Setbacks C2.1</b></p> | <p>C2.1 Table 1.<br/>                     Primary street setback: 6m.<br/>                     Secondary street setback: 1.5m.</p>  | <p>3. Built Form.</p> <p>C3.1 Additions should be of a contemporary design that do not mimic the heritage fabric of the contributory building and wider heritage area.</p> <p>C3.2 Additions shall be setback from the main roofline of the contributing building as so they are not visible from the <b>public domain view</b> of Brookman and Moir Streets. Refer to <b>Figure 4</b>.</p> <p>C3.3 The wall height of ground floor additions shall not exceed the wall height of the contributory place. Refer to <b>Figure 4</b>.</p> <p>C3.4 The height of second storey additions shall not exceed the maximum ridge height of the contributory place. Refer to <b>Figure 4</b>.</p> |
| <p><b>5.1.6 Building Height C6</b></p>   | <p>C6: In accordance with Table 3.<br/>                     Top of external wall (roof above): 7m.<br/>                     Top of external wall (concealed roof): 8m.<br/>                     Top of pitched roof: 10m.</p> | <p>3. Built Form</p> <p>C3.4 The height of second storey additions shall not exceed the maximum ridge height of the contributory place. Refer to <b>Figure 4</b>.</p>  |

# BROOKMAN AND MOIR STREETS - HERITAGE AREA GUIDELINES

| SPP7.3 VOLUME 1 DESIGN ELEMENT                               | R-CODE DEEMED-TO-COMPLY REQUIREMENT  | BROOKMAN AND MOIR STREETS HERITAGE AREA GUIDELINES  |
|--|--|---|
| <p><b>5.2.1 Setback of Garages and Carports C1.1-1.5</b></p> | <p>C1.1 Garages setback 4.5m from the primary street or sited at least 0.5m behind the main dwelling frontage, 3m where parallel to street, provided the parallel wall has openings, 1.5m from secondary streets, and on the boundary of communal streets or right of ways.</p> <p>C1.2 Carports setback in accordance with the primary street setback under C1.5.1.2.</p> | <p>5. Car parking.</p> <p>C5.1 There is no minimum number of on-site car parking spaces required to be provided.</p> <p>C5.2 Car parking and associated structures are not permitted in the front setback area.</p> <p>C5.3 Vehicle access shall be located as follows:</p> <ul style="list-style-type: none"> <li>a. for the street block between Brookman Street and Wellman Street, vehicle access to a garage or carport shall be obtained from Wellman street;</li> <li>b. for the portion of the heritage area bounded by Moir Street and Lake Street, vehicle access to a garage or carport shall be obtained by the rear right of way;</li> <li>c. for the street block between Brookman Street and Moir Street, corner lots shall obtain vehicle access to a carport from Forbes Road or Robinson Avenue.</li> </ul> <p>C5.4 Car parking infrastructure shall not involve the demolition of the original water closet.</p> |

# BROOKMAN AND MOIR STREETS - HERITAGE AREA GUIDELINES

| SPP7.3 VOLUME 1 DESIGN ELEMENT                 | R-CODE DEEMED-TO-COMPLY REQUIREMENT   | BROOKMAN AND MOIR STREETS HERITAGE AREA GUIDELINES   |
|--|---|--|
| <p><b>5.2.4 Street Walls and Fences C4</b></p> | <p>C4.1 50% visually permeable above 1.2m height with pillars maximum height.</p> <p>C4.2 Pillars to a height of 1.8m with maximum horizontal dimensions 400mm x 400mm.</p> | <p>4. Front Street Fences, Secondary Street Fence and the Front Garden.</p> <p>C4.1. Front fences can be solid up to 0.75m and visually permeable above to maximum of 1.2m.</p> <p>C4.2. Visually permeable secondary street (Forbes Street and Robinson Avenue) fences are to have a maximum height of 1.8m.</p> <p>C4.3 Street fencing fronting Wellman Street can be solid to a height of 1.8m.</p> |
| <p><b>5.4.3 Outbuildings</b></p>               | <p>C5.4.3(iii) outbuildings are to be setback as per Table 2a and 2B.</p>   | <p>3. Built Form.</p> <p>C3.3 The wall height of ground floor additions shall not exceed the wall height of the contributory place. Refer to <b>Figure 4</b>.</p> <p>C3.5 Side setbacks shall not encroach into the setback established by the contributory building. Refer to <b>Figure 5</b>.</p>  |

# BROOKMAN AND MOIR STREETS - HERITAGE AREA GUIDELINES

| SPP7.3 VOLUME 1 DESIGN ELEMENT  | R-CODE DEEMED-TO-COMPLY REQUIREMENT   | BROOKMAN AND MOIR STREETS HERITAGE AREA GUIDELINES  |
|---|---|---|
| <b>External Fixtures, Utilities and Facilities 5.4.4</b><br><b>C4.1, C4.2, C4.3, C4.4</b> | <p>Solar collectors installed on the roof or other parts of buildings</p> <p>Television aerials and plumbing vent pipes sited above roof line and external roof water down pipes</p> <p>All other fixtures concealed from view.</p> | <p>As per Built Form Policy requirements.</p> <p>Also refer to HCWA Guidelines for Renewable Energy Systems in State Registered Places.</p> |

# BROOKMAN AND MOIR STREETS - HERITAGE AREA GUIDELINES

## APPENDIX 2: POLICY GUIDANCE

The key documents for assessing development applications within the Brookman and Moir Streets Heritage Area are the:

- Brookman and Moir Streets Heritage Area Guidelines;
- Local Planning Policy 7.1.1 Built Form (Built Form Policy);
- State Planning Policy 7.3 Residential Design Codes (R-Codes); and
- Local Planning Policy 7.6.1 Heritage Management - Development Guidelines for Heritage and Adjacent Properties (LPP 7.6.1).

To the extent that they are relevant to the development application, applicants are also to have due regard to a range of matters listed in Clause 67(2) of the Regulations.

| KEY CRITERIA                 | APPLICABLE POLICY   |                              |                                      |                                      |
|------------------------------|---|------------------------------|--------------------------------------|--------------------------------------|
|                              | Brookman and Moir Streets Heritage Area Guidelines          | Built Form Policy            | R-Codes                              | LPP 7.6.1                            |
| <b>Demolition</b>            | Clause 1 - Demolition                                       |                              |                                      | A3.2 and A3.2                        |
| <b>Building height</b>       | Clause 3 - Built Form                                       | Clause 5.3 - Building Height |                                      |                                      |
| <b>Conservation</b>          | Clause 2 - Conservation<br>Clause 6 - Materials and Colours |                              |                                      | A1.1 and A1.2,<br>A2.4-A2.6          |
| <b>Street setbacks</b>       | Clause 3 - Built Form                                       | Clause 5.1 - Street Setback  |                                      |                                      |
| <b>Lot boundary setbacks</b> |   |                              | Clause 5.1.3 - Lot Boundary setbacks | A2.1 - Building Scale, Bulk and Mass |
| <b>Open space</b>            |   |                              | 5.1 4 - Open Space                   |                                      |

# BROOKMAN AND MOIR STREETS - HERITAGE AREA GUIDELINES

| KEY CRITERIA                | APPLICABLE POLICY   |   |  |  |
|-----------------------------|---|---|--|--|
|                             | Brookman and Moir Streets Heritage Area Guidelines                    | Built Form Policy   | R-Codes  | LPP 7.6.1  |
| Street fencing and walls    | Clause 4 - Front Fences, Secondary Street Fences and the Front Garden |   |  |  |
| Solar access                |   |   | Clause 5.4.2 - Solar Access for Adjoining Sites<br>25% of the adjoining southern lot |  |
| Visual privacy              |   |   | Clause 5.4.1 Visual Privacy  |  |
| Utility location            |   | Clause 5.10 - External Fixtures, Utilities and Facilities |  | Clause 6 - Solar Panels to Heritage Listed Buildings |
| Car parking                 | Cause 5 - Car parking   | 5.4 - Garages and Carports                                |  |  |
| Appearance of new additions | Clause 3 - Built Form   |   |  | A2.1 Building Scale, Bulk and Mass                   |
| Landscaping                 | Clause 4 - Front Fences, Secondary Street Fences and the Front Garden |   | Clause 5.3.2 - Landscaping   |  |