



## CITY OF VINCENT



# RESIDENTIAL DESIGN ELEMENTS POLICY

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## **1.0 INTRODUCTION**

The City of Vincent and its community places great value on the residential amenity and character that can be found within the City. Comprising numerous localities and defined precincts, the City recognises that there are distinct differences in local character between the various localities. The unique character of each of these localities is greatly valued by residents, and every effort should be made to preserve and enhance this character while not compromising future residential development.

For the purpose of preserving and maintaining this residential amenity, while still allowing new development to occur, the City has prepared the Residential Design Elements Policy. The purpose of the Residential Design Elements Policy is to provide a comprehensive basis for the control and management of residential development within the City of Vincent.

The Policy have been constructed to function as a tool that will assist in protecting the existing character of residential areas within the City of Vincent, while at the same time allowing for new development to occur in a balanced manner.

## **2.0 AIMS OF RESIDENTIAL DESIGN ELEMENTS POLICY**

The primary aims of the Residential Design Elements Policy are to:

- encourage residential development that harmonises with the existing streetscape and complements the character of the locality;
- promote housing design of the highest possible quality;
- manage residential development in a way that recognises the needs of innovative design and contemporary lifestyles;
- clarify the assessment provisions in order to create consistent and desired residential development outcomes for all stakeholders;
- encourage building design that incorporates sustainable and energy efficient design that befits the local climate and provides comfortable living conditions;
- respect places of State and local heritage significance;
- encourage a diversity in housing styles, while facilitating new residential development that complements the character of the street;
- promote 'Crime Prevention Through Environmental Design' (CPTED) principles in residential developments; and
- provide a comprehensive set of residential design guidelines that reflects the values of the wider community and a transparent assessment process.

## **3.0 STATUS OF RESIDENTIAL DESIGN ELEMENTS POLICY**

All residential development is required to comply with the Residential Design Elements Policy, in addition to the City of Vincent Town Planning Scheme No.1 (Scheme) and the Residential Design Codes (R Codes), which, legally, form part of the Scheme.

This Policy is a planning policy pursuant to the Scheme. This means that both the City and intending developers are obliged to take the Policy fully into account. Variations and departures are possible; however need to be suitably justified by both the developer and the City.

The R Codes state that a planning application is not needed for single dwellings, except on lots less than 350 square metres. However, in many cases proposed developments require the City to use its discretion to relax one or more requirements of the Scheme, the R Codes and the Residential Design Elements Policy. Where development seeks a variation to the requirements of the R Codes or this Policy, regardless of the size of the lot, it will be necessary to lodge an application for planning approval pursuant to the Scheme.

## **4.0 RELATIONSHIP WITH OTHER DOCUMENTS**

### **4.1 Relationships**

The Residential Design Elements Policy is inherently interrelated and influenced by a number of other policies and strategies that relate to residential development at both a local and State wide level. This Policy is a crucial reference for any person considering residential development within the City, as it clearly sets out the City's and community's expectations of residential development and what is considered acceptable and unacceptable.

### **4.2 Residential Design Codes**

The main tool for controlling residential development in Western Australia is the Residential Design Codes (R Codes). While the R Codes are very comprehensive, the R Codes also recognise that by its very nature, the R Codes cannot cater for specific differences that may exist from one local authority to another. Therefore, the R Codes allow for variations to some of the provisions, where there may be a different or unique local context in terms of residential development.

Part 2.6 'Local Planning Policies' of the R Codes allows the Council to prepare local planning policies for streetscape, building design, boundary walls, and building height that provide alternative requirements to the R Codes to suit the local planning context. This Policy has been structured around these variations that are permitted by the R Codes, with the intent to provide clear and concise guidance for residential development within the City of Vincent.

The Residential Design Elements Policy endeavours to complement the R Codes, by augmenting and further developing specific provisions with more specific policy measures pertinent to residential development within the City of Vincent. **If there are inconsistencies between the provisions of the R Codes and the Residential Design Elements Policy, the provisions of the Policy overrides the R Codes.**

Access to the most up to date information and policies relating to residential development within the City can also be found on the City's website, [www.vincent.wa.gov.au](http://www.vincent.wa.gov.au)

**4.3 Vincent Vision 2024**

*Vincent Vision 2024* was a community engagement project that was successfully undertaken by the City in 2005 as part of the review of Town Planning Scheme.

No.1. *Vincent Vision 2024* was the process, which actively engaged the community in voicing its issues and values with the City, and encouraged a visioning process whereby the community was able to develop visions for the City as it saw it in 2024.

*Vincent Vision 2024* has provided the City with valuable and fundamental background information for the progression of the future planning of the City of Vincent, and ensured that the community’s issues, values and visions were subsequently represented in all Council documentation and operations.

The outcomes and subsequent recommendations resulting from *Vincent Vision 2024*, served as further impetus and justification for the Residential Design Elements Policy to be formulated. Housing density and maintaining the residential amenity within the City was identified as one of the most important issues and challenges to the community and for the City to address. Such factors as changes through the subdivision of land and the demolition of older houses have altered the character of some areas within the City, and through the community visioning process, the community’s desire to see the preservation and enhancement rather than the destruction of the residential character within the City, was strongly resonated.

The importance of the outcomes from *Vincent Vision 2024* given by the City in recognising the community’s issues, values and visions, particularly in regard to residential development have been cross-referenced throughout this Policy.

A summary of the *Vincent Vision 2024* process in regards to residential development is as follows:

Stage	Action	Outcome
1. Understanding Vincent	This involved asking the community where are we now and where are we going?	A list of values and issues identified by the community.
2. Creating the Vision	This involved asking the community where do we want to be?	A list of guiding principles.
3. Achieving the Vision	This questions how do we get there and how do we make sure we get there?	Residential Design Elements Policy incorporating the guiding principles from stage 2 to ensue the future residential development in the City is indicative of the community’s aspirations and visions.

#### **4.4 Other Local Planning Policies**

The Residential Design Elements Policy will replace 39 of the City's Policies relating to Residential Design Guidelines (including 31 Locality Statements) and 8 of the City's Policies relating to Residential Design Guidelines – Design Elements (Local Character, Environmental Design, Street Setbacks, Street Walls and Fences, Vehicular Access, Site Levels, Building Scale and Privacy).

While the Residential Design Elements Policy functions as the primary reference tool for residential development within the City, it is important for the developers and assessors to also be aware of other relevant residential development related policies.

The Residential Design Elements Policy is intended to be a generic basis from which residential development is controlled and managed, however, it is also recommended that the planning policies relating to the following matters also be considered as part of the preparation process prior to submitting a development application:

- Residential Streetscapes;
- Residential Subdivisions;
- Single Bedroom Dwellings;
- Parking and Access; and
- Heritage Management.

## **5.0 HOW THE RESIDENTIAL DESIGN ELEMENTS POLICY FUNCTIONS**

### **5.1 Functions of Components**

The Residential Design Elements Policy has been developed specifically to function as a reference tool providing guidance for both the City and developers to use when considering an application for a residential development. As mentioned previously, the Residential Design Elements Policy has been structured to correlate directly with the R Codes, providing more specific information relating to residential development as it relates to the local planning context within the City of Vincent. Where no other local planning policy is in place, the R Codes and the Scheme will apply in relation to residential development.

The Residential Design Elements Policy consists of two sections. Its structure is such that when read in conjunction with the R Codes, the two documents can effectively be read as one.

The detailed development requirements are set out under the following design elements:

- Streetscape; and
- Building Design.

Each of the above design elements comprises the following components:

- Overview;
- Vincent Vision;
- Objectives;
- Guidance Notes; and
- Assessment Table comprising Design Solution and Deemed-to-Comply Criteria.

The “*Vincent Vision*” specified for each element outlines the community’s values and visions identified as part of the *Vincent Vision 2024* project in relation to the subject element. As a result, various guidance principles are outlined to implement the community’s values and visions into the Residential Design Elements Policy and therefore, future residential development within the City.

The “*Objectives*” specified for each design element describes the residential development outcomes that the City is attempting to achieve.

The “*Guidance Notes*” provide introductory, explanatory and additional information that supplements the Assessment Table to achieve the desired development outcome for each element. The Guidance Notes also provide development direction when variations to the specific Deemed-to-Comply Criteria are proposed. For these reasons the Guidance Notes are to be read in conjunction with the Assessment Table.

The “*Design Solution*” in the Assessment Table also outline the provisions that are to be satisfied in order to achieve the desired development outcome. The Design Solution details both prescriptive and performance based provisions. The City will assess the development proposal against these criteria when assessing and determining an application.

The “*Deemed-to-Comply Criteria*” in the Assessment Table, provide a means by which development can be “deemed to comply” with the relevant Design Solution and therefore provide a speedy and certain path to approval.’

Additionally, design illustrations have been included that graphically represent the Deemed-to-Comply Criteria. The design solutions that are provided in the illustrations are by no means exhaustive, and applicants are encouraged to develop acceptable design responses that are site specific.

**5.2 Development Assessment**

Development which complies with the provisions of the R Codes and the Residential Design Elements Policy is still required not to unduly impact on the amenity of the area and be compatible with the existing development and character in the surrounding area.

Development which meets the relevant Deemed-to-Comply Criteria is likely to be approved. Where the Design Solution is not met, development will be assessed against the relevant Deemed-to-Comply Criteria. The applicant must provide written justification that the proposal satisfies the relevant Design Solution. The City may approve a development proposal which does not fully comply with the Deemed-to-Comply Criteria, if the City is satisfied that the development proposal meets the relevant Design Solution.

It is noted that the City generally discourages an accumulation of variations to development requirements however consideration will be given to variations dependant on the nature and extent of variation.

Where considered appropriate, the City may require a development application to be submitted with an accompanying Amenity Impact Statement which:

- demonstrates consideration has been given to the impact on the amenity of adjacent properties and the streetscape; and
- outlines any measures that have been or will be taken to mitigate any likely undue impacts on the amenity of adjacent properties.

Please note that it may be helpful to arrange a pre-lodgement meeting with a Planning Officer to discuss any potential issues with the proposal and possible solutions.



**5.3 Advertising**

Applications for development seeking variations to any part of the R Codes and/or this Policy will be required to be advertised in accordance with Clause 37 of the Scheme. The advertising process will be guided by the City’s Community Consultation Policy.

**5.4 Approval Process**

Part 2.3 ‘The Approval Process’ and Part 2.4 ‘Accompanying Information’ of the R Codes provides details of the necessary information to be submitted with any application. Applications which are incomplete cannot be processed until the necessary accompanying information has been provided.

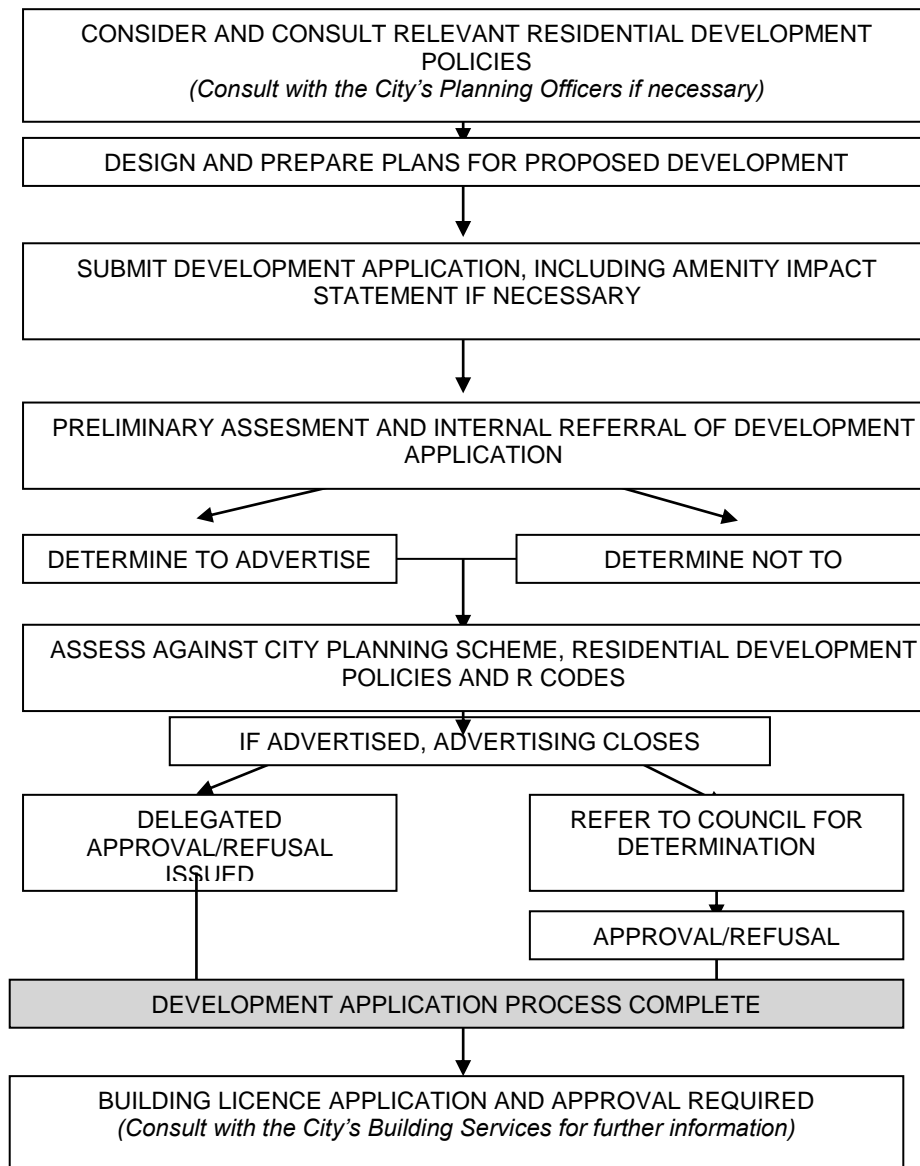


Figure 1: Development Approval Process

## 6.0 STREETScape

### 6.1 Overview

Streetscape refers to the visual appearance of a street. Streetscape assessment and site analysis are the first steps in the design process and ensure that the proposal is the most appropriate design solution for both the site and the locality.

The presentation of buildings in a street is a critical element of a streetscape and contributes significantly to the character of both the street and the locality.

Desirable streetscapes are those in which the dwellings and associated spaces create attractive neighbourhoods and therefore development needs to be sensitive to the environmental context in which it is being placed.

### 6.2 Vincent Vision 2024

The *Vincent Vision 2024* project indicates the community values the following relating to streetscape:

- *Heritage buildings, intact streetscapes and a sense of place;*
- *Current density, scale and family friendly housing; and*
- *Open streetscapes and passive surveillance.*

Issues relating to streetscape identified by the community are as follows:

- *Poor design and impacts on the streetscape;*
- *Neglected houses and poor streetscapes; and*
- *High front walls.*

The values and issues identified by the community form the following guiding principles relating to streetscape, and these principles are implemented into future residential development through the Guidance Notes and Assessment Table in this Policy:

- *Streetscapes are a vital and integral part of a quality residential and neighbourhood environment;*
- *Setbacks and scale are such that the height of buildings do not dominate the street and retain a human scale and quality;*
- *Permeable fencing in front of the building line lowers the security risk by not concealing the building from the passive surveillance of the street;*
- *A safe pedestrian friendly environment is achieved by reducing the speed and dominance of vehicle traffic, with quality footpaths and street trees on both sides of the road; and*
- *Car parking solutions are innovative and reduce the visual dominance and presence of cars in the environment.*

### 6.3 Objectives

- To ensure that development is sensitive to the streetscape and established character of the locality;
- To reinforce existing locality characteristics such as dwelling setbacks, heights, landscaping patterns and fence design;
- To ensure that the appearance of new development is of a high quality and contributes positively to the existing streetscape;

- To promote development that reinforces the dominant streetscape rhythm and considers issues such as spacing and proportion of built form;
- To preserve the natural, built and cultural significance of streetscapes.
- To minimise the impact of vehicles on the amenity of the streetscape;
- To reduce resident on-site parking where public transport is available within reasonable walking distance or where low car parking demand exists;
- To minimise the extent of hard surfaces forward of the building line; and
- To ensure that garages, carports and other developments within the front setback area do not detract from the dominant elements of the dwellings within the streetscape.

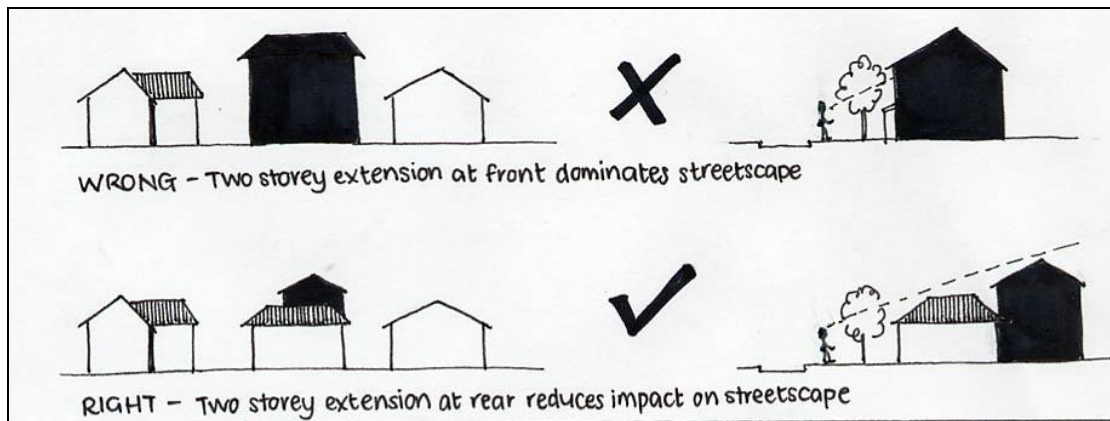
**6.4 Guidance Notes**

**6.4.1 Streetscape Character**

Residential development, including alterations and additions, should complement and enhance the existing streetscape. New development should be designed to harmonise with the streetscape and adjoining properties. Development which visually dominates to have an undue impact on the streetscape or existing properties in building form, style or height, will be discouraged.

Single storey streetscapes are greatly valued within the City, and should be enhanced wherever possible. Any new dwellings or extensions, which are located within an area that is characterised by single storey buildings, should be mindful of the streetscape character.

Where a new two storey development or extension is proposed, it must be demonstrated by the applicant that the impact of the development will not unduly affect the streetscape.



*Figure 2: Examples of Desirable and Undesirable Dwellings along a Street with regard to Bulk and Scale*

**6.4.2 Building Setbacks**

Setbacks define the overall footprint of a dwelling in relation to the front, side and rear boundaries.

Appropriate street setback controls can positively contribute to the public domain by creating a more open streetscape character and a consistency of street facades. Street setbacks can also be used to enhance the setting for the dwelling.

Street setbacks are measured from the front boundary to the outer edge of the dwelling wall. The separation between dwellings is also important and contributes to the overall form of the locality and the rhythm of the streetscape.

To retain and enhance the character of a streetscape, the City expects designers and developers to adopt setbacks that are in harmony with adjoining and adjacent land and which will not unduly detract from an existing streetscape. The City reserves the right to require greater setbacks than specified in the R Codes, in situations where it is important for preserving streetscape and harmony of design.

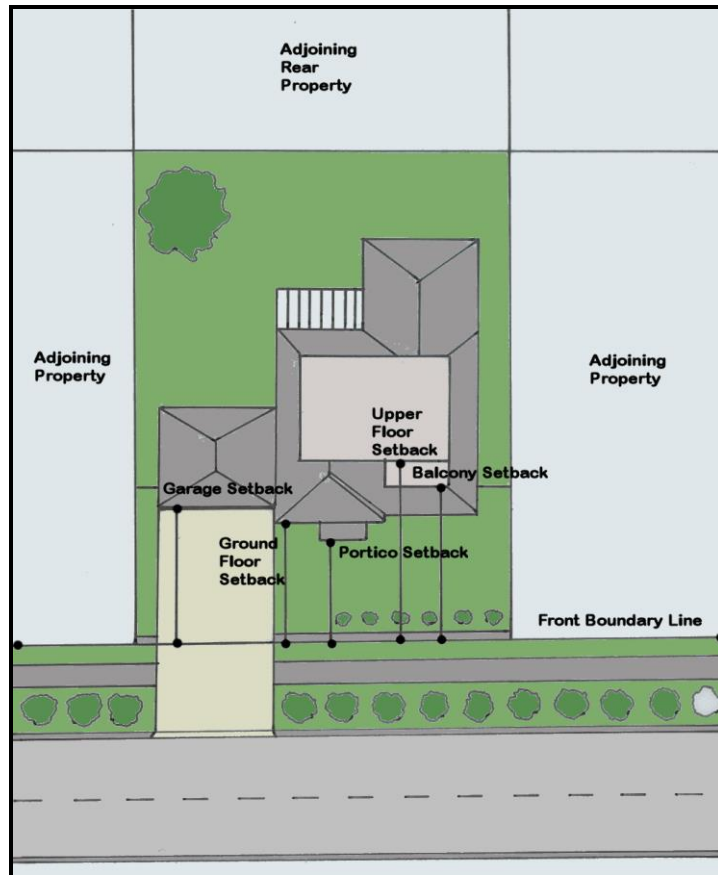


Figure 3: The Measurement of Setbacks

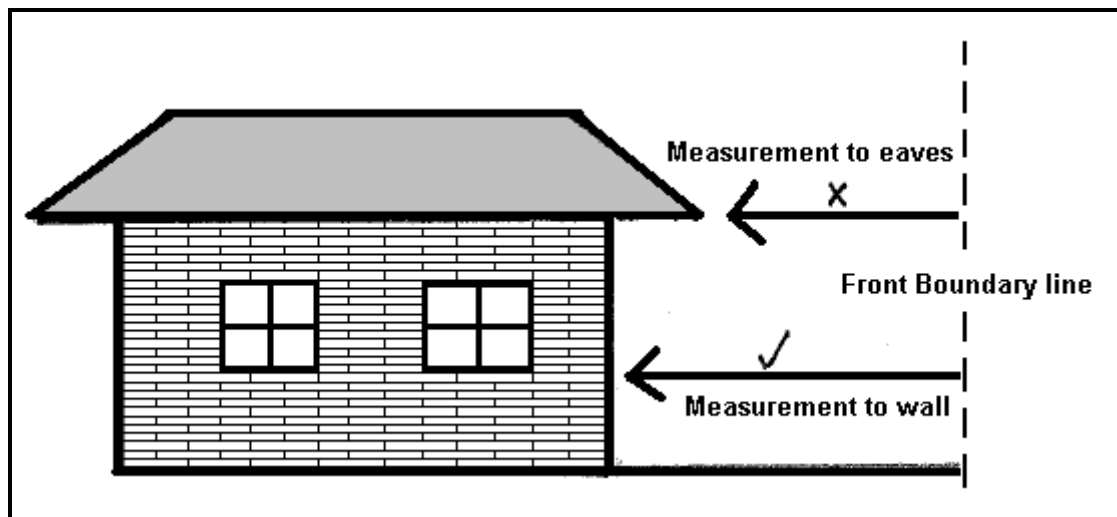


Figure 4: The Measurement of Setbacks (side view)

**(i) Street Setbacks**

The street setback area is the area between the front/street boundary and the building line as determined pursuant to Deemed-to-Comply Criteria SADC 5. Street Setbacks.

Development must be appropriately located on the site having regard to the existing setback of adjoining properties, and the setback pattern of the street block within which the proposed development or addition is situated.



Figure 5: Demonstration of Street Setback Area

**(ii) Minor Incursions into Street Setback Area**

Minor incursions and projections are permitted where it will not detract from the character of the streetscape or dominate the appearance of the existing dwelling. A minor incursion refers to a porch, balcony, verandah, chimney or the equivalent.

**(iii) Side Setbacks**

Development must be appropriately located on the site having regard to the existing setback of adjoining properties, and the setback pattern of the street block within which the proposed development or addition is situated.

The City recognises that in some instances variations to side setback requirements can be considered. If a variation to a side setback requirement is proposed the subject wall must be vertically articulated to the satisfaction of City. In these instances, building walls are permitted to have a maximum of 30 per cent of the length of the subject walls being “flush”; meaning a maximum of 30 per cent of the upper floor may be vertically in line with the ground floor. The remaining 70 percent of the walls must have a vertically staggered setback to the ground floor to reduce the impact on the affected neighbour and add interest to the building design.

The setbacks for additions to existing dwellings of heritage significance may be acceptable where they do not distort or obscure the cultural heritage significance of the place or detract from its interpretation or appreciation.

**(iv) Setbacks of Garages and Carports**

Garages and carports are not to visually dominate the site or the streetscape, and not to detract from the dominant elements of dwellings within the streetscape context.

Garages and carports are to be located behind the street setback line and at the side of the dwelling where space exists and accessed from an abutting right of way where a right of way exists.

The City recognises that there are exceptional circumstances where garages and carports can be accessed by the primary street regardless whether a right of way is available to the property. One such instance is where more than 50 per cent of the dwellings in the immediate street block, on the same side of the street that the subject dwelling is located have carports or garages accessed from the primary street, regardless that a right of way is available. In these instances the carports or garages must respect the existing streetscape and will be assessed on their own merits with careful consideration of the streetscape and impact on surrounding amenity.

Street block refers to the collection of lots on both sides of the street bounded by the nearest intersecting streets.

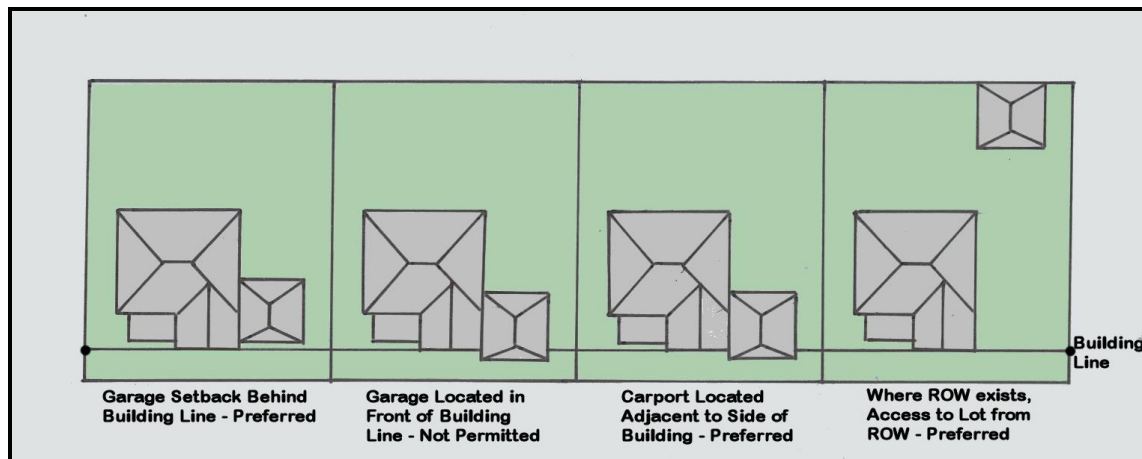


Figure 6: Location of Garages and Carports on Lots

**(v) Setbacks from Rights of Way**

A minimum manoeuvring depth of 6 metres (including truncations where necessary) must be achieved for any development utilising a right of way for vehicular access onto a property. If the right of way width is less than 6 metres, the development must be setback to provide the deficit distance required to achieve the minimum 6 metre manoeuvring distance.

Future widening of rights of way to 6 metres will require landowners abutting a right of way to provide half the deficit distance to achieve the 6 metres free of any type of development. Notwithstanding this, building setbacks to a right of way will be measured from the original right of way/property boundary provided that the deficit distance required to achieve the 6 metre width is free of any type of development.

Where a pedestrian access way is proposed adjacent to a right of way, half the deficit distance required to achieve the minimum 6 metre right of way width must be provided free of any kind of development in addition to the pedestrian access way so the pedestrian access way is not reduced if widening of the right of way occurs.

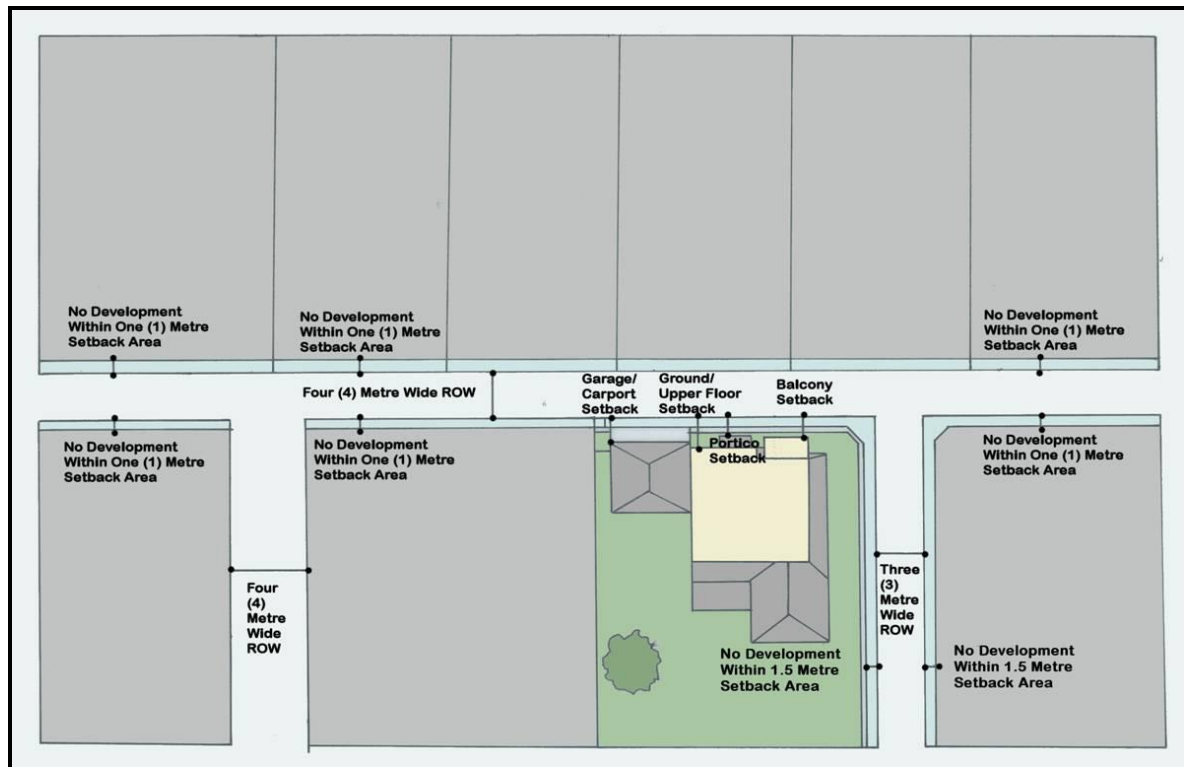


Figure 7: Setback Requirements for Development Adjacent to Rights of Way

**(vi) Dual Street Frontages and Corner Sites**

For properties with dual street frontages (that is lots abutting gazetted roads on both the front and side/rear property boundaries), the primary street frontage will be determined by the established streetscape pattern and the direction most dwellings are oriented.

In these cases the development may have a reduced building setback for streetscape reasons.

**(vii) Buildings on Boundary**

The City of Vincent is located within an inner urban area, being only 3 kilometres from the Perth Central Business District. Consequently the density of housing is generally higher than in the more outer metropolitan local governments. With the higher densities, in order to accommodate dwellings, the setback distances between dwellings are generally less than those setbacks found in the outer suburbs.

As a result, there should be an expectation and acceptance that in some circumstances, where medium to higher density development is permitted, buildings, or part thereof, will be built on common boundaries. In these instances, there are important considerations that must be made, when designing a new dwelling or extension to an existing dwelling.

Nil setbacks will be guided primarily by the provisions of the R Codes. Nil setbacks on more than one boundary and two storey building boundary walls will generally not be supported, unless suitable justification is provided, and an Amenity Impact Statement is submitted as part of the application for development.

### **6.4.3 Safety and Security**

Development should be designed to:

- Enable residents to survey streets, public areas and approaches to dwelling entries;
- Provide adequate lighting to all paths, access ways, parking areas and building entries;
- Reduce the potential for crime, vandalism and fear; and
- Enhance the 'sense of community'.

The following considerations should be given when designing a new dwelling or additions to an existing dwelling:

#### **(i) Surveillance**

Development is to be located and incorporate design features and activities that create a perception of increased risk of detection for perpetrators of criminal activity and of increased safety and security for legitimate users.

#### **(ii) Access Control**

Development is to incorporate design features that may deny offenders access to targets, reduce their escape opportunities and guide legitimate users through the environment. Natural access control involves the use of the environment to clearly mark borders and transitional zones to psychologically deter movement of illegitimate users into protected (private) space.

#### **(iii) Target Hardening (Security Measures)**

Target hardening is the physical securing of buildings and places from offenders. Areas that are rundown and the subject of graffiti and vandalism are generally more intimidating than areas, which do not display such characteristics. The removal of indicators of crime and the use of positive maintenance with lighting, painting, management of vegetation creates a cared-for environment that can reduce the fear of crime and encourage legitimate behaviour.

### **6.4.4 Street Walls and Fences**

#### **(i) Design**

The character of a street can be easily defined by a number of influences, in addition to the housing styles and setbacks. Street walls and fences are such influences, which can affect the appearance and feel of a street, depending on the design and the interrelationship between the dwelling and the street.

Within the City of Vincent, walls and fences adjacent to primary and secondary streets are considered particularly important to the visual integrity of a streetscape and local character of an area. High solid walls adjacent to a street are undesirable as they disrupt the streetscape, destroy the setting of the dwelling and compromise security. It is appropriate to design street walls and fences to ensure that a clear view exists between the dwelling (particularly its main entry) and the street. Fences higher than 1.2 metres are to be 'visually permeable'.

Should a solid wall be needed because of traffic noise or glare, it is to be designed to occupy as little of the width of the lot as necessary.



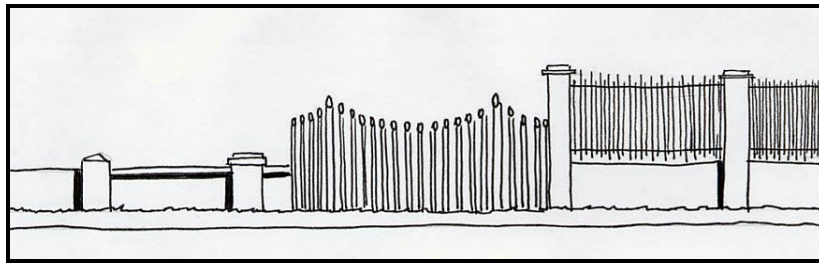


Figure 8: Acceptable Fencing Types

Street walls and fences are not to substantially obstruct views of the dwelling, including the main entry from the street.



Figure 9: Front Fencing Types – Encouraged

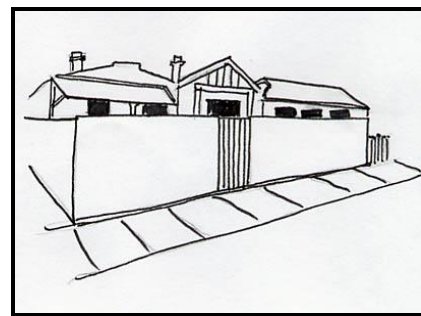


Figure 10: Front Fencing Types - Discouraged



Figure 11: Compliant Fencing and Visual Permeability

The Deemed-to-Comply Criteria for street walls and fences may be varied in the following instances:

- For sites that slope more than 300 millimetres, the height requirements may be varied, however by no more than 300 millimetres after which the wall and fence must be stepped down; and
- To incorporate meter boxes into the wall and fence and such meter box wall and fence must be a maximum length of 1.0 metre and perpendicular to the street boundary.

In such instances, the street walls and fences are to incorporate one or more of the following elements:

- Landscaping to soften the impact of the walls and fences on the streetscape;
- The use of high quality, light-weight materials to compensate for the variation; and
- The incorporation of additional design features to add interest to the streetscape.

**(ii) Measuring Height of Walls and Fences**

*Street Walls and Fences:* The height is measured from the adjacent footpath level to the top of the fence and wall.

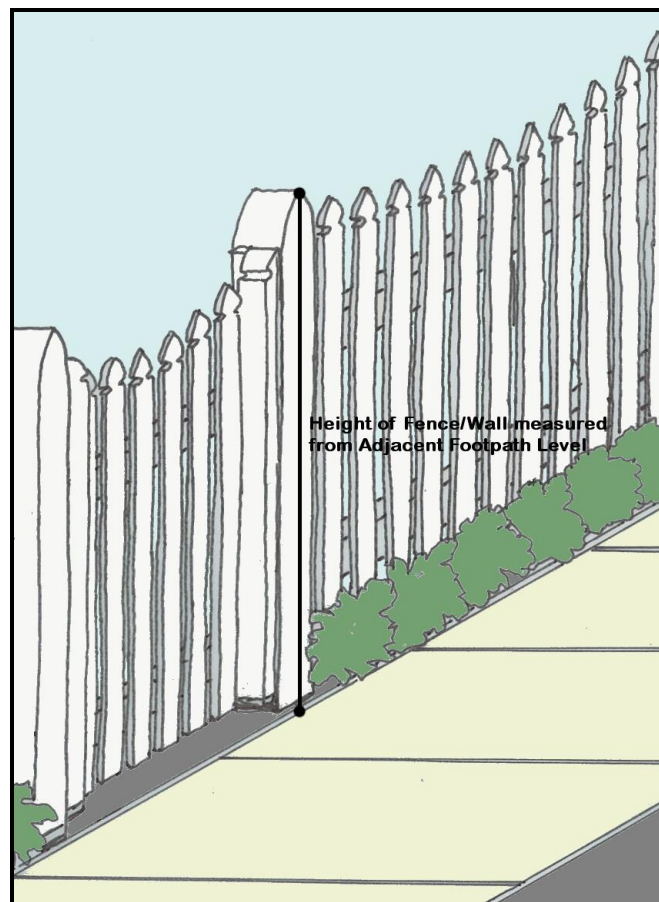


Figure 12: Measurement of Street Walls and Fences

Side and Rear Boundary Walls and Fences: The height is measured from the "certified" natural ground level immediately inside the boundary to the top of the fence.

“Certified natural ground level” refers to the levels on a site which precede the proposed development, excluding any site works unless approved by the City or established as part of subdivision of the land preceding development.

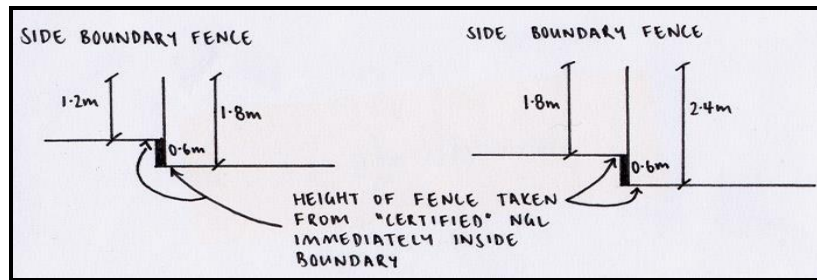


Figure 13: Measurement of Side and Rear Boundary Fence

**(iii) Dividing Walls and Fences**

Any dividing wall and fence between properties behind the street setback area is to comply with the City’s Fences Local Law.

**6.4.5 Car Parking and Vehicular Access**

**(i) On-Site Car Parking**

Adequate on-site car parking is to be provided for the development.

**(ii) Driveways**

The introduction of new driveways or widening of existing driveways fronting onto a street are discouraged, as they:

- Impact on the visual quality of the street;
- Impact on the safety, including proximity to obstructions such as street trees, signs, and the ability to enter and leave a site in forward gear; and
- Possibly reduce the number of verge side car parking spaces.

**6.4.6 Landscaping**

Landscaping is to increase tree coverage and diversity throughout the City through new plantings and maintenance of existing trees and vegetation. The planting of Australian native plant species is strongly encouraged to reduce water consumption.

The unnecessary removal or pruning of significant trees or other significant vegetation is not permitted.

Please refer to the City’s Policy relating to ‘Trees of Significance’.

**6.4.7 Minor Development**

Development which meets the standards defined in the City’s Policy relating to ‘Minor Nature Development’ is exempt from the need to obtain planning approval in accordance with Clause 33(d) of Town Planning Scheme No. 1.

**ELEMENT ONE – STREETSCAPE**

**ASSESSMENT TABLE**

Design Solution	Deemed-to-Comply Criteria	Guidance Notes Clause Reference
<b>Streetscape Character</b>	<b>Streetscape Character</b>	<b>6.4.1</b>
<p><b>SPC 1. Streetscape Character</b></p> <p>(i) The prominent characteristics of the streetscape is to be identified and considered as part of the design process.</p> <p>(ii) Development is to both preserve and enhance the visual character of the adjacent dwellings and streetscape by considering;</p> <ul style="list-style-type: none"> <li>• Architectural design elements;</li> <li>• Building bulk and scale;</li> <li>• Building setbacks;</li> <li>• Building design;</li> <li>• Relationship between the private and public domain;</li> <li>• Landscape themes; and</li> <li>• Fencing styles.</li> </ul> <p>(iii) High quality contemporary design is encouraged where considered appropriate within the context of the existing and emerging streetscape character.</p>	<p><b>SADC 1. Streetscape Character</b></p> <p>(a) Any development is to comply with the requirements specified in clause BDADC 4.</p> <p>(b) Elevations to the street are to be enhanced through horizontal and vertical articulation and elements such as balconies, verandahs, terraces, windows and visually permeable fencing.</p> <p>(c) Lot widths are to be consistent with the predominant lot width within the immediate street block on both sides of the street.</p> <p><i>Note: Streetscape refers to the overall aesthetic of the street when viewed from the street.</i></p> <p><i>Articulation is defined as architectural composition in which elements and parts of the building are expressed logically, distinctly, and consistently, with clear joints. For the purposes of this Policy articulation refers to points within a dwelling that clearly distinguish one part of the dwelling from another, such as setbacks between the ground and upper floors and indentations or ‘breaks’ within building walls.</i></p>	<p><b>6.4.1</b></p>

Design Solution	Deemed-to-Comply Criteria	Guidance Notes Clause Reference
<p><b>SPC 2. Alterations and Additions to Existing Dwelling</b></p> <p>(i) Development is to be architecturally harmonious with the streetscape.</p>	<p><b>SADC 2. Alterations and Additions to Existing Dwelling</b></p> <p>(a) Additions to existing dwellings should be:</p> <ul style="list-style-type: none"> <li>• Clearly distinguishable and complement the existing dwelling; and</li> <li>• Of contemporary and compatible design styles.</li> </ul> <p><i>Note: For major additions, applicants should consider utilising the services of an architect or architectural designer so as to optimise the design outcome.</i></p>	<p><b>6.4.1</b></p>
<p><b>SPC 3. New Dwellings</b></p> <p>(i) New dwellings are to respect the existing character of the locality and the streetscape.</p>	<p><b>SADC 3. New Dwellings</b></p> <p>(a) New dwellings are to comply with the requirements specified in clause BDADC 4.</p>	<p><b>6.4.1</b></p>
<p><b>SPC 4. Public Domain and Communal Spaces</b></p> <p>(i) Development is to make a positive contribution to the public domain and communal spaces.</p>	<p><b>SADC 4. Public Domain and Communal Spaces</b></p> <p>(a) Development is to make a positive contribution to the public domain and communal spaces by ensuring that:</p> <ul style="list-style-type: none"> <li>• Dwellings are orientated towards the primary street;</li> <li>• It is appropriately scaled and preserves the character of the streetscape; and</li> <li>• Landscaping does not compromise sight lines to communal spaces.</li> </ul>	<p><b>6.4.1</b></p>
<p><b>Building Setbacks</b></p>	<p><b>Building Setbacks</b></p>	<p><b>6.4.2</b></p>
<p><b>SPC 5. Street Setbacks</b></p> <p>(i) Development is to be appropriately located on site to:</p> <ul style="list-style-type: none"> <li>• Maintain streetscape character;</li> </ul>	<p><b>SADC 5. Street Setbacks</b></p> <p>(a) The primary street setback is to reflect the predominant streetscape pattern for the immediate locality which is defined as being the average setback of the 5 adjoining properties on each side of the development.</p>	<p><b>6.4.2 (i)</b></p>

Design Solution	Deemed-to-Comply Criteria	Guidance Notes Clause Reference						
<ul style="list-style-type: none"> <li>• Ensure the amenity of neighbouring properties is maintained;</li> <li>• Allow for the provision of landscaping and space for additional tree plantings to grow to maturity;</li> <li>• Facilitate solar access for the development site and adjoining properties;</li> <li>• Protect significant vegetation; and</li> <li>• Facilitate efficient use of the site.</li> </ul> <p>(ii) Variations to the Deemed-to-Comply Criteria relating to upper floor setbacks may be considered where it is demonstrated that the lesser upper floor setbacks incorporate appropriate articulation, including but not limited to; varying finishes and staggering of the upper floor walls to moderate the impact of the building on the existing or emerging streetscape and the lesser setback is integral to the contemporary design of the development.</p>	<p>(b) Where the predominant setback pattern requires a setback distance that exceeds or is less than the below required minimum, the greater or lesser setback required to maintain the character of the street will apply.</p> <p>(c) The upper floor setbacks are as follows:</p> <table border="1" data-bbox="628 674 1163 1146"> <thead> <tr> <th data-bbox="628 674 855 846">Upper Floor Feature Facing Primary Street</th> <th data-bbox="860 674 1163 846">Setback (metres)</th> </tr> </thead> <tbody> <tr> <td data-bbox="628 846 855 1012">Walls on Upper Floor</td> <td data-bbox="860 846 1163 1012">A minimum of two metres behind each portion of the ground floor setback.</td> </tr> <tr> <td data-bbox="628 1012 855 1146">Balconies on Upper Floor</td> <td data-bbox="860 1012 1163 1146">A minimum of 1 metre behind the ground floor setback.</td> </tr> </tbody> </table> <p><i>Note: The above setback requirements are a minimum requirement and an average of the above requirements is not acceptable.</i></p> <p><i>Streetscape Character ‘...refers to the aesthetic relationship that exists between built form, vegetation and topographic characteristics in both the private and public domains that distinguish one place from another. Streetscape character generally refers to the consistency of architectural style, building height, front/side setbacks, scale and bulk’.</i></p>	Upper Floor Feature Facing Primary Street	Setback (metres)	Walls on Upper Floor	A minimum of two metres behind each portion of the ground floor setback.	Balconies on Upper Floor	A minimum of 1 metre behind the ground floor setback.	
Upper Floor Feature Facing Primary Street	Setback (metres)							
Walls on Upper Floor	A minimum of two metres behind each portion of the ground floor setback.							
Balconies on Upper Floor	A minimum of 1 metre behind the ground floor setback.							

Design Solution	Deemed-to-Comply Criteria	Guidance Notes Clause Reference
<p><b>SPC 6. Minor Incursions Into Street Setback Area</b></p> <p>(i) Minor incursions and projections may be permitted where it will not detract from the character of the streetscape or dominate the appearance of the existing dwelling.</p>	<p><b>SADC 6. Minor Incursions Into Street Setback Area</b></p> <p>(a) A porch, verandah, chimney or the equivalent may project not more than one (1) metre into the street setback area, provided that the maximum total width of such projections do not exceed 20 percent of the lot frontage at any level.</p> <p>(b) Eaves are not to project more than one (1) metre into the street setback area for the full width of the building provided there is no pillar, support structure or the like under the eaves.</p>	<p>6.4.2 (ii)</p>
<p><b>SPC 7. Side Setbacks</b></p> <p>(i) Side setbacks are to:</p> <ul style="list-style-type: none"> <li>• Allow for significant landscaping between buildings, particularly for two storey structures to soften the visual appearance when viewed from the street and neighbouring properties;</li> <li>• Ensure adequate daylight, direct sun and ventilation for buildings;</li> <li>• Moderate the visual impact of building bulk and scale on neighbouring properties;</li> <li>• Assist with the protection of reasonable privacy between adjoining properties;</li> <li>• Complement the rhythm of the streetscape; and</li> </ul>	<p><del><b>SADC 7. Side Setbacks</b></del></p> <p><i>Note: This Clause was amended by the Council at its Ordinary Meeting held on 9 July 2013.</i></p>	<p>6.4.2 (iii)</p>

Design Solution	Deemed-to-Comply Criteria	Guidance Notes Clause Reference
<ul style="list-style-type: none"> <li>• Respect the setbacks of dwellings of heritage significance.</li> </ul>		
<p><b>SPC 8. Setback of Garages and Carports</b></p> <p>(i) Garages and carports are not to visually dominate the site or the streetscape.</p>	<p><b>SADC 8. Setback of Garages and Carports</b></p> <p>(a) Car parking, garages and carports are to be located at the rear of the property and accessed via a right of way where a right of way exists and the property has legal right of access to the right of way.</p> <p>(b) Notwithstanding the above, vehicular access to car parking, carports and garages for single houses may be from a street, regardless whether a right of way is available to the property, where;</p> <p>(1) the right of way is unsealed or not programmed to be sealed within the current, or subsequent, financial year in accordance with the City’s right of way upgrade program; or</p> <p>(2) more than 50 per cent of the dwellings in the immediate street block, on the same side of the street that the subject dwelling is located have carports or garages accessed from the primary street; or</p> <p>(3) the applicant demonstrates there is a mobility or access issue by using the right of way; or</p> <p>(4) the applicant demonstrates there would be a major impact on the existing amenity or open space at the rear of the property by using the right of way.</p> <p>(c) Where vehicular access to car parking, carports and garages are permitted to be from a street (primary or secondary), the following requirements are to be met:</p>	<p><b>6.4.2 (iv)</b></p>



Design Solution	Deemed-to-Comply Criteria	Guidance Notes Clause Reference
	<ul style="list-style-type: none"> <li>(1) Garages and carports should be integrated into the development, and should be constructed of compatible materials, colours, scale and roof pitch to the dwellings on site;</li> <li>(2) Garages are to be setback a minimum of 500 millimetres behind line of the front main building line of the dwelling (not open verandah, porch, portico and the like);</li> <li>(3) Carports should be located behind the street setback line and at the side of the dwelling where space exists;</li> <li>(4) Carports may be located within the street setback area provided it is one hundred (100) per cent open on all sides at all times (open style gates/panels with a minimum visual permeability of eighty (80) per cent are permitted), except where it may abut the front main building wall of the dwelling (not open verandah, porch, portico and the like);</li> <li>(5) Solid roller doors, tilt doors and the like are not permitted for any carports located within the street setback area; and</li> <li>(6) The total width of any carport (inclusive of support structures) within the street setback area is not to exceed 50 per cent of the lot frontage at the building line or primary street setback, whichever is the greater.</li> </ul>	
<p><b>SPC 9. Setbacks from Rights of Way</b></p> <ul style="list-style-type: none"> <li>(i) The setback is to be compatible and consistent with the established pattern of setbacks presenting to the right of way.</li> <li>(ii) The minimum width of a right of way is to be</li> </ul>	<p><b>SADC 9. Setbacks from Rights of Way</b></p> <ul style="list-style-type: none"> <li>(a) The minimum width of a right of way is to be six (6) metres to enable adequate manoeuvring distance for vehicles.</li> <li>(b) Dwellings fronting a right of way are required to be setback as follows:</li> </ul>	<p><b>6.4.2 (v)</b></p>

Design Solution	Deemed-to-Comply Criteria		Guidance Notes Clause Reference												
<p>6 metres, in accordance with the Western Australian Planning Commission’s Policy DC 2.6 – ‘Residential Road Planning’. However, there are a number of rights of way within the City that are less than 6 metres wide. Where this is the case, the minimum manoeuvring distance of 6 metres still needs to be met.</p>	<table border="1"> <thead> <tr> <th data-bbox="619 367 858 443">Feature facing Right of Way</th> <th data-bbox="863 367 1161 443">Minimum Setback (metres)</th> </tr> </thead> <tbody> <tr> <td data-bbox="619 443 858 607">Porches, Verandahs, Porticos, and the Like</td> <td data-bbox="863 443 1161 607">1.5</td> </tr> <tr> <td data-bbox="619 607 858 741">Building Walls on Ground Floor</td> <td data-bbox="863 607 1161 741">2.0</td> </tr> <tr> <td data-bbox="619 741 858 842">Balconies on Upper Floor</td> <td data-bbox="863 741 1161 842">2.5</td> </tr> <tr> <td data-bbox="619 842 858 1010">Building Walls on Upper Floors</td> <td data-bbox="863 842 1161 1010">1 metre behind each portion of the ground floor setback.</td> </tr> <tr> <td data-bbox="619 1010 858 1205">Carports and Garages</td> <td data-bbox="863 1010 1161 1205">6 metres manoeuvring distance located directly in front of carport and garage.</td> </tr> </tbody> </table>		Feature facing Right of Way	Minimum Setback (metres)	Porches, Verandahs, Porticos, and the Like	1.5	Building Walls on Ground Floor	2.0	Balconies on Upper Floor	2.5	Building Walls on Upper Floors	1 metre behind each portion of the ground floor setback.	Carports and Garages	6 metres manoeuvring distance located directly in front of carport and garage.	
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Carports and Garages	6 metres manoeuvring distance located directly in front of carport and garage.														
<p><i>Note:</i>                  (Setback to right of way is measured from the original property boundary provided that the area required to widen the right of way is free of any kind of development (refer to Guidance Notes).”</p> <p>(c) Garages and carports are to have adequate sight lines to rights of way as per Australian Standards requirements, AS 2890.1.</p> <p>(d) Development is to comply with the Western Australian Planning Commission’s Planning Bulletin No. 33 dated July 1999 relating to Rights-of-Way in Established Areas – Guidelines.</p>															

Design Solution	Deemed-to-Comply Criteria	Guidance Notes Clause Reference																						
<p><b>SPC 10. Dual Street Frontages and Corner Sites</b></p> <p>(i) Dwellings on dual street frontages or corner lots are to present an attractive and interactive elevation to each street frontage. This may be achieved by utilising the following design elements:</p> <ul style="list-style-type: none"> <li>• Wrap around design (design that interacts with all street frontages);</li> <li>• Landscaping;</li> <li>• Feature windows;</li> <li>• Staggering of height and setbacks;</li> <li>• External wall surface treatments and finishes; and</li> <li>• Building articulation.</li> </ul>	<p><b>SADC 10. Dual Street Frontages and Corner Sites</b></p> <p>(a) For new development at the rear of original corner sites or dual frontage sites, the following minimum street setbacks are to apply:                      (1) As measured from the original secondary street</p> <table border="1" data-bbox="639 689 1157 1368"> <thead> <tr> <th>Feature facing Secondary Street</th> <th>Minimum Setback (metres)</th> </tr> </thead> <tbody> <tr> <td>Porches, Verandahs, Porticos and the Like</td> <td>1.5</td> </tr> <tr> <td>Building Walls on Ground Floor</td> <td>2.5</td> </tr> <tr> <td>Balconies on Upper Floor</td> <td>3.0</td> </tr> <tr> <td>Building Walls on Upper Floor</td> <td>1.5 metres behind each portion of the ground floor setback.</td> </tr> <tr> <td>Carports and Garages</td> <td>In accordance with clause SADC 8</td> </tr> </tbody> </table> <p>(b) For new development on the front of the original corner site, the following minimum street setbacks are to apply:                      (1) As measured from the primary street:</p> <table border="1" data-bbox="639 1518 1157 2063"> <thead> <tr> <th>Feature facing Primary Street</th> <th>Minimum Setback (metres)</th> </tr> </thead> <tbody> <tr> <td>Building Walls on Ground Floor</td> <td>In accordance with clause SADC 5</td> </tr> <tr> <td>Building Walls on Upper Floor</td> <td>In accordance with clause SADC 5</td> </tr> <tr> <td>Balconies on Upper Floor</td> <td>In accordance with clause SADC 5</td> </tr> <tr> <td>Carports and Garages</td> <td>In accordance with clause SADC 8</td> </tr> </tbody> </table>	Feature facing Secondary Street	Minimum Setback (metres)	Porches, Verandahs, Porticos and the Like	1.5	Building Walls on Ground Floor	2.5	Balconies on Upper Floor	3.0	Building Walls on Upper Floor	1.5 metres behind each portion of the ground floor setback.	Carports and Garages	In accordance with clause SADC 8	Feature facing Primary Street	Minimum Setback (metres)	Building Walls on Ground Floor	In accordance with clause SADC 5	Building Walls on Upper Floor	In accordance with clause SADC 5	Balconies on Upper Floor	In accordance with clause SADC 5	Carports and Garages	In accordance with clause SADC 8	<p>6.4.2 (vi)</p>
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Design Solution	Deemed-to-Comply Criteria	Guidance Notes Clause Reference						
	<p>(2) As measured from the secondary street:</p> <table border="1" data-bbox="641 456 1155 931"> <thead> <tr> <th data-bbox="641 456 874 555">Feature facing Secondary Street</th> <th data-bbox="874 456 1155 555">Minimum Setback (metres)</th> </tr> </thead> <tbody> <tr> <td data-bbox="641 555 874 763">Building Walls on Ground Floor, including Porches, Verandahs and the Like</td> <td data-bbox="874 555 1155 763">1.5</td> </tr> <tr> <td data-bbox="641 763 874 931">Building Walls on Upper Floor; including Balconies and the Like</td> <td data-bbox="874 763 1155 931">0.5 metre behind each portion of the ground floor setback.</td> </tr> </tbody> </table> <p><i>Note:</i>                      Upper floor walls (including balconies and the like) fronting the secondary street are to be vertically staggered and articulated by a minimum of 0.5 metre from the ground floor. Extensive blank or unarticulated walls to street frontages will not be supported.</p>	Feature facing Secondary Street	Minimum Setback (metres)	Building Walls on Ground Floor, including Porches, Verandahs and the Like	1.5	Building Walls on Upper Floor; including Balconies and the Like	0.5 metre behind each portion of the ground floor setback.	
Feature facing Secondary Street	Minimum Setback (metres)							
Building Walls on Ground Floor, including Porches, Verandahs and the Like	1.5							
Building Walls on Upper Floor; including Balconies and the Like	0.5 metre behind each portion of the ground floor setback.							
<p><b>SPC 11. Buildings on Boundary</b></p> <p>(i) Boundary walls are not to have an undue impact on the affected neighbour and the amenity of the streetscape.</p>	<p><b>SADC 11. Buildings on Boundary</b></p> <p>(a) The term ‘up to a boundary’ means either on the boundary or any point closer than 0.75 metres between the boundary and the setback provided by table 1, tables 2a and 2b, figures 2a – 2e, and figure 3 of the Residential Design Codes.</p> <p>(b) Walls built up to a boundary behind the front setback line are to be in accordance with clause 6.3.2 A2 of the Residential Design Codes.</p>	<p><b>6.4.2 (vii)</b></p>						

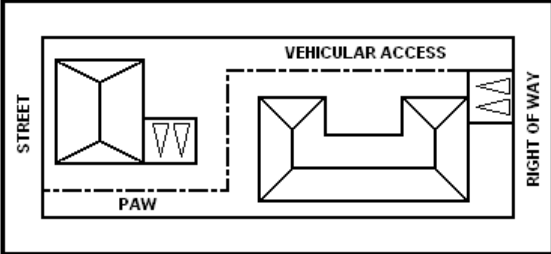
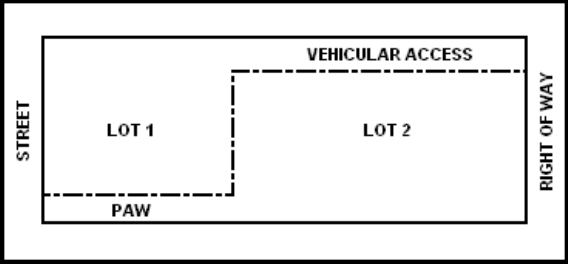
Design Solution	Deemed-to-Comply Criteria	Guidance Notes Clause Reference
<b>Safety and Security</b>	<b>Safety and Security</b>	<b>6.4.3</b>
<p><b>SPC 12. Safety and Security</b></p> <p>(i) Development to be designed to enhance the safety and security of the surrounding area.</p>	<p><b>SADC 12. Safety and Security</b></p> <p>(a) Surveillance</p> <p>The design of a development is to facilitate passive surveillance by:</p> <ul style="list-style-type: none"> <li>• Ensuring clear sight lines to the public realm from adjacent buildings;</li> <li>• Lighting primary pedestrian routes into and out of a property/dwelling;</li> <li>• Ensuring that site level changes on a property do not result in the obscuring of public places;</li> <li>• Street walls and fences being visually permeable; and</li> <li>• At least one major opening window to a habitable room facing the street and right of way (where practical), on the ground and upper floors.</li> </ul> <p>(b) Access Control and Target Hardening</p> <p>The design of a development is to facilitate access control by:</p> <ul style="list-style-type: none"> <li>• Providing secure access against offenders with gates and such defining structures; and</li> <li>• Integrating security screens and bars to windows as design elements and not as afterthoughts</li> </ul>	<p><b>6.4.3</b></p> <p><b>6.4.3 (i)</b></p> <p><b>6.4.3 (ii) &amp; (iii)</b></p>
<b>Street Walls and Fences</b>	<b>Street Walls and Fences</b>	<b>6.4.4</b>
<p><b>SPC 13. Street Walls and Fences</b></p> <p>(i) Street walls and fences are to be designed so that:</p> <ul style="list-style-type: none"> <li>• Buildings, especially their entrances, are clearly visible from the primary street;</li> <li>• A clear line of</li> </ul>	<p><b>SADC 13. Street Walls and Fences</b></p> <p>(a) Street walls and fences are to be of a style and materials compatible with those of the dwelling on site and/or walls or fences of the immediate surrounding area.</p> <p>(1) Street walls and fences <del>designed with</del> <u>constructed from</u> fibre cement or <del>metal sheeting</del> are not acceptable.</p> <p>(2) <u>Street walls and fences on the side</u></p>	<p><b>6.4.4 (i), (ii) &amp; (iii)</b></p>

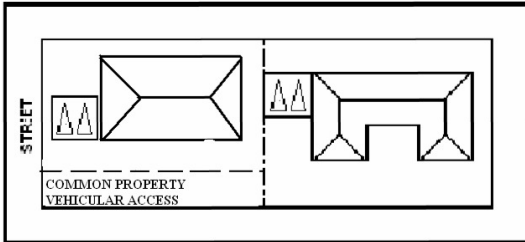
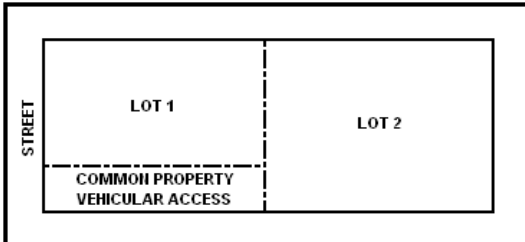
Design Solution	Deemed-to-Comply Criteria	Guidance Notes Clause Reference
<p>demarcation is provided between the street and development;</p> <ul style="list-style-type: none"> <li>• They are in keeping with the desired streetscape; and</li> <li>• Provide adequate sightlines at vehicle access points.</li> </ul>	<p><u>boundaries only within the primary street setback area constructed from metal sheeting are permitted provided they meet all other requirements relating to height and provide adequate sight lines.</u></p> <p>(43) Street walls and fences within the primary street setback area, including along the side boundaries, and front walls and fences to new infill dwellings fronting a right of way or dedicated road to be as follows:</p> <ul style="list-style-type: none"> <li>• Maximum height of 1.8 metres above adjacent footpath level; and</li> <li>• Maximum height of piers with decorative capping to be 2 metres above adjacent footpath level;</li> <li>• Maximum height of solid portion of wall to be 1.2 metres above adjacent footpath level and a minimum of fifty percent visually permeable above 1.2 metres; and</li> <li>• Posts and piers are to have a maximum width 355 millimetres and a maximum diameter of 500 millimetres; and</li> <li>• The distance between piers should not be less than the height of the piers except where pedestrian gates are proposed.</li> </ul> <p>(24) Street walls and fences to secondary streets, behind the primary street setback line, or walls and fences to the primary streets where those streets are district distributor roads are to comply with the following:</p> <ul style="list-style-type: none"> <li>• Solid portion of wall may increase to a maximum height of 1.8 metres above adjacent footpath level provided that the wall or fence has at least two (2) significant appropriate design features (as determined by the City of Vincent) to reduce the visual impact – for example,</li> </ul>	

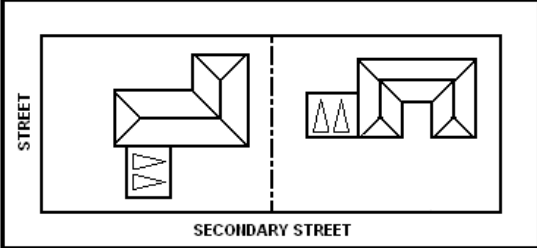
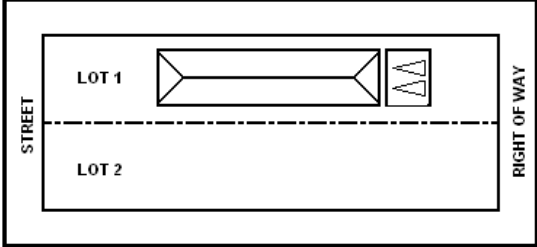
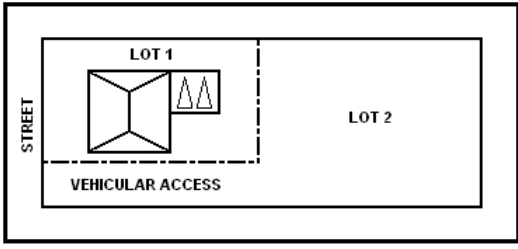
Design Solution	Deemed-to-Comply Criteria	Guidance Notes Clause Reference
	<p>significant open structures, recesses and/or planters facing the road at regular intervals and varying materials, finishes and/or colours; and</p> <ul style="list-style-type: none"> <li>Maximum height of piers with decorative capping to be 2 metres above adjacent footpath level.</li> </ul> <p><i>Note:</i> Please refer to Guidance Notes clause 6.4.4 (i) for instances where the City may consider a variation to the above requirements.</p> <p>(35) Street walls and fences to incorporate visual truncations that comply with the City’s Policy relating to Truncations.</p>	
<b>Car Parking and Vehicular Access</b>	<b>Car Parking and Vehicular Access</b>	<b>6.4.5</b>
<p><b>SPC 14. On-site Car Parking</b></p> <p>(i) On-site car parking is to be provided to adequately meet the needs of residents.</p> <p>(ii) The provision of more than 2 car parking spaces per dwelling is discouraged in locations where there is availability of public transport.</p>	<p><del><b>SADC 14. On-site Car Parking</b></del></p> <p><i>Note: This Clause was amended by the Council at its Ordinary Meeting held on 9 July 2013.</i></p>	<p><b>6.4.5</b></p> <p><b>6.4.5 (i)</b></p>
<p><del><b>SPC 15. Driveways and Crossovers</b></del></p>	<p><del><b>SADC 15. Driveways and Crossovers</b></del></p> <p><i>Note: This Clause was amended by the Council at its Ordinary Meeting held on 9 July 2013.</i></p>	<p><b>6.4.5 (ii)</b></p>

Design Solution	Deemed-to-Comply Criteria	Guidance Notes Clause Reference
<p><b>SPC 16. Landscaping</b></p> <p>(i) All residential development requires the provision of landscaping of a high standard.</p>	<p><b>SADC 16. Landscaping</b></p> <p>(a) Landscaping should enhance the character of the development by softening the built form.</p> <p style="padding-left: 40px;">This can be achieved by:</p> <ul style="list-style-type: none"> <li>• Planting small to medium canopy trees;</li> <li>• Minimising excavation (cut) and fill;</li> <li>• Planting native plant species;</li> <li>• Planting species to conserve biodiversity;</li> <li>• Planting vegetation of differing heights; and</li> <li>• Weed management.</li> </ul> <p>(b) A landscaping plan is to be submitted in the following circumstances:</p> <ul style="list-style-type: none"> <li>• Grouped dwelling development applications;</li> <li>• Multiple dwelling applications; or</li> <li>• Any development application where the City deems a landscape plan is necessary.</li> </ul>	<p><b>6.4.6</b></p>
<p><b>SPC 17. Lot Configuration and Subdivision Pattern</b></p> <p>(i) The lot configuration and subdivision pattern does not have an undue impact on the streetscape and surrounding amenity.</p> <p>(ii) The lot configuration and subdivision pattern enable future development to be developed in an energy efficient and environmentally sustainable way.</p>	<p><b>SADC 17. Lot Configuration and Subdivision Pattern</b></p> <p>(a) The lot configuration and subdivision pattern are to reflect the existing predominant subdivision pattern of the immediate street block the subject property is located, in terms of layout and orientation.</p> <p>(b) The lot configuration and subdivision pattern are to provide opportunities for effective use of the northern portion of the lot, without compromising the northerly aspect of adjoining lots. Street and lot orientation and lot dimension should facilitate the siting and design of dwellings that can minimise non-renewable energy use; therefore lots are to be orientated east-west and/or north-south, where feasible.</p>	



Design Solution	Deemed-to-Comply Criteria	Guidance Notes Clause Reference
<p><i>Note: This Clause was amended by the Council at its Ordinary Meeting held on 9 July 2013</i></p>	<p>(c) Possible lot configurations that may be supported by the City of Vincent are as follows:</p>  <p>(1) The retention of an existing dwelling(s) with frontage to a primary street and a right of way.</p> <p>(aa) Vehicular access for the proposed rear lot(s)/dwelling(s) is to be from the right of way only.</p> <p>(bb) Vehicular access for the existing dwelling(s) lot is to be from the right of way.</p> <p>(cc) Vehicular access to the existing dwelling(s) lot may be from the primary street in the following situations only.</p> <ul style="list-style-type: none"> <li>• There is an existing crossover/vehicular access from the primary street to the existing dwelling(s) lot; and</li> <li>• The applicant demonstrates that more than 50 per cent of lots within the immediate street block the subject property is located has crossovers/vehicular access to the primary street.</li> </ul> <p>(2) Vacant site with frontage to a primary street and right of way.</p> 	

Design Solution	Deemed-to-Comply Criteria	Guidance Notes Clause Reference
	<p>(aa) Vehicular access to the proposed lots is to be provided from the right of way only.</p> <p>(3) The retention of an existing dwelling(s) with frontage only to a primary street.</p>  <p>(aa) Vehicular access to existing dwelling(s) lot and proposed rear lot(s)/dwelling(s) is to be shared as common property.</p> <p><i>Note: Common Property</i></p> <p><i>In survey strata and strata subdivisions common property is to be functional and for the use of all proposed lots, such as a driveway that provides vehicular access for all proposed lots.</i></p> <p>(4) Vacant site with frontage only to a primary street.</p>  <p>(aa) Vehicular access to the proposed lot(s)/dwelling(s) is to be shared as common property, and a maximum of one crossover is permitted per subdivision.</p> <p>(5) The subdivision of a corner site where each new lot has frontage to a dedicated road.</p>	

Design Solution	Deemed-to-Comply Criteria	Guidance Notes Clause Reference
	 <p>(6) The retention of an existing dwelling(s) or vacant site where the block is split down the middle.</p>  <p>(aa) Subdivision of an existing lot down the middle will only be considered where it will result in new lot widths consistent with more than 50 per cent of lots within the immediate street block.</p> <p>(7) Green Title/Freehold Battleaxe Lot.</p>  <p>(aa) Each lot is to have direct frontage to a dedicated road.</p> <p>(bb) Vehicular access is to comply with clause ADC 5. Vehicular Access.</p> <p>(cc) Green Title/freehold battleaxe subdivision will only be supported where the following criteria are met:</p> <p>(aaa) an existing dwelling(s) fronts the street and</p>	

Design Solution	Deemed-to-Comply Criteria	Guidance Notes Clause Reference
	<p>this dwelling is retained;</p> <p>(bbb) the existing vehicular access to the existing dwelling(s) is not sufficient in terms of width and manoeuvring to service both the existing dwelling(s) lot and proposed lot(s)/dwelling(s);</p> <p>(ccc) the property does not have legal access to a right of way; and</p> <p>(ddd) the subdivision complies with the requirements of the R Codes.</p> <p><i>Note:</i></p> <p><i>“Green title/freehold battleaxe lot/site” means a green title/freehold single house lot or site that has a frontage to a public road only through a vehicular access way that is part of the lot, but the term excludes a lot that has vehicular access from a private or communal street or right of way connected to a public road.”</i></p> <p><i>Note: This Clause was amended by the Council at its Ordinary Meeting held on 9 July 2013</i></p>	

Design Solution	Deemed-to-Comply Criteria	Guidance Notes Clause Reference
<p><b>SPC 18. Small Lot Subdivision</b></p> <p>(i) Small lots are capable of accommodating development that provides adequate amenities to future occupants of that development without having an undue impact on the streetscape and surrounding amenity.</p> <p><i>Note: This Clause was amended by the Council at its Ordinary Meeting held on 9 July 2013</i></p>	<p><b>SADC 18. Small Lot Subdivision</b></p> <p>(a) A lot with a width/frontage less than 8 metres may be supported where the following criteria are achieved:</p> <ul style="list-style-type: none"> <li>• The property has vehicular access from a right of way; and</li> <li>• Similar development/subdivision has occurred along the street within the subject street block; or</li> <li>• The proposal involves the re-adaptation of the existing dwelling into two dwellings.</li> </ul> <p>In all cases compliance with all other provisions of the City of Vincent Policy relating to Residential Design Elements and the R Codes is required.</p> <p>(b) Lots with a width/frontage less than 8 metres and/or lots with an area less than 300 square metres and/or lots with an encumbrance (such as a rights of carriageway easement) that reduces the effective area of the lot to less than 300 square metres, and/or lots with an awkward shape, are to meet the following criteria to the satisfaction of the City of Vincent:</p> <p>(aa) the City of Vincent has approved a Planning Approval and/or Building Licence for the development of dwelling(s) on the subject lot; and</p> <p>(bb) the perimeter walls of the approved dwelling(s), including the garage walls and carport walls/pillars have been constructed to plate height.</p> <p><i>Notes:</i></p> <p>(a)“wall plate” means a horizontal structure on a wall to distribute the pressure from the roof frame structure;</p> <p>(b)“plate height” means the height of the wall between the natural ground</p>	

Design Solution	Deemed-to-Comply Criteria	Guidance Notes Clause Reference
	<p><i>level and the wall plate; and</i></p> <p><i>(c) The above Planning Approval and construction to plate height requirements provide the following benefits to the City of Vincent and the applicant of the proposed subdivision:</i></p> <ul style="list-style-type: none"> <li><i>• Demonstrates that a dwelling with adequate amenities on site, while not unduly affecting the amenity of the surrounding area, can be developed on the small lots;</i></li> <li><i>• Prevents Planning Approval lapsing after two (2) years or the owner changing over time, and the applicant and the City of Vincent having to re-negotiate an appropriate new development on the small lots;</i></li> <li><i>• Prevents newly subdivided lots remaining vacant; and</i></li> <li><i>• Provided certainty of the scale and nature of development on the small lots.</i></li> </ul> <p><i>Note: This Clause was amended by the Council at its Ordinary Meeting held on 9 July 2013</i></p>	

Design Solution	Deemed-to-Comply Criteria	Guidance Notes Clause Reference
<p><b>SPC 19. Pedestrian Access</b></p> <p>(i) All lots are to be provided with pedestrian access to a dedicated road.</p> <p><i>Note: This Clause was amended by the Council at its Ordinary Meeting held on 9 July 2013</i></p>	<p><b>SADC 19. Pedestrian Access</b></p> <p>(a) Each lot that does not have direct frontage to a dedicated road is to be provided with a pedestrian access way (PAW) to a dedicated road.</p> <p>(b) The minimum width of the pedestrian access way is to be 1.5 metres.</p> <p>(c) The minimum width of the pedestrian access way may be reduced to 1 metre where it is directly adjacent to existing buildings worthy of retention. The minimum width of the remaining pedestrian access way is to 1.5 metres.</p> <p><i>Note:</i></p> <p><i>Where a subdivision is located adjacent to a right of way, a PAW to the gazetted road is required on the original lot and no portion of the right of way (including area required for any future right of way widening) is to form part of the PAW.</i></p> <p><i>Note: This Clause was amended by the Council at its Ordinary Meeting held on 9 July 2013</i></p>	

## **7.0 BUILDING DESIGN**

### **7.1 Overview**

Building design has a major impact on the physical and visual amenity of a place. It can reinforce an area's existing character or relate to an area's desired character. Major building design elements such as height and scale are critical in addressing impacts from development such as overshadowing, overlooking and view loss.

In recent times, it has become imperative for building design to incorporate energy efficient elements to create more sustainable development and minimise the impact of residential development on the environment.

### **7.2 Vincent Vision 2024**

The *Vincent Vision 2024* project indicates the community values the following relating to building design:

- *Mix in housing stock and density;*
- *Blend of old and new housing;*
- *Blend of character and contemporary; and*
- *Sustainable design.*

The building design issues identified by the community are as follows:

- *Poor design and impacts on the streetscape;*
- *Lack of environmental design principles/Energy inefficient design and buildings;*
- *Declining housing affordability and diversity; and*
- *Impact of new developments and poor design.*

The values and issues identified by the community form the following guiding principles relating to building design. These principles are implemented into future development through the Guidance Notes and Assessment Table in this Policy:

- *Traditional housing is preserved, while contemporary housing blends in and contributes to the unique atmosphere and character of the area;*
- *New housing development is of exemplary design, high amenity and quality and does not negatively impact on surrounding residents or the neighbourhood; and*
- *Excellence in housing design embraces contemporary architecture, energy efficiency and a diversity of style.*

### **7.3 OBJECTIVES**

#### **Streetscape and Surrounding Amenity**

- To ensure that development responds to the existing scale and character of the street;
- To distribute building height and bulk appropriately on the development site to protect the amenity of adjoining lots and maintain the visual integrity of the streetscape;
- To allow adequate solar access and ventilation to penetrate habitable rooms and outdoor area for both the development site and adjoining properties;



- To encourage well designed and attractive buildings that contribute and interact responsively to the site and its surrounding streetscape; and
- To prevent inappropriate overdevelopment of sites.

### **Privacy**

- To ensure an acceptable level of visual and acoustic privacy is achieved for development sites and abutting lots.

### **Energy Efficient Design**

- To maximise the use of renewable energy sources (such as sunlight);
- To promote and create development that use less energy, cost less to run and are more climatically comfortable;
- To contribute positively towards the overall reduction in greenhouse gas emissions; and
- To ensure that design for good environmental performance and amenity is considered in conjunction with other design and amenity considerations in the City of Vincent's context.

## **7.4 Guidance Notes**

*Applicants are encouraged to use the services of a qualified architect or architectural designer to prepare the development plans for any new dwellings or proposed alterations and additions to an existing dwelling.*

### **7.4.1 Preservation of Amenity on Adjoining Land and Surrounding Area**

Any new development, including alterations and additions, is to consider preserving the amenity of adjoining neighbours and the surrounding area. Such impacts include, but are not limited to, overlooking, overshadowing, loss of views and building design in relation to the existing streetscape and rhythm.

Where considered appropriate, the City may require a development application to be submitted with an accompanying Amenity Impact Statement which:

- demonstrates consideration has been given to the impact on the amenity of adjacent properties; and
- outlines any measures that have been taken to mitigate any likely undue impacts on the amenity of adjacent properties.

An Amenity Impact Statement may be required where a variation to the Deemed-to-Comply Criteria of the Policy is proposed. An Amenity Impact Statement will explain how a proposed development will respond to the Objectives and Design Solution of the Policy, and put forward planning justification as to why a variation should be considered.

An Amenity Impact Statement is to demonstrate that the proposed variation will not have an undue impact on the amenity of the adjoining properties and the surrounding area. An Amenity Impact Statement may be supported by materials such as photographs, photograph montages, sketches and architectural models.

Advice and assistance in developing an Amenity Impact Statement can be sought from the City's Planning, Building and Heritage Services section.

**7.4.2 Streetscape Character Considerations**

Housing styles, setbacks and general appearance of dwellings within a street, are some of the many features of a streetscape that influences its character. The City and its community places great value on the varied and unique streetscape character that can be found within the City, to such an extent that certain streetscapes within the City will be identified and specifically addressed.

New dwellings and developments are to be compatible in bulk and scale with adjoining properties and established streetscapes. Any departure from an established streetscape pattern can significantly alter the character and significance of the streetscape and its community value and will not be supported.

Applicants must make reference to any Residential Streetscapes Policy, as part of the design process in preparing a development application as well as document and consider the existing pattern of development.

**7.4.3 Roof Forms**

Roofs play a significant role in the urban character of a streetscape. The City encourages traditional roof forms for infill development in the older residential areas of the City or where there is a strong representation of similar housing styles and roof forms.

Hipped or gabled roof forms are the preferred roof form for any portions of buildings which contribute to the immediate streetscape, unless it can be demonstrated that the roof form proposed can be comfortably accommodated within the streetscape and not detract from the established streetscape.



*Figure 14: Examples of Preferred Roof Forms*

The City recognises that in some residential areas there may be opportunity for more innovative design and architectural styles, and in these instances, may consider alternate roof forms. These will be assessed on their merit, at the time of application.

Roofs with a steeper pitch than 45 degrees will be treated as an external wall.

### 7.4.4 Building Bulk

Where the streetscape character is predominantly single storey, any upper storey additions or two storey developments are to reinforce this streetscape pattern, by ensuring that the overall bulk and scale of the development as viewed directly from street level is reduced and does not have an undue impact on the streetscape, to the satisfaction of the City.



Figure 15: Additions Demonstrating Consideration of Single Storey Streetscape

### 7.4.5 Building Height

#### (i) Building Height Measures

Building height contributes to the bulk and scale of dwellings and therefore the streetscape and the amenities of neighbouring properties.

Building wall height refers to the vertical height of a wall measured from natural ground level to above the eaves.

Roof pitch height refers to the vertical height measured from natural ground level to the highest point of the roof pitch or apex.

#### (ii) Measuring Building and Roof Height

The measurement of building height for the purpose of residential development within the City of Vincent is as follows:

- Building height will be measured from the natural ground level immediately below the relevant point on the wall or roof to above the eaves/roof or highest point of the roof respectively;
- On sloping sites, for the purpose of calculating building setbacks only, the average wall height will be considered the wall height;
- “Natural ground level” refers to the levels on a site which precede the proposed development, excluding any site works unless approved by the Council or established as part of subdivision of the land preceding development; and.
- Minor projections such as chimneys, TV aerials, satellite dishes, vent pipes and air conditioners are exempted from building height, however are addressed as incidental development in the R Codes.

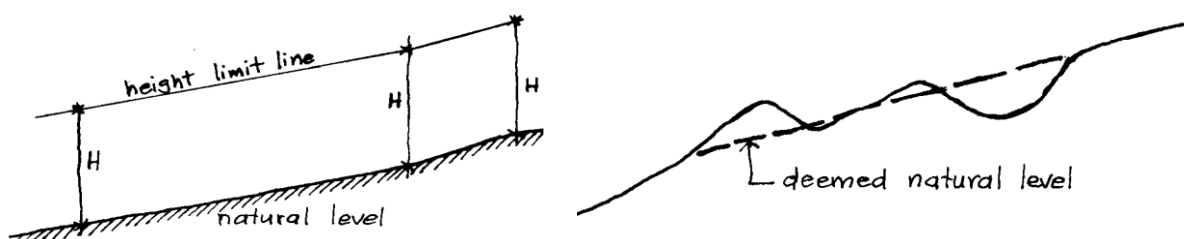


Figure 16: Irregular Terrain to be Interpreted in a Commonsense Way for the Purpose of Determining Natural Ground Level.

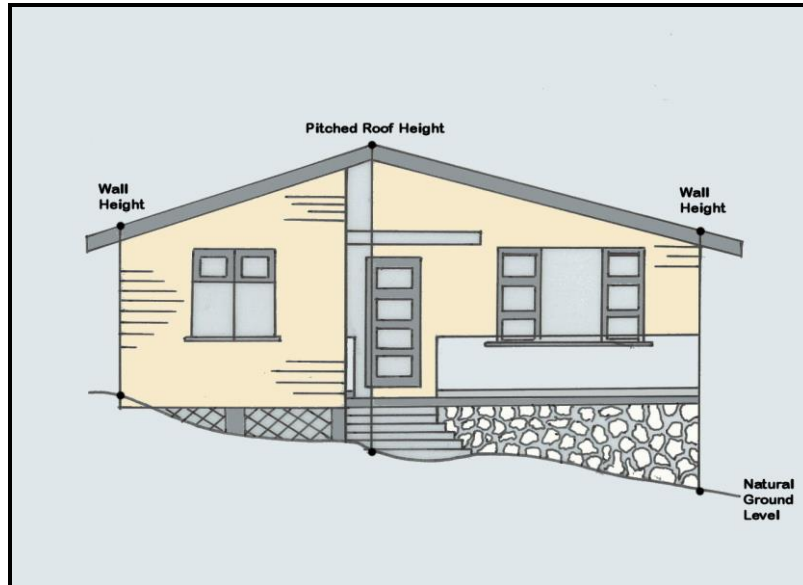


Figure 17: The Measurement of Wall and Roof Heights for Pitched Roof Development.

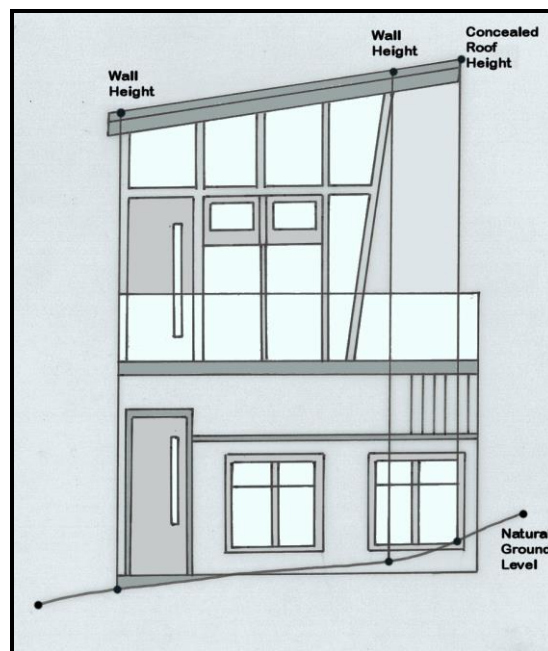


Figure 18: The Measurement of Wall and Roof Heights for Concealed Roof Development.

Three storey dwellings are generally not compatible in scale and dominate existing development in the City, therefore are generally not supported.

Variations to the maximum building wall and roof heights may be considered due to topographical or other environmental considerations and physical constraints of retaining an existing dwelling. Provided the streetscape and amenity of the affected adjacent properties is protected, variations to building height may be considered in the following exceptional circumstances:

- The natural ground level of the site is sloping, provided that a compliant two storey height presence is maintained when viewed from the street;

- A second storey addition to an existing dwelling has a maximum floor to ceiling height of 2.4 metres, and this height can be slightly increased to proportionally balance the ground and upper floor when viewed from the street; and
- Where more than 50 per cent of the existing dwellings on the immediate street block on the same side of the street which the proposed dwelling/addition is located, are higher than the building external wall and/or pitched roof height requirements specified in Building Height Deemed-to-Comply Criteria in the Assessment Table. In such situations, the dwelling wall and pitched roof heights may be increased to be consistent with, but no greater than the heights of the majority of dwellings located on the immediate street block on the same side of the street.

Development that propose a variation to the building height requirements must comply with the following:

- The upper floor being setback from the primary street property boundary such distance to adequately protect the streetscape and surrounding amenity;
- The upper floor being articulated and setback from the side property boundaries to minimise impact on the affected neighbours (refer to clause 6.4.2 (iii)); and
- Compliance with the overshadowing and privacy requirements of the R Codes.

#### 7.4.6 Lofts

A loft is the space contained between the roof pitch and the top of the ceiling of the storey immediately below it.



Figure 19: Loft Contained Within the Roof Space

A loft addition must not represent or resemble an additional storey but must be wholly contained within the roof space of the proposed or existing dwelling. It does not constitute as an additional storey if wholly contained within the roof space.

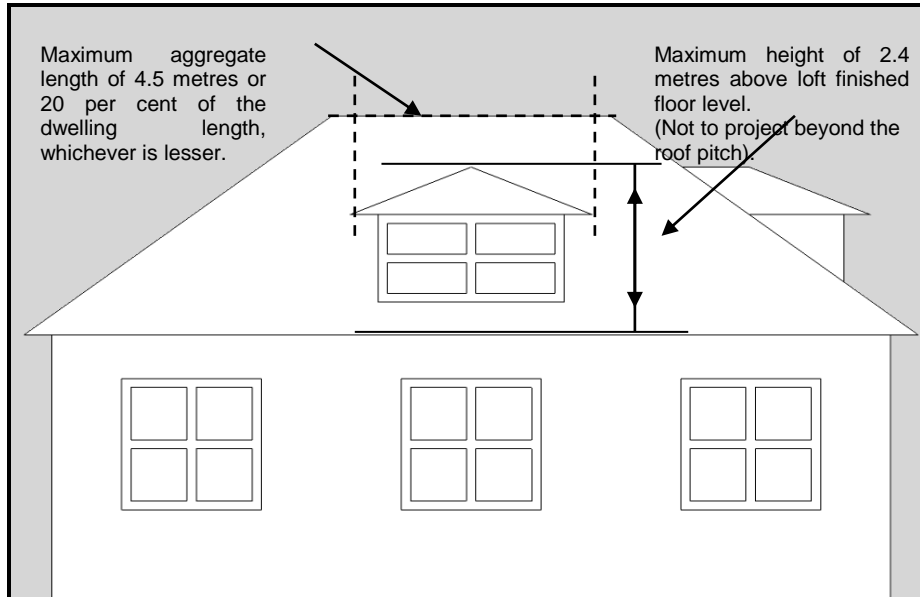


Figure 20: Dormer Windows and Gables to Lofts

#### 7.4.7 Site Levels

Variations in topography make a significant contribution to the local character of an area. In many locations within the City of Vincent, the topography affords views out of the locality. These views are highly valued by residents and can be shared if the natural topography is respected during the development process.

Excavation below natural level is not usually as visually obtrusive as filling above natural level and as such 'cutting' behind the street setback line is usually acceptable.

Whereas, filling above natural ground level, where it results in replacing a natural slope with level ground and retaining walls, may be visually obtrusive and care needs to be taken with the materials and design of the proposed development.

- (i) It is desirable that the development of land avoids major interference with the natural or pre-existing site levels, and preserves the natural topography.
- (ii) In order to share views with the maximum number of dwellings, a consistent scale in buildings that respects the natural topography is required.
- (iii) In accordance with the City's 'Minor Nature Development' Policy, filling or excavation of land that does not exceed 500 millimetres in height, does not constitute a significant alteration of the natural ground level and planning approval is not required.
- (iv) Any fill greater than 500 millimetres constitutes a significant alteration of the natural ground level and has implications on building height, solar access and privacy and requires planning approval.
- (v) Excavation behind the street setback line is generally acceptable as it is not usually as visually obtrusive as filling above natural ground level.

#### 7.4.8 Privacy

The protection of residential privacy is an important issue for the residents of the City of Vincent. The City recognises that reasonable side setbacks alone cannot realistically achieve absolute visual privacy.

To attain adequate levels of visual privacy, setbacks need to be complemented by thoughtful design and supplemented with various screening devices that are sensitive to issues such as orientation, access to winter sun, streetscape and amenity of adjacent properties.

**(i) Visual Privacy**

Dwelling designs should take into consideration both topographic and site specific features of the lot, and any potential overlooking that may result from the proposed development of the site.

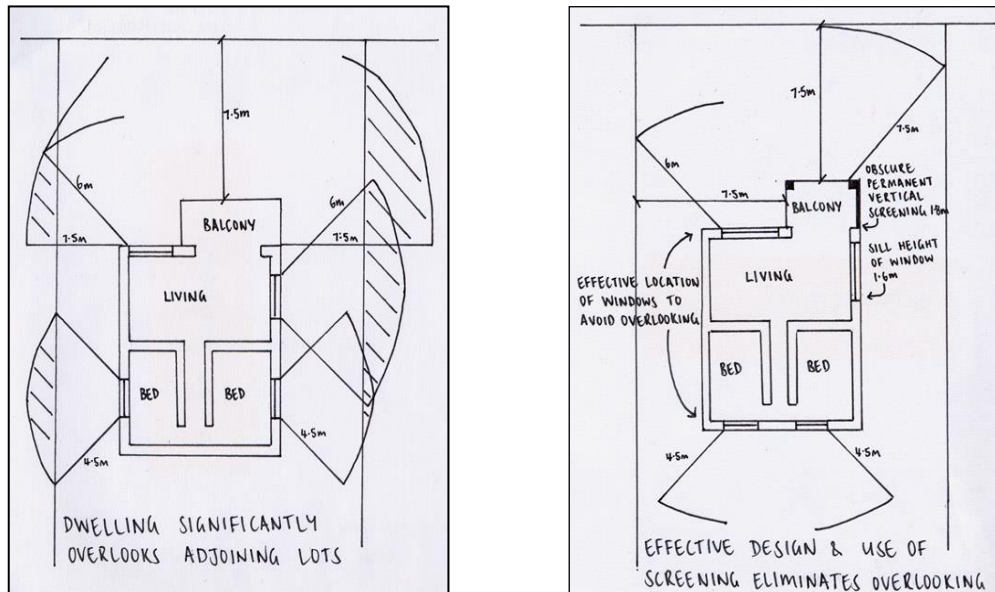


Figure 21: Examples of Non-Compliant and Compliant Scenarios and Demonstration of how Privacy Cone of Vision is Calculated.

**(ii) Other Forms of Visual Screening**

- (a) Horizontal Screening - This form of screening prevents unreasonable overlooking to adjacent lots while still allowing occupants to take advantage of distant views. Pergolas, planter boxes and the like can be used to provide screening that limits nearby overlooking but still allows for distant viewing (that is to the city skyline).

The horizontal plane of vision is calculated by drawing lines at each side of the opening and at 90 degrees to the opening. The sides of the horizontal plane of vision will then extend away from the opening and at 45 degrees to the drawn lines (or 135 degrees to the opening). The line of sight setback distance applicable to that particular opening will determine how far the plane of vision should extend in each case.

- (b) Balustrades, fin walls and louvres, where used to screen views from active habitable spaces such as balconies, decks or terraces, these should be sufficiently obscured to prevent unreasonable overlooking, to the satisfaction of the City. Additionally, the screening should be of sufficient length and width and be at least 1.6 metres in height above the finished floor level to ensure adequate visual privacy is achieved.
- (c) Vegetation - The exclusive use of mature landscaping for privacy screening is not considered to be sufficient to meet the privacy requirements of the Policy and it is therefore recommended that it be used to supplement other forms of visual screening.

**(iii) Acoustic Privacy**

If the operation of electric gates, electric garage doors or other mechanical equipment, such as air-conditioning units and swimming pool pumps, results in loss of amenity for an adjacent resident, the owner/occupant of the subject lot will be required to demonstrate that during operation of the equipment, the noise emitted does not exceed the assigned decibel levels in accordance with the *Environmental Protection Regulations 1997 (as amended)*.

**7.4.9 Energy Efficient Design**

**(i) Greenhouse Gas Emissions**

Global and national concern is rising over the effect of greenhouse gas emissions on climate change. The energy consumed in buildings is responsible for a significant proportion of total greenhouse gas emissions and improving energy efficiency in buildings is an important part of Australia's programme to reduce greenhouse gas emissions.

Some of the key advantages of buildings that use energy more efficiently are:

- Reduced energy costs;
- Increased levels of occupant comfort; and
- Reduced emissions of carbon dioxide and other greenhouse gases.

**(ii) Solar Access**

Development should be mindful of its impact on adjoining lots with respect to overshadowing.

Strata lots will be treated as separate lots in calculating overshadowing.

Where alterations and additions to dwellings are proposed, the total overshadowing for the whole development must comply with the solar access requirements of the R Codes.

It is acknowledged that physical factors may exist that minimise energy efficient principles from being implemented, such as:

- Lot orientation, streetscape and the existing urban form within established localities;
- Heritage restrictions; and
- Site topography.

In these circumstances, the development proposal will be assessed on its individual merit.

The following measures are encouraged to maximise solar access while reducing the extent of overshadowing:

- Internal and external living areas should be orientated in order to maximise solar access;
- Non-habitable rooms, such as laundries, bathrooms and storerooms should be located away from the northern aspect; and

Skylights, translucent roofs and glass bricks should be used to improve solar access.



#### **7.4.10 Adaptable and Accessible Housing**

Adaptable housing refers to dwellings with design features that are easily adapted at a later date with the changing needs of the occupants.

Accessible housing is generally a purpose built dwelling for a person or persons with a disability. This may be a new dwelling or one that is modified to suit the occupant. Adaptations made during dwelling modifications are not to be confused with 'adaptable housing'.

Housing design using these principles can benefit a wide section of the community by catering to the changing needs of individual residents.

**ELEMENT TWO - BUILDING DESIGN**

<b>ASSESSMENT TABLE</b>		
Design Solution	Deemed-to-Comply Criteria	Guidance Note Clause Reference
Preservation of Amenity on Adjoining Land and Surrounding Area	Preservation of Amenity on Adjoining Land and Surrounding Area	7.4.1
<p><b>BDPC 1. Preservation of Amenity on Adjoining Land and Surrounding Area</b></p> <p>(i) Development is to avoid compromising the amenity of adjoining properties by:</p> <ul style="list-style-type: none"> <li>• Maintaining an appropriate visual relationship between dwellings;</li> <li>• Ensuring that appropriate solar access to the outdoor living area and major openings of the subject site and adjacent lots is not significantly reduced; and</li> <li>• Ensuring that the privacy and sense of space of the adjoining lots is not unduly compromised</li> </ul>	<p><b>BDADC 1. Preservation of Amenity on Adjoining Land and Surrounding Area</b></p> <p>(a) This is to be achieved by:</p> <ul style="list-style-type: none"> <li>• Ensuring appropriate side setbacks and landscaping are incorporated into the development;</li> <li>• Compliance with the building height provisions of the Residential Design Codes and Residential Design Elements Policy;</li> <li>• Minimising the amount of excavation and fill on the development site;</li> <li>• Staggering openings to habitable spaces (between properties) to preserve reasonable privacy;</li> <li>• Giving consideration of overshadowing on adjoining lots, particularly in relation to outdoor living areas and major openings to habitable rooms; and</li> <li>• Adequately articulating the upper floor to the satisfaction of the City to mitigate the impact on adjoining neighbours.</li> </ul>	7.4.1

Design Solution	Deemed-to-Comply Criteria	Guidance Note Clause Reference
Streetscape Character Considerations	Streetscape Character Considerations	7.4.2
<p><b>BDPC 2. Streetscape Character Considerations</b></p> <p>(i) Development is to avoid compromising the streetscape.</p>	<p><b>BDADC 2. Streetscape Character Considerations</b></p> <p>(a) This is to be achieved by considering the existing:</p> <ul style="list-style-type: none"> <li>• Housing style;</li> <li>• Building setbacks (front and side);</li> <li>• Roof form;</li> <li>• Building bulk;</li> <li>• Building height; and</li> <li>• Building orientation</li> </ul>	7.4.2
Roof Forms	Roof Forms	7.4.3
<p><b>BDPC 3. Roof Forms</b></p> <p>(i) The roof of a building is to be designed so that:</p> <ul style="list-style-type: none"> <li>• It does not unduly increase the bulk of the building;</li> <li>• In areas with recognised streetscape value it complements the existing streetscape character and the elements that contribute to this character; and</li> <li>• It does not cause undue overshadowing of adjacent properties and open space.</li> </ul>	<p><b>BDADC 3. Roof Forms</b></p> <p>(a) This is to be achieved through:</p> <ul style="list-style-type: none"> <li>• The use of appropriate materials, colour and roof pitch;</li> <li>• The use of roof pitches between 30 degrees and 45 degrees (inclusive) being encouraged; and</li> <li>• The use of lower pitched roofs where they are compatible with existing development and streetscape.</li> </ul>	7.4.3

Design Solution	Deemed-to-Comply Criteria	Guidance Note Clause Reference
Building Bulk	Building Bulk	7.4.4
<p><b>BDPC 4. Building Bulk</b></p> <p>(i) Building bulk and scale is to complement the existing streetscape.</p> <p>(ii) New development is to be designed to complement, rather than dominate the immediate streetscape and adjacent properties.</p>	<p><b>BDADC 4. Building Bulk</b></p> <p>(a) In a predominantly single storey streetscape (where more than 50 per cent of the dwellings in the immediate street block, on the same side of the street that the subject dwelling is located is single storey), new development is required to minimise the impact of any upper floor at the primary street frontage.</p> <p>There are several design responses that may achieve this. The following options may be considered but are not limited to:</p> <ul style="list-style-type: none"> <li>• single storey additions to the side or rear of the dwelling; or</li> <li>• inclusion of an appropriate loft within the existing roof space; or</li> <li>• setting the upper floor back an appropriate distance so that it does not dominate the streetscape when viewed from street level. The appropriate distance will vary with each individual property; or</li> <li>• concealing the upper floor through the design of the roof; or</li> <li>• the incorporation of appropriate horizontal and vertical articulation to minimise the impact of the upper floor on the streetscape when viewed from street level.</li> </ul> <p>Notwithstanding the above, any upper floor is to comply with the minimum upper floor street setback requirements specified in clause SADC 5.</p> <p><i>Note:</i>  <i>The above are suggestions and are not considered to be the only design responses to minimise the impact of the bulk and scale. It is the applicants' responsibility to appropriately demonstrate that the impact of the upper floor of the building is minimised and does not have an undue impact on the existing streetscape with regard to bulk when viewed from street level.</i></p>	7.4.4

Design Solution	Deemed-to-Comply Criteria	Guidance Note Clause Reference								
	(b)Any upper floor addition is to be designed to provide relief and articulation contributing to variety and interest in the streetscape by incorporating fenestration (windows), balconies, awnings, verandahs, the use of various building materials, finishes and/or colours or any other architectural features and avoiding large expanses of ‘flush’ vertical surfaces.									
<b>Building Height</b>	<b>Building Height</b>	<b>7.4.5</b>								
<p><b>BDPC 5. Building Height</b></p> <p>(i) Building height is to be considered to:</p> <ul style="list-style-type: none"> <li>Limit the height of dwellings so that no individual dwelling dominates the streetscape;</li> <li>Limit the extent of overshadowing and visual intrusion on the private space of neighbouring properties; and</li> <li>Maintain the character and integrity of the existing streetscape.</li> </ul>	<p><b>BDADC 5. Building Height</b></p> <p>(a) The maximum height of a dwelling is to be 2 storeys (including any garage, loft or the like) and comply with the following measurements:</p> <table border="1" data-bbox="576 976 1153 1424"> <thead> <tr> <th>Element</th> <th>Maximum Allowable Heights For Two Storey Dwellings</th> </tr> </thead> <tbody> <tr> <td>Top of external wall (roof above) (1)</td> <td>6.0 metres</td> </tr> <tr> <td>Top of external wall (concealed roof) (1)</td> <td>7.0 metres</td> </tr> <tr> <td>Top of pitched roof (1)</td> <td>9.0 metres</td> </tr> </tbody> </table> <p>Notes:</p> <p>(1)The above heights are the absolute maximum and are not to be increased, regardless of the length of gable walls or roof ridges as may be permitted under the requirements of the Residential Design Codes.</p> <p>(2)Please refer to Guidance Notes clause 7.4.5 for instances where the City may consider a variation to the building external wall and pitched roof height requirements.</p> <p>(3)The following matters are to be considered with regard to the potential impact on neighbouring properties:</p> <ul style="list-style-type: none"> <li>minimise overshadowing and overlooking of living and private open space areas; and</li> </ul>	Element	Maximum Allowable Heights For Two Storey Dwellings	Top of external wall (roof above) (1)	6.0 metres	Top of external wall (concealed roof) (1)	7.0 metres	Top of pitched roof (1)	9.0 metres	<b>7.4.5</b>
Element	Maximum Allowable Heights For Two Storey Dwellings									
Top of external wall (roof above) (1)	6.0 metres									
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Top of pitched roof (1)	9.0 metres									

Design Solution	Deemed-to-Comply Criteria	Guidance Note Clause Reference
	<ul style="list-style-type: none"> <li>relationship to the streetscape.</li> </ul> Definition: “Basement” means a storey of a building partly or wholly underground, such structure which supports those portions of the building which come above it.	
<b>Lofts</b>	<b>Lofts</b>	<b>7.4.6</b>
<b>BDPC 6. Lofts</b> (i) The use of space within the roof space for habitable purposes is encouraged in lieu of a second storey, particularly in streetscapes that comprise predominantly single storey dwellings.	<b>BDADC 6. Lofts</b> (a) Lofts are to be contained between the roof pitch area (no less than 35 degrees and no greater than 45 degrees) and the top of the ceiling of the storey immediately below; (b) Lofts are not to: <ul style="list-style-type: none"> <li>Increase the bulk of the building;</li> <li>Resemble an additional storey;</li> <li>Cause undue overshadowing of adjacent properties and open spaces;</li> <li>Unduly overlook adjoining properties; and</li> <li>Result in relaxation of other requirements (especially height and setbacks) to accommodate useable floor area for lofts.</li> </ul> (c) Dormer windows and gables to lofts are to comply with the following requirements: <ul style="list-style-type: none"> <li>Not project beyond the roof pitch nor exceed 2.4 metres above the loft finished floor level, whichever is the greater; and</li> <li>Maximum aggregate length of 4.5 metres or 20 per cent of the length of the dwelling on that particular elevation, whichever is the lesser.</li> </ul>	<b>7.4.6</b>
<b>Site Levels</b>	<b>Site Levels</b>	<b>7.4.7</b>
<b>BDPC 7. Cut and Fill</b>	<b>BDADC 7. Cut and Fill</b> <i>Note: This Clause was amended by the Council at its Ordinary Meeting held on 9 July 2013.</i>	<b>7.4.7</b>
<b>BDPC 8. Retaining Walls</b>	<b>BDADC 8. Retaining Walls</b> <i>Note: This Clause was amended by the Council at its Ordinary Meeting held on 9 July 2013.</i>	<b>7.4.7</b>

Design Solution	Deemed-to-Comply Criteria	Guidance Note Clause Reference
<b>Privacy</b>	<b>Privacy</b>	<b>7.4.8</b>
<p><b>BDPC 9. Visual Privacy</b></p> <p>(i) Private open spaces and habitable rooms of a dwelling and adjacent dwellings should be protected from direct and unreasonable overlooking.</p> <p>(ii) The owners of affected properties providing written consent stating no objection to the specified privacy encroachment.</p> <p><i>Note: This Clause was amended by the Council at its Ordinary Meeting held on 9 July 2013.</i></p>	<p><del><b>BDADC 9. Visual Privacy</b></del></p> <p><i>Note: This Clause was amended by the Council at its Ordinary Meeting held on 9 July 2013.</i></p>	<p><b>7.4.8 (i) &amp; (ii) (a), (b), (c)</b></p>
<p><b>BDPC 10. Acoustic Privacy</b></p> <p>(i) The transmission of noise between adjoining properties should be minimised.</p> <p>(ii) Dwellings abutting major roads and other noise generating land uses should be designed and sited to minimise noise impacts.</p>	<p><b>BDADC 10. Acoustic Privacy</b></p> <p>(a) Mechanical equipment such as air-conditioning units, swimming pool pumps and garage door and electric gate motors are not to be located:</p> <ul style="list-style-type: none"> <li>• Within the side setback area of a dwelling;</li> <li>• Directly adjacent to openings to internal habitable areas (such as bedrooms or living rooms) of neighbouring dwellings; and</li> <li>• Directly adjacent to the primary outdoor living area of any neighbouring property.</li> </ul>	<p><b>7.4.8 (iii)</b></p>

Design Solution	Deemed-to-Comply Criteria	Guidance Note Clause Reference
(iii) At all times the City will take into account the visual character and streetscape of an area when considering the appropriateness of any noise barrier.	(b) Walls common to more than one dwelling are to be appropriately treated (filled or insulated against noise) to the satisfaction of the City.	
<b>Energy Efficient Design</b>	<b>Energy Efficient Design</b>	<b>7.4.9</b>
<b>BDPC 11. Energy Efficient Design</b>  (i) Development should incorporate energy efficient design principles.	<b>BDADC 11. Energy Efficient Design</b>  (a) Development proposals should incorporate the following energy efficient design principles where practical: <ul style="list-style-type: none"> <li>• A northerly orientation;</li> <li>• Thermal mass to regulate heat loss/gain;</li> </ul>	<b>7.4.9 (i)</b>
	<ul style="list-style-type: none"> <li>• Access to winter sun and summer shade by appropriate location of windows and shading elements, such as trees;</li> <li>• Cross ventilation;</li> <li>• Controls or measures to prevent unnecessary heat gain and loss;</li> <li>• Screening of windows by sun protection measures;</li> <li>• Roof and wall insulation;</li> <li>• Solar heating; and</li> <li>• Careful selection and location of trees and shrubs around the curtilage of the dwelling.</li> </ul>	



Design Solution	Deemed-to-Comply Criteria	Guidance Note Clause Reference
<p><b>BDPC 12. Solar Access</b></p> <p>(i) Development design will endeavour to:</p> <ul style="list-style-type: none"> <li>• Minimise overshadowing of adjacent properties and outdoor living areas;</li> <li>• Provide a reasonable level of solar access to habitable areas and outdoor living areas by considering building siting and orientation;</li> <li>• Regulate the desired amount of solar access to habitable rooms and outdoor living areas with the placement of windows; and</li> <li>• Provide sun protection with the use of sun shading devices and by the placement of appropriate canopy trees.</li> </ul>	<p><b>BDADC 12. Solar Access</b></p> <p>Overshadowing/solar access for adjoining properties is to comply with the Deemed-to-Comply Solar Access requirements of the Residential Design Codes.</p>	<p>7.4.9 (ii)</p>

Design Solution	Deemed-to-Comply Criteria	Guidance Note Clause Reference
<p><b>Adaptable and Accessible Housing</b></p>	<p><b>Adaptable and Accessible Housing</b></p>	<p><b>7.4.10</b></p>
<p><b>BDPC 13. Adaptable and Accessible Housing</b></p> <p>(i) Development design will endeavour to incorporate design features that are reasonably adapted at a later date with the changing needs of the occupants.</p>	<p><b>BDADC 13. Adaptable and Accessible Housing</b></p> <p>(a) Applicants and designers are strongly encouraged to refer to the following standards when preparing development applications for ‘Accessible’ and ‘Adaptable’ Housing:</p> <p>(1) Siting</p> <ul style="list-style-type: none"> <li>• A lot not exceeding a 1:14 gradient is required for any development site proposing to construct ‘adaptable housing’;</li> <li>• A continuous accessible path of travel is to be provided from the street frontage and the vehicle parking area to the entry of the dwelling; and</li> <li>• Paths and walkways are to be slip resistant.</li> </ul> <p>(2) Letterboxes</p> <ul style="list-style-type: none"> <li>• Letterboxes are to be situated in a central location and adjacent to the street entry; and</li> <li>• Letterboxes are to be located on a hard standing area connected by an accessible and continuous pathway.</li> </ul> <p>(3) Car Parking</p> <ul style="list-style-type: none"> <li>• Car parking bays are to be a minimum of 6 metres by 3.8 metres;</li> <li>• The minimum internal clearance of the garage or carport is to be 2.5 metres;</li> <li>• All car parking spaces are to be provided with permanent roof cover; and</li> <li>• Access between the car parking and the dwelling are to be provided with permanent roof cover.</li> </ul> <p>(4) Accessible Entry</p> <ul style="list-style-type: none"> <li>• All dwelling entry points are to be level and not exceed a maximum grade of 1:14 in order to facilitate wheelchair</li> </ul>	<p><b>7.4.10</b></p>

Design Solution	Deemed-to-Comply Criteria	Guidance Note Clause Reference
	<p>access;</p> <ul style="list-style-type: none"> <li>• The dwelling entry is to be provided with permanent roof cover; and</li> <li>• The primary entry point of the dwelling is to be provided with a security door.</li> </ul> <p>(5) Exterior – General</p> <ul style="list-style-type: none"> <li>• All external door are to be keyed alike; and</li> <li>• Security screening is to be installed on all exterior openings.</li> </ul> <p>(6) Internal – General</p> <ul style="list-style-type: none"> <li>• Internal door are to have a minimum clearance of 820 millimetres; and</li> <li>• Internal corridors are to have a minimum width of 1000 millimetres;</li> <li>• Shower and toilet facilities are to be provided in accordance with Australian Standard AS1428.1.</li> </ul>	