

5.3 NOS. 378 AND 380 (LOTS: 68 & 67; D/P: 613) WILLIAM STREET, PERTH - PROPOSED CHANGE OF USE FROM WAREHOUSE/STORAGE TO TAVERN, RESTAURANT/CAFE AND SHOP INCLUDING ALTERATIONS AND ADDITIONS

Ward: South

- Attachments:**
1. Consultation and Location Plan
 2. Development Plans
 3. Applicant's Development Application Report
 4. Traffic Impact Assessment
 5. Parking Management Plan
 6. Venue Management Plan
 7. Public Interest Assessment
 8. Acoustic Report
 9. Waste Management Plan
 10. Environmentally Sustainable Design Statement
 11. Summary of Submissions - Applicant's Response
 12. Summary of Submissions - Administration's Response
 13. Determination Advice Notes

RECOMMENDATION:

That Council, in accordance with the provisions of the City of Vincent Local Planning Scheme No. 2 and the Metropolitan Region Scheme, APPROVES the development application for a Proposed Change of Use from Warehouse/Storage to Tavern, Restaurant/Café and Shop including Alterations and Additions at Nos. 378 and 380 (Lots: 68 & 67; D/P: 613) William Street, Perth, in accordance with the plans shown in Attachment 2, subject to the following conditions, with the associated determination advice notes in Attachment 13:

1. Development Approval

This approval is for a Change of Use from Warehouse/Storage to Tavern, Restaurant/Café and Shop including Alterations and Additions as shown on the approved plans dated 11 August 2023. No other development forms part of this approval;

2. Use of Premises

2.1 This approval is for a Tavern, Restaurant/Café and Shop as defined in the City of Vincent Local Planning Scheme No. 2. The use of the subject land for any other land use may require further approval from the City;

2.2 The total number of persons attending the premises at any one time, including staff and customers, shall be limited to 365 persons; and

2.3 The premises shall be limited to the following hours of operation:

- Monday to Saturday: 7:00am to 12:00am; and
- Sunday and Public Holidays: 7:00am to 10:00pm;

unless an Extended Trading Permit for alternative hours is issued by the Racing Gaming and Liquor Division of the Department of Local Government, Sport and Cultural Industries;

3. Building Design

3.1 Prior to the issue of a building permit, a detailed schedule of external finishes (including materials, colour schemes and details) shall be submitted to and approved by the City. The development shall be finished in accordance with the approved schedule prior to the use or occupation of the development, to the satisfaction of the City;

- 3.2 All external fixtures and building plant, including air conditioning units, piping, ducting and water tanks, shall be located so as to minimise any visual and noise impact on surrounding landowners, and screened from view from the street, and surrounding properties to the satisfaction of the City; and
- 3.3 The roof of the proposed addition shall have a maximum solar absorptance rating of 0.4, to the satisfaction of the City;

4. Façade Design

- 4.1 Doors and windows and adjacent floor areas fronting William Street shall maintain an active and interactive relationship with the street, to the satisfaction of the City;
- 4.2 Glazing and/or tinting shall have a minimum of 70 percent visible light transmission to provide unobscured visibility between the street and the interior of the tenancy, to the satisfaction of the City;
- 4.3 Internal security and privacy treatments shall be located and installed internally behind the glazing line or recessed, and shall be transparent and visually permeable to allow views inside the building and enable internal light sources to be seen from the street, to the satisfaction of the City; and
- 4.4 The fencing to the William Street elevation shall be visually unobstructed, to the satisfaction of the City;

5. Landscaping

- 5.1 An amended detailed landscape and reticulation plan for the development site, to the satisfaction of the City, shall be lodged with and approved by the City, prior to the issue of a Building Permit. The plan shall be drawn to a scale of 1:100 or 1:200, shall be generally in accordance with the plan previously provided to the City dated 11 August 2023, and show the following:
 - The location and type of existing and proposed plants, including pot sizes;
 - The location of a minimum of 9.2 percent deep soil areas across the site;
 - Areas to be irrigated or reticulated;
 - Tree species within the raised planting area near the north east boundary of the development site shall be revised to a *Tipuana Tipu* or *Gledistia*, on advice from the City's Parks Team; and
- 5.2 All works shown in the approved landscape and reticulation plan as identified in Condition 5.1 shall be undertaken in accordance with the approved plans to the City's satisfaction, prior to occupancy or use of the development and maintained thereafter to the satisfaction of the City at the expense of the owners/occupiers;

6. Access and Bicycle Facilities

- 6.1 Prior to occupancy or use of the development, redundant or "blind" crossovers shall be removed and the verge and kerb made good to the satisfaction of the City, at the applicant/owner's full expense;
- 6.2 All pedestrian access levels shall match into existing verge and footpath levels, to the satisfaction of the City; and
- 6.3 24 bicycle parking spaces shall be provided on-site in accordance with Australian Standard AS2890.3 prior to the occupation or use of the development, to the satisfaction of the City;

7. Payment-in-lieu of Car Parking

- 7.1 A payment-in-lieu contribution of \$22,507.20 shall be paid to the City for the shortfall of four car parking spaces, based on the cost of \$5,626.80 per bay as set out in the City's 2023-24 Schedule of Fees and Charges, prior to the occupancy or use of the development or by entering into a written agreement with the City to pay the payment-in-lieu over an agreed period up to five years; and**
- 7.2 Prior to the occupancy or use of the development the owner(s) or the applicant on behalf of the owner(s) shall comply with the following requirements:**
- (a) pay a payment-in-lieu contribution of \$22,507.20 for the equivalent value of four car parking spaces, based on the cost of \$5,626.80 per bay as set out in the City's 2023-24 Schedule of Fees and Charges;**

OR

- (b) lodge an appropriate assurance bond/ bank guarantee of a value of \$22,507.20 to the satisfaction of the City. This assurance bond / bank guarantee will only be released to the owner(s) / applicant where the subject 'Approval to Commence Development' did not commence and subsequently expired;**

8. Stormwater

Stormwater from all roofed and paved areas shall be collected and contained on site. Stormwater must not affect or be allowed to flow onto or into any other property or road reserve;

9. Waste Management

- 9.1 The Waste Management Plan dated 11 August 2023 shall be implemented at all times to the satisfaction of the City;**
- 9.2 Waste collection shall be limited to between 9:00am to 7:00pm Monday to Sunday and public holidays; and**
- 9.3 Waste and refuse generated on the site by the development shall be collected by a private contractor at the expense of the applicant/landowner;**

10. Acoustic Report and Noise Management

- 10.1 Prior to the issue of a Building Permit, an updated acoustic report shall be submitted to the City to demonstrate compliance with the City's Sound Attenuation Policy 7.5.21, namely in relation to the construction of the building and mechanical services. The recommended measures of the report shall be implemented, to the satisfaction of the City; and**
- 10.2 All assumptions and recommendations included in the approved acoustic report prepared by Acoustic Consultants Australia, dated 13 July 2023, shall be implemented as part of the development, to the satisfaction of the City prior to the occupancy or use of the development and maintained thereafter to the satisfaction of the City at the expense of the owners/occupiers;**

11. Construction Management Plan

Prior to the issue of a Building Permit, a Construction Management Plan that details how the construction of the development will be managed to minimise the impact on the surrounding (including demolition and/or forward works).

The Construction Management Plan is required to address the following concerns that relate to any works to take place on the site:

- Public safety, amenity and site security;
- Contact details of essential site personnel;
- Construction operating hours;
- Noise control and vibration management;
- Air, sand and dust management;
- Stormwater and sediment control;
- Soil excavation method;
- Waste management and materials re-use;
- Traffic and access management;
- Parking arrangements for contractors and subcontractors;
- Consultation plan with nearby properties; and
- Compliance with AS4970-2009 relating to the protection of trees on the verge adjacent to the development site; and

12. Venue Management

The premises shall operate in accordance with the approved Venue Management Plan dated 11 August 2023, to the satisfaction of the City.

EXECUTIVE SUMMARY:

The purpose of this report is to consider an application for development approval for a change of use from Warehouse/Storage to Tavern, Restaurant/Café and Shop including Alterations and Additions to an existing single storey commercial development at Nos. 378 and 380 William Street, Perth (the subject site).

The subject site contains an existing vacant commercial development. This is a single storey warehouse building with associated car parking and landscaping.

The warehouse building is setback 13 metres from William Street and is proposed to be retained as part of the development.

The subject site is zoned District Centre under the City's Local Planning Scheme No. 2 (LPS2) and is located within the Town Centre Built Form Area under the City's Policy No. 7.1.1 – Built Form (Built Form Policy), with a building height standard of four storeys.

The application proposes the adaptive reuse of the existing building on the subject site to facilitate the opening of a hospitality/entertainment venue with a maximum of 350 customers and 15 employees on-site at any given time. The venue is proposed to operate as a record store and café, and a bar with kitchen and beer garden, which are consistent with the Shop, Restaurant/Café and Tavern land uses, respectively, under LPS2.

The proposed works to the existing building would involve the demolition of portions of internal and external walls to facilitate the installation of new openings and an internal fit-out of the building to accommodate the proposed uses. Single storey additions are proposed to the existing building along the rear boundary to accommodate toilet facilities and a bar. A new building is proposed towards the front of the site, including a parasol roof located over alfresco dining to accommodate the proposed café, shop and radio booth.

No on-site car parking is proposed to be provided and the redundant crossover is proposed to be removed.

Across the site, the development also includes acoustic walls along the north-east and south-east lot boundaries and landscaping works along the front boundary and within the outdoor alfresco dining areas.

The key areas of discretion being sought under the planning framework relate to land use acceptability, the provision of car parking facilities, landscaping, and building design.

The proposed Tavern land use is capable of approval in the District Centre zone under LPS2, and the City's Policy No. 7.5.7 – Licensed Premises (Licensed Premises Policy) sets out that this zone is the most compatible zone for this use, along with the Commercial zone. The site is highly accessible, and the proposed use would contribute to the level of activity and diversity of uses within the William Street Town Centre. The proposal would re-activate a vacant site and would contribute to passive surveillance of public spaces throughout the day.

The development proposes nil on-site car parking bays which results in a shortfall of 55 bays. The applicant has provided a Traffic Impact Assessment and Parking Management Plan in support of the proposed car parking shortfall. The information provided by the applicant demonstrates that the additional demand for car parking generated by the proposed development would be capable of being accommodated by existing public car parking. The development would inevitably reduce the current availability of public parking bays in the locality and a condition of approval is included in the officer recommendation for a partial payment-in-lieu of car parking contribution equivalent to four bays to assist the City in managing and maintaining these public parking facilities.

The application was referred to the Chairperson of the City's Design Review Panel for comment who support the proposed building design. The development would be sympathetic to the existing streetscape character and would provide articulation and visual interest when viewed from the streetscape.

Across the site, the development would provide appropriately sized deep soil areas to support landscaping and tree planting. The location of the landscaping would assist with softening the impact of the proposed built form as viewed from the street while providing internal amenity for visitors to the development.

The proposed development is acceptable as considered against the planning framework and is recommended for approval subject to conditions.

PROPOSAL:

The development application proposes adaptive reuse of the existing single storey building on the subject site including alterations and additions and a change the use from Warehouse/Storage to Tavern, Restaurant/Café and Shop as defined under the City's LPS2. The venue is proposed to operate as a record store and café, a bar with kitchen and beer garden, and an online radio studio.

Details of the proposed development works include:

- Retention of the existing single storey building, with the following alterations and additions proposed:
 - Demolition of existing internal walls of the building and portions of external walls to facilitate the installation of new openings.
 - Internal fit-out of the existing building to accommodate the Restaurant use with associated kitchen and storerooms.
 - Single storey additions to the existing building along the rear boundary to accommodate toilet facilities and a bar.
 - A pergola structure to the north of the existing building with alfresco dining.
- Construction of a new freestanding building to the front of the site to accommodate the proposed café, shop and radio booth.
- Construction of a parasol roof located in the front setback area, located over alfresco dining.
- 24 bicycle parking spaces, nil on-site car parking and the removal of the redundant crossover that currently provides vehicle access from William Street. All access to the development is to be provided from William Street.
- Installation of a rainwater tank, located between the existing building and William Street.
- Acoustic walls located on the north-east and south-east lot boundaries.
- Demolition and reconstruction of a front fence.
- Bin storage to the northern boundary.
- Landscape works along the front boundary and within the outdoor alfresco dining areas.

Details of the proposed land use operation include:

- The venue is proposed to operate as a multi-purpose space incorporating:
 - Passenger Bar: A portion of the Tavern which includes a lounge bar and kitchen, located within the existing building on the site.
 - Passenger Backyard: A portion of the Tavern which includes beer garden, located to the northern boundary of the site.
 - Record Shop and Café with Passenger Radio: Retail shop with a café and internet radio station component, located within the proposed building in the front setback area.
- The venue would have a maximum of 350 customers and 15 employees on-site at any given time.
- The operating hours of the venue would be:
 - Monday to Saturday: 7:00am – 12:00am (midnight).
 - Sundays and Public Holidays: 7:00am – 10:00pm.

The applicant has provided indicative opening hours for the different elements of the proposed development. The applicant has stated that these represent the 'maximum viable operating hours' with the exact times and public holiday hours to vary based on levels of trading. The indicative opening hours are:

- Tavern and Restaurant portion of the development:
 - Monday and Tuesday: 4:00pm to 11:00pm.
 - Wednesday and Thursday: 4:00pm to 12:00am (midnight).
 - Friday and Saturday: 11:00am to 12:00am (midnight).
 - Sunday: 11:00am to 10:00pm.
- Café and Shop portion of the development:
 - Monday to Friday: 7:00am to 4:00pm.
 - Saturday: 9:00am to 4:00pm.
 - Sunday: Closed.

Plans of the proposal are included as **Attachment 2**.

The applicant's supporting documentation including the applicant's development application report, a Traffic Impact Assessment, Parking Management Plan, Venue Management Plan, Public Interest Assessment, Acoustic Report, Waste Management Plan and Environmentally Sustainable Design Statement are included as **Attachments 3 – 10**, respectively.

BACKGROUND:

Landowner:	Green Pillars Pty Ltd, Lau Brother Management Pty Ltd and Christopher Sui Hung Lau
Applicant:	Method Planning
Client:	Sound Hospitality Pty Ltd
Date of Application:	11 August 2023
Zoning:	MRS: Urban LPS2: Zone: District Centre R Code: N/A
Built Form Area:	Town Centre
Existing Land Use:	Warehouse/Storage
Proposed Use Class:	Tavern, Restaurant/Café and Shop
Lot Area:	988 square metres
Right of Way (ROW):	N/A
Heritage List:	N/A

Site Context and Zoning

The subject site is bound by William Street to the north-west, a recently constructed single storey supermarket development to the south-west, single storey commercial developments to the south-east and a vacant lot to the north-east. A location plan is included as **Attachment 1**.

The surrounding development context generally consists of commercial developments located along William Street. The area is characterised by a mix of shop/retail and hospitality businesses with residential development located along side streets, including Monger Street, Money Street, and Forbes Road.

The subject site contains an existing vacant commercial development, being a single storey warehouse building with associated car parking and landscaping. The warehouse building is setback 13 metres from William Street and is proposed to be retained as part of the development.

The subject site and the properties to the north-east and south-west of the subject site that front William Street are zoned District Centre under LPS2 and are located within the Town Centre Built Form Area under the City's Built Form Policy, with a building height standard of four storeys. To the south-east, the properties fronting Money Street are zoned Commercial and are located within the Mixed Use Area Built Form Area under the Built Form Policy, with a building height standard of four storeys.

DETAILS:

Summary Assessment

The table below summarises the planning assessment of the proposal against the provisions of LPS2, the City's Built Form Policy and relevant local planning policies, including the Non-Residential Parking Policy, Licensed Premises Policy and Policy No. 7.5.21 - Sound Attenuation (Sound Attenuation Policy). In each instance where the proposal requires the discretion of Council, the relevant planning element is discussed in the Detailed Assessment section following from this table.

Planning Element	Use Permissibility/ Deemed-to-Comply/ Acceptable Outcomes (or equivalent)	Requires the Discretion of Council
Land Use		✓
Hours of Operation	✓	
Building Height	✓	
Street Setback	✓	
Side and Rear Setbacks	✓	
Orientation	✓	
Landscaping		✓
Public Domain Interface		✓
Pedestrian Access and Entries	✓	
Car Parking		✓
Bicycle Parking		✓
Façade Design		✓
Roof Design	✓	
Adaptive Reuse	✓	
Environmentally Sustainable Design		✓
Utilities	✓	
Non-Residential Parking Policy		✓
Licensed Premises Policy	✓	
Sound Attenuation Policy	✓	

Detailed Assessment

The deemed-to-comply/acceptable outcome assessment of the element that requires the discretion of Council is as follows:

Land Use	
Use Permissibility	Proposal
LPS2	
'P' Use	Tavern - 'A' Use
Landscaping	
Acceptable Outcomes	Proposal
Built Form Policy Volume 3, Clause 1.5	
A1.5.1 – 12% (118.6 square metres) of the site to be provided as deep soil areas.	9.2% (90.6 square metres) deep soil areas.

Car and Bicycle Parking	
Deemed-to-Comply Standard	Proposal
<p>Policy No. 7.7.1 – Non-Residential Development Parking Requirements</p> <p><i>Car Parking</i> 55 car parking bays required based on maximum of 350 patrons and 15 staff.</p> <p><i>Bicycle Parking</i> 16 long-term bicycle parking spaces required.</p>	<p>Nil on-site car parking bays provided.</p> <p>Nil long-term bicycle parking spaces provided.</p>
Public Domain Interface	
Acceptable Outcomes	Proposal
<p>Built Form Policy Volume 3, Clause 1.5</p> <p>A1.7.7 – Bins are not located within the primary street setback or in locations visible from the primary street.</p> <p>A1.7.8 – Services and utilities that are located in the primary street setback are integrated into the design of the development and do not detract from the amenity and visual appearance of the street frontage.</p>	<p>The bin store would be visible from the primary street.</p> <p>The rainwater tank would be located in the primary street setback area and would not be integrated into the design of the building.</p>
Façade Design	
Acceptable Outcomes	Proposal
<p>Built Form Policy Volume 3, Clause 1.13</p> <p>A1.13.2 – Commercial ground floor spaces shall have a finished floor level to finished ceiling level height of a minimum of 3.5m.</p> <p>A1.13.3 – Commercial development shall provide a continuous protective awning over the pedestrian footpath.</p> <p>A1.13.6 – Where provided, doorways shall have a depth between 500mm and 1.5m to clearly articulate entrances to commercial buildings and tenancies.</p>	<p>The café tenancy and radio tenancy have an internal ceiling height of 3.2m.</p> <p>No awning is provided over the pedestrian footpath.</p> <p>The café tenancy and radio tenancy do not have recessed doorways.</p>
Environmentally Sustainable Design	
Acceptable Outcomes	Proposal
<p>Built Form Policy Volume 3, Clause 1.17</p> <p>A1.17.2 – Development achieves one of the permitted environmental performance standards detailed, or their equivalent. These include Green Building Council of Australia’s Green Star Rating System or a Life Cycle Assessment in Accordance with EN15978.</p>	<p>No report detailing how the development achieves the environmental performance standards has been provided.</p>

The above elements of the proposal do not meet the specified standards and are discussed in the Comments section below.

CONSULTATION/ADVERTISING:

Community consultation was undertaken in accordance with the *Planning and Development (Local Planning Schemes) Regulations 2015* for a period of 14 days, from 14 September 2023 to 29 September 2023. In accordance with the City's Community and Stakeholder Engagement Policy, the method of consultation included a notice on the City's website, a sign to the William Street frontage and letters mailed to owners and occupiers to nearby properties.

The standard consultation radius for the application, in accordance with the City's Community and Stakeholder Engagement Policy, was all adjoining and adjacent properties. The Community and Stakeholder Engagement Policy permits the Administration to vary the consultation radius on a case-by-case basis where owners and/or occupiers of properties in the vicinity of the proposed development are likely to be affected by the granting of development approval.

Due to the scale of the development and the extent of the departures proposed to the car parking requirements of the City's Non-Residential Parking Policy, Administration extended the consultation radius to include additional properties along William Street, Monger Street and Money Street, as shown in **Attachment 1**. This is because these streets are within a 150-metre walking distance of the subject site and could reasonably be locations that visitors to the venue may utilise for car parking.

A total of 46 submissions were received at the conclusion of the advertising period including 43 submissions of support and three submissions objecting to the proposal.

The key comments received in support are summarised as follows:

- The proposal is consistent with the objectives of the District Centre zone.
- The proposal would activate the vacant site and improve the vibrancy in the area while offering additional surveillance to the street in non-peak times.
- The applicant has provided adequate consideration of parking and noise concerns with suitable separation from residential zones provided.
- The area in which the site is located is well-serviced by public transport.
- The uses proposed as part of the development would provide a benefit to the wider community.
- The landscaping will provide an active street front that will enhance the passive surveillance of the area.

The key concerns raised are summarised as follows:

- Concerns regarding the impact of the proposed development on the availability of car parking in the surrounding area. Further concerns regarding the impact on the existing surrounding businesses due to the proposed car parking shortfall.
- Comments regarding the existence of similar land uses in the surrounding area and the lack of public need for the proposed development.
- Concerns regarding the proposed development not being in keeping with the lower density of the area and the resulting impact on the existing amenity of the area.

A detailed summary of submissions received during consultation, along with the applicant's response to the submissions received is provided as **Attachment 11**. Administration's response to the summary of submissions is provided as **Attachment 12**.

Department of Planning, Lands and Heritage – Other Regional Road Referral:

In accordance with Delegation 2022/03 from the Western Australian Planning Commission (WAPC), the application was referred to the Department of Planning, Lands and Heritage (DPLH). This is because a portion of the lot is reserved in the Metropolitan Region Scheme as a Regional Road and the development has a construction value greater than \$250,000.

DPLH provided written comments advising no objection to the proposed development on Other Regional Roads planning grounds.

Design Review Panel (DRP):

Referred to DRP: Yes

The proposal was referred to a Chairperson of the City’s DRP for comment. The DRP Chairperson noted positive aspects of the proposal that are summarised below.

- The proposed uses would provide activation to William Street as well as the surrounding context and would contribute to the streetscape positively. The alfresco area fronting William Street will provide a high amenity semi-public space for the local community.
- The streetscape interface is active with the with pedestrian entry addressing William Street with a large alfresco area on the streetscape and visually permeable front fencing which will generate high levels of passive surveillance of the street.
- The adaptive re-use of the existing structure is supported from a local character and environmentally sustainable design perspective. The inclusion of the rainwater collection tank and oversized gutter at the front of the new building are also support as an environmentally sustainable design initiative.
- The proposal provides landscaping which includes native species, integrated into to the alfresco area. The soft planting proposed on the streetscape interface will contribute to the amenity of the area and will soften the visual impact of the front fence on the streetscape.
- The internal planning of the buildings and outdoor alfresco areas are functional and efficient with the main building and outdoor alfresco area receiving good natural and north light access.
- The proposal integrates a diverse range of high-quality materiality such as face brick, textured concrete, galvanised roof sheeting, timber windows, trellis mesh screening, a parasol roof and timber beams which are all materials derived from the surrounding context. The development will provide a contemporary industrial aesthetic and an interesting new offering on William Street.
- The inclusion of bike racks in the alfresco area in lieu of car parking on the site and the removal of the existing crossover is supported in this location.

The DRP Chairperson also provided comments that require further consideration. These are addressed further below.

The table below shows the design review evaluation by the DRP Chairperson as considered against the 10 principles of good design.

Design Review Progress Report	
	<i>Supported</i>
	<i>Pending further attention</i>
	<i>Not supported</i>
	<i>No comment provided/Insufficient information</i>
	DRP Chairperson Referral
Principle 1 – Context & Character	
Principle 2 – Landscape Quality	
Principle 3 – Built Form and Scale	
Principle 4 – Functionality & Built Quality	
Principle 5 – Sustainability	
Principle 6 – Amenity	
Principle 7 – Legibility	
Principle 8 – Safety	
Principle 9 – Community	
Principle 10 – Aesthetics	

The application was not referred back to the DRP Chairperson, as the applicant elected not to make any modifications to the proposal to address their comments and requested any changes be dealt with through conditions of approval.

The table below provides a summary of the outstanding DRP comments and Administration's response to these.

DRP Comments Received	Administration Comment:
<p><u>Principle 1 – Context and Character</u></p> <p>The DRP Chairperson was supportive the development against Principle 1 but provided the below additional comments.</p> <p>As the front fence forms a prominent, unusual, element on the streetscape it would be appropriate to request further detail in relation to the design of the fence and encourage the applicant to introduce some diversity of materiality or design.</p> <p>The applicant is alternatively encouraged to add diversity in the design/pattern of the steel members. This could add more visual interest to that interface.</p>	<p>The front fence provides a suitable transition between the public and private domain. The landscaping proposed either side of the fence would soften the impact of the proposed development and presence of the fence on the streetscape.</p> <p>The design of the fence meets the Acceptable Outcomes for fencing in the City's Built Form Policy. The design provides a high level of visual permeability which allows for street surveillance which enhances the amenity of the area. A condition of approval is included in the officer recommendation for the fence to be visually unobstructed, to the satisfaction of the City to ensure an acceptable level of visual permeability is provided.</p> <p>The materiality of the fence is consistent with existing fences and vehicle access gates within the streetscape.</p>
<p><u>Principle 2 – Landscape Quality</u></p> <p>The applicant is encouraged to look for further opportunities to increase deep soil areas on the site including in the front alfresco area and provide a larger tree to provide additional shade on the footpath.</p> <p>Further detail is recommended in relation to tree and planting pot sizes as well as reticulation systems.</p>	<p>The landscaping across the site would provide deep soil areas with sufficient area and volume to sustain healthy plant and tree growth. The proposed landscaping is suitably located to reduce the impact of the development on the streetscape and with suitable shade provided by the existing mature street trees.</p> <p>A condition of approval has been included in the officer recommendation requiring an updated landscaping plan to include details of the proposed tree and planting pot sizes and reticulation systems.</p>

LEGAL/POLICY:

- *Planning and Development Act 2005;*
- *Planning and Development (Local Planning Schemes) Regulations 2015;*
- *City of Vincent Local Planning Scheme No. 2;*
- *Community and Stakeholder Engagement Policy;*
- *Policy No. 7.1.1 – Built Form Policy;*
- *Policy No. 7.5.7 – Licensed Premises;*
- *Policy No. 7.5.21 – Sound Attenuation;*
- *Policy No. 7.7.1 – Non-Residential Development Parking Requirements; and*
- *Western Australian Planning Commission – Delegation 2022/03 – Powers of Local Governments Metropolitan Region Scheme.*

Planning and Development Act 2005

In accordance with Schedule 2, Clause 76(2) of the *Planning and Development (Local Planning Schemes) Regulations 2015* and Part 14 of the *Planning and Development Act 2005*, the applicant would have the right to apply to the State Administrative Tribunal for a review of Council's determination.

Local Planning Scheme No. 2

In considering the appropriateness of the use, Council is required to consider the objectives of the District Centre zone under LPS2, as follows:

- *To provide a community focus point for people, services, employment and leisure that are highly accessible and do not expand into or adversely impact on adjoining residential areas.*
- *To encourage high quality, pedestrian-friendly, street-orientated development that responds to and enhances the key elements of each District Centre, and to develop areas for public interaction.*
- *To ensure levels of activity, accessibility and diversity of uses and density is sufficient to sustain public transport and enable casual surveillance of public spaces.*
- *To ensure development design incorporates sustainability principles, with particular regard to waste management and recycling and including but not limited to solar passive design, energy efficiency and water conservation.*
- *To ensure the provision of a wide range of different types of residential accommodation, including affordable, social and special needs, high density residential and tourist accommodation, to meet the diverse needs of the community.*
- *To provide a broad range of employment opportunities to encourage diversity and self-sufficiency within the Centre.*
- *To encourage the retention and promotion of uses including but not limited to specialty shopping, restaurants, cafes and entertainment.*
- *To ensure that the City's District Centres are developed with due regard to State Planning Policy 4.2 – Activity Centres for Perth and Peel.*

Delegation to Determine Applications:

The matter is being referred to Council for determination in accordance with the City's Register of Delegations, Authorisations and Appointments.

This is because the delegation does not extend to applications for development approval that propose a Tavern land use with more than 120 patrons. The application proposes a Tavern land use with a maximum of 350 patrons.

RISK MANAGEMENT IMPLICATIONS:

There are minimal risks to Council and the City's business function when Council exercises its discretionary power to determine a planning application.

STRATEGIC IMPLICATIONS:

This is in keeping with the City's Strategic Community Plan 2022-2032:

Innovative and Accountable

Our decision-making process is consistent and transparent, and decisions are aligned to our strategic direction.

SUSTAINABILITY IMPLICATIONS:

The City has assessed the application against the environmentally sustainable design provisions of the City's Policy No. 7.1.1 – Built Form. These provisions are informed by the key sustainability outcomes of the City's Sustainable Environment Strategy 2019-2024, which requires new developments to demonstrate best practice in respect to reductions in energy, water and waste and improving urban greening.

PUBLIC HEALTH IMPLICATIONS:

This is not in keeping with the following priority health outcomes of the City's *Public Health Plan 2020-2025*:

Reduced harmful alcohol use

FINANCIAL/BUDGET IMPLICATIONS:

There are no finance or budget implications from this report.

COMMENTS:Summary Assessment

In assessing the application against the planning framework, it is recommended for approval. The following key comments are of relevance:

- The Tavern land use is contemplated in the District Centre zone and the City's Licensed Premises Policy recommends that licensed premises such as this be concentrated within Town Centre areas.
- The proposed use would contribute to the level of activity and diversity of uses within the William Street Town Centre. The proposal would re-activate a vacant site and would contribute to passive surveillance of public spaces throughout the day.
- The proposed parking shortfall is appropriate because there is sufficient capacity within existing parking facilities and availability of high frequency public transport options that are in close proximity to the site. A partial payment-in-lieu of car parking contribution equivalent to four bays is recommended to assist the City in managing public parking facilities that would be utilised by those attending the proposed development.
- The shortfall in long-term bicycle bays is acceptable as the development would provide an overall surplus of bicycle parking bays, with the short-term bays suitably located within the subject site in a location that can be monitored by staff and easily accessed by patrons of the venue.
- The building design of the proposed development including the façade design and public domain interface would provide suitable articulation and visual interest when viewed from William Street. The interface with the public realm would be sympathetic to the existing streetscape character and was supported by the Chairperson of the City's DRP.
- The development would deliver a high-quality landscaping outcome which includes three trees and smaller plantings across the site that would make an effective contribution to the City's green canopy, and the reduction the impact of the urban heat island effect on the site.

A more detailed assessment against the discretionary aspects of the application is set out below. These relate to consideration against LPS2 and relevant local planning policies.

Land Use

Tavern is an 'A' use within the District Centre zone. This means that the use is not permitted unless Council exercises discretion by approving a development application for the use following consultation with the community.

In considering the appropriateness of the use, due regard is to be given to the objectives of the District Centre zone under LPS2.

The proposal is consistent with the [objectives](#) of the District Centre zone under LPS2 for the following reasons:

- Contribution to Town Centre: The Tavern would provide an entertainment venue that would contribute to the level of activity and the diversity of uses within the William Street Town Centre. The proposal would contribute to the existing retail and hospitality uses within the area and would support employment opportunities. The ground floor design provides an open streetscape presence which encourages engagement and visual interaction with the street. This is achieved through the location of alfresco dining in the front setback area with visually permeable fencing, along with low planting and landscaping to facilitate engagement with the street. The café and record studio provides large open windows facing the street to increase surveillance and an engagement with the street.

- Site Location and Alternate Modes of Transport: The site is highly accessible by pedestrians and by public transport. The area provides a high level of walkability, and the site is located adjacent to a high frequency bus route stop on William Street and approximately 700 metres of the Perth Train Station.
- Streetscape Character: The development proposes alterations and additions to the existing building which would be sympathetic to the existing streetscape character. The proposal would include an alfresco area located adjacent to the public realm and would provide a high amenity space that would contribute street level activity and vibrancy for the local community. The proposed building façade design of the William Street elevation and its materiality have been reviewed by the Chairperson of the City's DRP and would be consistent with the surrounding streetscape.
- Passive Surveillance: The application proposes multiple complementary land uses which are proposed to operate on the subject site throughout the day and would contribute to passive surveillance of public spaces. The proposed ground level streetscape interface of the development is active with visually permeable front fencing and an alfresco area located adjacent to the William Street frontage which would enable surveillance of streetscape.
- Site Activation: The proposed development would re-activate the site which has been vacant for several years. The proposed land uses would contribute positively to vibrancy and both daytime and night-time activity of William Street.
- Consistency with Licensed Premises Policy: The City's Licensed Premises Policy sets out that the District Centre zone to be the most compatible area for this type of use and recommends that licensed premises be concentrated within Town Centre areas. The location of the development within the William Street Town Centre is consistent with the provisions of this Policy.
- Sustainability: The proposal would include an adaptive re-use of the existing building on-site. This supports prolonging the building's life by using the premises for a new purpose and is consistent with sustainability principles. Additional environmental sustainability measures that have been incorporated into the proposed development is further detailed in the Environmental Sustainability Design section below.
- Impact on Residential Areas: The development provides suitable measures to mitigate any adverse impacts on residential properties in proximity to the subject site. The development focuses patron activity on William Street, with acoustic noise walls and back of house facilities providing suitable separation to residential properties on Money Street. The nearest residential property is located adjacent to the subject site to the south along Money Street. A lodging house is adjoining the subject site to the south-east. The Acoustic Report submitted by the applicant, included as **Attachment 8**, demonstrates that the development would be compliant with the *Environmental Protection (Noise) Regulations 1997*.
- Waste Management: The applicant has submitted a Waste Management Plan, included as **Attachment 9**, in accordance with the City's Waste Guidelines for New Developments. The Waste Management Plan confirms the waste from the premises would be able to be appropriately managed with private collection. A condition of approval is included in the officer recommendation requiring compliance with the Waste Management Plan and collection of waste during daytime hours to ensure compliance with the prescribed noise limits in the *Environmental Protection (Noise) Regulations 1997* (Noise Regulations).
- Venue Management: The City's Licensed Premises Policy requires the submission of a venue management plan for a tavern land use. The applicant has submitted a Venue Management Plan included as **Attachment 6**. Venue management plans inform the operation of premises to minimise the impacts of noise and patron behaviour on the amenity of the surrounding area. The submitted Venue Management Plan includes guidance on the management of patron behaviour and noise. These measures include communication with entertainment providers to ensure compliance with the noise requirements of the venue and the creation and maintenance of a complaints register to monitor offence, noise or disturbance issues. The Venue Management Plan ensures that the premises could be operated so as not to unduly impact the amenity of the area. A condition of approval is included in the officer recommendation requiring the premises to operate in accordance with the approved Venue Management Plan.

Car Parking

The venue is proposed to have a capacity of 365 persons, comprised of 350 patrons and 15 staff.

Under the City's Non-Residential Parking Policy, the development would require a total of 55 on-site car parking bays.

No parking bays would be provided on-site, resulting in a 55 bay shortfall.

Applicant's Transport Impact Assessment and Parking Management Plan

As part of the proposal the applicant submitted a Traffic Impact Assessment (TIA) prepared by Move Consultants and a Parking Management Plan (PMP) which are included as **Attachment 4** and **5**. The TIA and PMP outline the following:

- Operations as a Licensed Venue: The applicant does not anticipate the development to operate as a typical hospitality establishment due to the site constraints associated with the inner-city location. The TIA details that the site is afforded access to high quality public transport, and pedestrian and cycling infrastructure, along with changing public attitudes associated with drinking and driving and the increased popularity and accessibility ride share services.
- Trip Generation and Car Parking Demand: The trip generation and mode split data for the proposal was calculated within the TIA to reflect the location of the proposal on a high frequency bus route and within a significant urban catchment which would attract a high proportion of walk-in trade. The data within the TIA indicates that:
 - The venue is not expected to approach full capacity (350 patrons) at any given time due to the staggered nature of the indicative operating times of different portions of the proposed development. Due to these varied operating times, the applicant's TIA anticipates that the maximum expected occupancy at any given time is 75 percent, being 262 patrons.
 - Based on ABS Census data as well as established mode split and travel patterns associated with similar approved developments, the TIA assumes that 40 percent of trips would be via private motor vehicle. The TIA assumes the remaining 60 percent of trips would be via public transport, walking, cycling or ride-share.
 - The anticipated peak periods of the development for trip generation would be Friday 12:00pm to 2:00pm and 8:00pm to 10:00pm, and Saturday 7:00pm to 8:00pm. Based on the assumed mode split for the proposal, the development would generate a total of approximately 118 private vehicle trips (including arrivals and departures) daily. During the anticipated peak periods the development would generate 7 vehicle trips per hour during the Friday daytime and Saturday evening periods, and 18 vehicle trips per hour during the Friday evening period.
 - The surveyed peak period of the development for car parking demand and supply would be Friday 10:00am to 2:00pm and 5:00pm to 10:00pm, and Saturday 5:00pm to 10:00pm. The development would likely generate demand for between 1 to 4 car parking bays during a typical Friday daytime peak hour between 10:00am and 2:00pm, and between 10 to 14 car parking bays during a typical Friday or Saturday evening peak hour between 5:00pm and 10:00pm.
 - Based on the results of the car parking survey detailed below, the car parking demand could be accommodated within the existing on-street car parking supply within a 400 metre walking distance of the site. This distance is considered the acceptable standard for a walkable catchment with 400 metres being reflective of a five-minute walk.
- Service Bays: The development does not propose any vehicle access to the site, with the existing crossover to William Street proposed to be removed. This would facilitate the creation of one new on-street parking space. The applicant proposes for this new space and the existing on-street car parking space adjacent to the subject site to be designated as drop-off/pick-up bays for ride share and delivery vehicles.
- Provision of Bicycle Parking: A total of 24 short term bicycle parking spaces are proposed to be provided on-site for staff and patrons with end-of-trip facilities and lockers available for staff.
- Promotion of Alternative Transport Options: A restriction of on-site parking supply is known to be a primary factor in promotion of mode shift away from car-as-driver transport and towards patrons utilising available alternative modes transport including carpooling, public transport and cycling. The location of the subject site in close proximity to major public transport and other non-motorised transport infrastructure for these alternative modes will assist in the transition towards more sustainable transport in the area.
- Traffic Generation: Based on a review of the modelled total traffic assessment and observed traffic operations of the surrounding road network, the anticipated site-generated traffic associated with the redevelopment proposal is negligible and no improvements to the surrounding road network will be required.
- Servicing: Rubbish collection and service, delivery, and loading activities can be accommodated along the existing William Street frontage during off-peak periods within the existing on-street parallel parking supply.
- Public Transport: The subject site is located directly on high frequency bus routes on William Street including the 980 route, with additional bus routes available.

The applicant’s TIA included in **Attachment 4** provides a public car parking survey This demonstrates the availability of public car parking bays within a 300 metre to 400 metre radius of the subject site and which is reflective of an average five-minute walk, as shown in the image below. The survey was undertaken during the anticipated peak operating times for the development. These times were on Friday 28 July 2023 10:00am to 2:00pm and 5:00pm to 10:00pm, and Saturday 29 July 2023 from 5:00pm to 10:00pm.



Figure 1: Car Parking Survey Locations (Source: Transport Impact Assessment)

The survey results are summarised as follows:

- There are 302 existing public car parking bays within the car parking survey locations.
- An average parking occupancy rate of 92.2 percent and a maximum of 95.7 percent during the Friday daytime peak period, with a minimum of 13 available public car parking bays.
- An average parking occupancy rate of 77.6 percent and a maximum of 94 percent during the Friday evening peak period, with a minimum of 18 available public car parking bays.
- An average parking occupancy rate of 88.1 percent and a maximum of 98 percent during the Saturday evening peak period, with a minimum of 6 available public car parking bays.

The applicant’s TIA also includes consideration of the recently constructed public SecurePark parking facility at No. 29 Lindsay Street that is located 100 metres away from the subject site. The car park is not yet operational. Following commencement of its operation, the car park would be privately operated and would be open to the public, subject to fees. The facility would provide an additional 43 bays which would be available for paid public use between 7:00am and 10:00pm. Administration has not included these bays in the assessment of the application as there is no certainty as to when the car park would become operational.

City of Vincent On-Street Car Parking Data

Administration sought to verify the applicant’s parking survey results by undertaking a review of the City’s 2018 Street Parking Survey data.

A review of the survey data relating to William Street, Robinson Avenue, Forbes Road, Monger Street, Lindsay Street and Money Street has been undertaken. The City’s 2018 Street Parking Survey data reviewed was conducted from 9:00am to 8:00pm on Wednesday 18 November 2018, Friday 30 November 2018 and Saturday 1 December 2018.

The survey data results are set out in the table below. The information for Friday and Saturday has been provided to demonstrate the maximum occupancy during the time periods that are most representative of the applicant’s peak period survey data. The information for Wednesday has been provided to demonstrate the average occupancy of the available car parking in the area throughout the midweek operating period for the development.

		Wednesday 18 November 2018 (all day)	Friday 30 November 2018 (midday period)	Friday 30 November 2018 (evening period)	Saturday 1 December 2018 (evening period)
	Total Bays	Average Occupancy	Maximum Occupancy		
William Street between Bulwer Street and Newcastle Street	76	61%	92%	86%	61%
Robinson Avenue between Lake Street and Lindsay Street	53	50%	64%	58%	53%
Forbes Road between Lake Street and William Street	31	51%	74%	68%	55%
Monger Street between William Street and Beaufort Street	41	42%	49%	44%	54%
Lindsay Street between Brisbane Street and Newcastle Street	50	69%	72%	84%	78%
Money Street between Monger Street and Newcastle Street	45	73%	89%	91%	76%

The applicant's TIA outlines that the Friday and Saturday evening peak periods for the venue would be from 5:00pm to 10:00pm. The City's 2018 Street Parking Survey was conducted from 9:00am to 8:00pm. This means it does not coincide directly with the peak periods of the venue, although it could be reasonably expected that likely car parking demands on car parking availability would be similar between the 8:00pm to 10:00pm period.

Acceptability of the Proposed Car Parking Shortfall

The proposal would be consistent with the [element objectives](#) of the Built Form Policy and [objectives](#) of the Non-Residential Parking Policy for the following reasons:

- **Car Parking Demand:** The applicant's TIA identifies that, except for the Friday and Saturday evening peak periods, the maximum car parking demand for the proposed development would be four bays. This would occur during the Friday daytime peak period. For all other operating hours, it is expected that fewer than four car parking bays would be required. The venue is proposed to operate for 117 hours per week. Of this, 10 operating hours would be within the Friday and Saturday evening peak periods. During the remaining 107 hours, four car parking bays would be adequate to satisfy the anticipated parking demands generated by the proposed development. A condition of approval for a payment-in-lieu contribution for these four bays is included in the officer recommendation.
- **Car Parking Availability:** The applicant's car parking survey and the City's 2018 Street Parking Survey data demonstrate that this demand for four car parking bays would be capable of being accommodated by the existing public car parking bays in the area.
- **Car Parking Availability at Peak Periods:** It is expected that the use of private vehicles to access the proposed development and the associated need to park in the area would reduce during peak periods. This is because the proposed development is located in an inner-city entertainment district that has limited on-street parking availability during these times.
- **Consistency with other Venues:** Limited staff and patron parking is provided on-site for the majority of venues within the William Street Town Centre. These venues include nearby supermarkets, restaurants, cafés and existing licensed venues such as 'Bar 399', 'Wines of While', 'Vincent Wine', 'Hotel Northbridge', 'The Brisbane Hotel' and 'Tom's Providore and Wine Bar'. This is because these sites have been predominantly built out and have existing buildings, with limited area on-site to provide for additional parking. This is common in the inner-city context where existing buildings are retained and the use of the buildings have been adapted and changed.

- Impact of Car Parking Demand: The additional demand for car parking that would be generated by the proposed development would inevitably reduce the current availability of public parking bays in the locality. A payment-in-lieu contribution is recommended to address this, as detailed below in the Payment-in-lieu of Car Parking Contribution section of this report.
- Location and Ability for Multi-Purpose Trips: The subject site is located within the William Street Town Centre which is a high traffic area for both pedestrians and vehicles with a wide range of services available. This location provides the opportunity for multi-purpose trips for people visiting the site and other venues within the precinct.
- Walkability of the Area: The area is highly walkable, with William Street providing a main pedestrian route to Perth CBD. The area offers high levels of pedestrian amenity through awnings along William Street providing weather protection and the availability of footpaths.
- Nature of the Use: The nature of a Tavern use, being a licensed premises, is such that it is expected that a large proportion of patrons would choose not to drive to the venue in private vehicles and to instead opt for taxi, ride-share, public transport, walking or other alternate modes of transport. The applicant's TIA anticipates that 60 percent of patrons will travel to the venue by alternative modes of transport, with 40 percent of patrons travelling to the venue via private motor vehicle (15 percent of these being drivers and 25 percent being passengers). This would have the effect of reducing the car parking demand for the development.
- Availability of Public Transport: The subject site is located on William Street which is a high frequency bus route with high levels of bus frequency throughout the day. The Perth Train Station is approximately 700 metres from the subject site and provides connections to the Perth rail network and the wider Perth metropolitan area. The free 'Blue CAT' is approximately 300 metres from the subject site and provides connections to Perth Underground, the Esplanade Busport and Barrack Street Jetty. The area has a high level of pedestrian amenity which would support the use of alternative means of transport to the site.
- Varying Operating Hours: It is intended for the different portions of the development to have varying operating hours, as detailed in the Proposal section of this report. The staggered operating hours that are intended to suit each of the different portions of the development would assist in reducing the overall car parking demand generated by the development at any one time.
- Promotion of Alternative Transport Options: A provision of no on-site car parking would be consistent with the applicant's intended approach based on the nature of the proposed land uses to support a shift towards more sustainable modes of transport. The applicant has stated that staff would be encouraged to use alternative transport modes such as walking, cycling and public transport given the site is highly accessible.
- Bicycle Parking: Bicycle parking and end of trip facilities are proposed to support active modes of transport. This is discussed in further detail in the 'Bicycle Parking' section of this report. The subject site is also in close proximity to Forbes Road which is designated as a 'good road riding environment' under the Department of Transport's Perth Bike Map.
- Traffic Generation and Car Parking Demand: The applicant's TIA demonstrates that the proposed development would not result in an unreasonable amount of traffic generation and would not be expected to contribute traffic congestion on surrounding residential streets. The TIA sets out that the development would likely generate demand between 1 to 4 car parking bays for private vehicles during a typical Friday daytime peak hour, between 10:00am and 2:00pm, and between 10 to 14 car parking bays for private vehicles during a typical Friday and Saturday evening peak hour between 5:00pm and 10:00pm. This is based on the private vehicle trips generated by the venue during peak periods contained in the TIA, the accessibility of the site via alternate modes of transport and the availability of public car parking in the surrounding area.
- Service Bay: The removal of the existing crossover William Street would facilitate the creation of one new on-street parking space. This new space would be able to be used as a pick-up/drop-off on-street car parking bay that would be available to service the proposed development and other venues along the street. This bay would be able to be utilised for rideshares and would improve the accessibility of the proposed development.

Payment-in-lieu of Car Parking Contribution

The Non-Residential Parking Policy outlines that payment-in-lieu of car parking may be required as a mechanism to enable otherwise desirable developments to proceed, where it can be demonstrated that it is not possible to provide sufficient parking on-site.

The current rate of payment-in-lieu contribution set out in the City's 2023/24 Fees and Charges is \$5,626.80 per bay. This would equate to a payment-in-lieu contribution of \$309,474.00 based on a 55 bay shortfall.

The applicant has provided the following written justification in support of the proposal for approval of the 55 bay car parking shortfall without the requirement for a payment-in-lieu contribution:

- Site Location: The location of the site within an urban/inner-city environment will attract significant levels of walk-in/cycle-in/local/multi-purpose trips within the catchment and will result in public transport use.
- Availability of Car Parking: The availability of public on-and-off street car parking supply within 300 metres to 400 metres walking distance of the site.
- Public Parking Supply: The results of the parking surveys provided in the TIA indicate a surplus in the public parking supply during the peak demand periods associated with the proposal which can more than adequately cater to the maximum anticipated car parking demands associated with these periods.
- Consistency with Planning Policy: This proposed approach is consistent with the City's Built Form Policy, the WAPC's Development Control Policy 1.6 – Planning to Support Transit Use and Transit Oriented Development, and the State Government's Draft Interim Guidance for Non-Residential Car Parking as well as 'best practice' in the planning for, and provision of, car parking infrastructure.

The applicant has also advised that in the instance that a payment-in-lieu of car parking contribution is determined to be appropriate by the decision maker, it should be a partial payment-in-lieu contribution equivalent to two car parking bays. This is to enable two pick-up/drop-off bays to be provided on-street within the existing parallel parking supply near the north-west boundary of the site on the east side of William Street.

Administration does not consider that a total concession of on-site car parking provision without the requirement for a payment-in-lieu contribution is appropriate in this instance. This is because the applicant's TIA demonstrates that it is not possible for the proposed development to provide sufficient parking on-site and would have a reliance on the City's existing public car parking facilities to support the demand of those attending the site via private vehicles.

Should the application be approved, it is recommended that a condition be imposed requiring a partial payment-in-lieu of car parking contribution equivalent to four bays. This would equate to \$22,507.20. The City recommends this for the following reasons:

- Impact on Public Car Parking: The additional demand for car parking that would be generated by the proposed development would be capable of being accommodated by existing public car parking facilities but would inevitably reduce the current availability of public parking bays in the locality. The development would contribute towards increasing the occupancy rates of existing public car parking bays. A payment-in-lieu contribution would assist the City in managing and maintaining these public car parking facilities that would support the demands of the proposed development and ensure that it would not have an adverse impact on the surrounding area with respect to the availability of public car parking.
- Venue Operations: As set out in the Applicant's TIA, the venue is not expected to operate at maximum capacity for the total operating hours due to the staggered nature of the operating hours proposed for the different spaces with a maximum peak occupancy of 75 percent.
- Payment-in-lieu Calculation: The payment-in-lieu contribution should not reflect the anticipated car parking demand at its maximum peak period or the lowest off-peak period. Rather, it would be reasonable for the payment-in-lieu contribution to reflect the maximum anticipated demand for car parking associated with the venue's operation outside of the Friday and Saturday evening peak periods. The applicant's TIA establishes that this demand would be for four car parking bays during the Friday daytime peak period.

Bicycle Parking

Under the City's Non-Residential Parking Policy, the development would require a total of seven short-term and 16 long-term bicycle parking spaces.

24 short-term bicycle bays and no long-term bicycle bays are proposed. This results in a 17 bay short-term surplus and a 16 bay long-term shortfall.

The bicycle parking proposal is acceptable and would satisfy the [objectives](#) of the Non-Residential Parking Policy for the following reasons:

- **Location of Bays:** The development would provide a surplus of bicycle parking bays overall. The shortfall in long-term bicycle bays is acceptable, as the short-term bays are suitably located within the subject site and in a location that can be monitored by staff and patrons of the venue. This will provide suitable level of security for the on-site bicycle parking that would meet the expectations of customers and staff seeking to travel to the development by bicycle.
- **Sustainability:** The development would support a shift towards active and sustainable transport modes by staff and patrons of the venue. This is consistent with the objectives of the Non-Residential Parking Policy and would assist in mitigating the impacts of the development providing no on-site car parking.
- **End of Trip Facilities:** An end-of-trip facility for staff is proposed within the existing building to enable the use of alternative transport options.
- **Varied Operating Hours:** The intended varying operating hours for each of the three spaces within the venue would assist in reducing the overall bicycle parking demand generated by the development.

Façade Design & Public Domain Interface

The proposed façade design and interface with the public realm would satisfy the [element objectives](#) of the Built Form Policy for the following reasons:

- **Consistency with the Street:** The proposed building addition located to the front of the site would be proportionate in scale to the existing building and would provide articulation to the street. The roofline does not exceed that of the existing building and the ceiling height does not affect its function as a commercial space. The existing streetscape contains a mixed typology of buildings and the heights of the proposed addition tie in with the existing commercial developments on William Street.
- **Building Scale:** The streetscape presentation provides a strong visual contrast between the existing building and the contemporary style and materials used for the proposed addition. The proposed building façade massing is well proportioned to provide streetscape presence without resulting in building bulk and scale.
- **Materiality:** The proposed building façade is well articulated using a range of contrasting colours, materials and finishes that are complementary with the surrounding streetscape and provide visual interest as viewed from the public realm. This includes face brickwork, galvanised steel cladding and roof sheeting, engineered timber, concrete, painted timber windows, and painted steel. The diverse range of materiality derived from the surrounding context was supported by the DRP Chairperson. The inclusion of expansive wraparound glazing on the proposed building addition would allow for visual engagement and views through the building to the beer garden area.
- **Landscaping:** The soft landscaping provided adjacent to the street would enhance the amenity of William Street. The landscaping would soften the proposed materiality of the development and provide contrast and visual interest to the existing streetscape context.
- **Surveillance and Activation:** The building would express the internal functions of the space by providing large areas of glazing to the street and western elevation of the building. This would assist in increasing the extent of casual surveillance to and from the street in addition to that provided by the front alfresco dining area.
- **Awning:** The William Street streetscape contains a range of commercial developments that provide different building typologies and setbacks to the street. This has resulted in buildings that do provide awnings to the street as well as several existing commercial developments that do not provide an awning to William Street. The proposed development would be consistent with the existing commercial developments that do not provide an awning to William Street. The existing building and proposed addition that form part of the development are setback from the street which provides a space for activity within the alfresco area in the front setback. This limits the opportunity and ability for an awning to be provided which extends over the footpath area along the street. The design of the proposed addition would provide a suitable urban edge to the streetscape without the provision of an awning. The parasol roof gutter which extends for the length of the William Street frontage would present similarly to an awning and provide visual interest from the street. The canopy of the existing mature trees in the verge also provides some weather protection for pedestrians.

- **Services:** The proportion of the façade taken up by the bin store and rainwater tank would not have an impact on the streetscape presentation of the development. This is because they are located towards the side boundaries of the site and occupy limited portions of the street frontage. This would ensure that they do not restrict views and engagement with the active street frontage. The visual impact of the rainwater tank would be mitigated through the provision of landscaping to provide a visual buffer to the street. The bin store would be suitably screened with the walls integrated into the design of the development and is setback approximately 5 metres from the street.
- **DRP Support:** The DRP Chairperson advised that the proposed façade design is supported with regard to the 'Context and Character', 'Built Form and Scale' and 'Aesthetics' design principles.

Landscaping:

The proposed landscaping would satisfy the [element objectives](#) of the Built Form Policy for the following reasons:

- **Streetscape Contribution:** The development would deliver a high quality landscaping outcome which facilitates streetscape activation and engagement with the alfresco dining area. The setback provided by the development allows for the provision of deep soil areas which include a tree and small plantings within these areas. This landscaping would assist with softening the impact of the proposed built form on the street.
- **Tree Canopy:** The application proposes three new trees across the subject site, located in appropriately sized deep soil areas to make an effective contribution to the City's green canopy and the amenity of William Street. The location of these trees would provide shading to the street and areas of the alfresco dining spaces which are not provided with permanent roof covering. The development would also provide vines and smaller planting across the site to assist in reducing the impact of the urban heat island effect.
- **Internal Amenity:** Landscaping is proposed in different areas across the site which would increase the amenity for visitors and users of the development. This includes the beer garden area which provides a lawn area, trees and climbing vines over the space to provide shade and plantings that would contribute to visual amenity.
- **Landscaping Plan:** The applicant proposes to provide an amended landscaping plan with specific tree species, pot sizes and a reticulation for the City's approval prior to the issue of a building permit. Should the application be approved, it is recommended that a condition be imposed requiring the submission of an amended detailed landscaping and reticulation plan prior to the issue of a building permit. It is recommended that the amended detailed landscaping plan include the provision of trees that are in accordance with the City's recommended species and contribute towards canopy coverage within the deep soil areas and planting areas.

Acoustic Report

The applicant submitted an Acoustic Report prepared by a qualified acoustic consultant in support of the proposal which is included as **Attachment 8**. The report assesses noise generated from the proposed use and its impact on surrounding properties.

The acoustic report identifies that music and patron noise from the proposed development have been modelled and assessed against the assigned levels of the *Environmental Protection (Noise) Regulations 1997*. The report provides the following discussion and recommendations:

- Compliance is predicted for all combined operations during daytime, evening or Sundays.
- Absorptive panels are required to be fitted to the internal surfaces of the outdoor bar and lounge to reduce the reverberation time of the venue when occupied.
- All external noise emissions have been predicted on the basis of a 6.0-metre-high boundary barrier built to contain noise from the garden bar and lounge bar noise breaking out through the entry doors. This has been provided in the development plans, included as **Attachment 2**.
- Music would be able to be played at the higher range of background listening levels at all times but should not be dominant of noise emissions for extended periods of time.
- Loudspeakers shall be installed with appropriate resilient mounts to stop vibration being transmitted to the building structure and should point inwards to the centre of the indoor and outdoor bars.
- A noise measuring/management device shall be set up to provide visual or electronic feedback to venue operators that the required noise levels are maintained at all times.
- Waste disposal and goods delivery shall be limited to daytime hours (between 9:00am and 7:00pm, Monday to Saturday).

The acoustic report confirms that noise levels generated from the premises during the proposed operating hours would comply with the relevant assigned noise levels under the *Environmental Protection (Noise) Regulations 1997*. The proposed use would not result in an adverse impact on the use and amenity of the surrounding area in relation to noise.

To ensure noise is appropriately managed and would not unduly impact the amenity of the surrounding area, the City recommends the following conditions of approval be imposed:

- The submission of an updated acoustic report to be provided prior to issuing a Building Permit to address mechanical and plant equipment selected through the detailed design of the development.
- The implementation of the assumptions and recommendations in the approved Acoustic Report prior to the occupancy or use of the development and maintenance of these for the life of the development.
- The operation of the venue in accordance with the approved Venue Management Plan which includes noise management measures.

Notwithstanding the recommended conditions, the venue would also be subject to comply at all times with the requirements of the *Environmental Protection (Noise) Regulations 1997*.

Environmentally Sustainable Design

Clause 1.17 of the Built Form Policy relating to Environmentally Sustainable Design (ESD) sets out acceptable outcomes to be achieved for commercial development. This is for an ESD report to be provided that includes an assessment against the Green Building Council of Australia's Green Star Rating System Report or Life Cycle Assessment.

The applicant has provided an ESD Statement which does not include a rating under the Green Building Council of Australia's Green Star Rating System Report or a Life Cycle Assessment. The ESD Statement is included as **Attachment 10**.

The Statement identifies the following built form, site planning and construction measures that have been incorporated into the proposed addition and that would satisfy the [element objectives](#) of the Built Form Policy in respect to environmentally sustainable design. These include:

- Building Waste: Retention and preservation of most of the existing building on-site with existing materials and construction waste to be reused/recycled to minimise building waste.
- External Shading and Heat Absorption: A six metre high wall is proposed to be constructed along the northern boundary of the site to provide external shading and limit heat absorption. The wall function as a sound barrier and would be painted a light colour/white to reduce heat island effect.
- Natural Light and Ventilation: Additional windows and glazing are proposed to the northern elevation of the existing building to maximise use of natural light and winter sun for passive heating. Operable windows would enable cross ventilation to supplement or replace the artificial cooling in summer.
- Roof Design: The roof space is proposed to remain clear of plant and equipment to maximise space for solar panels.
- Solar Absorptance: Zinalume roof sheeting is proposed. A condition of approval included in the officer recommendation to secure this and to ensure its solar absorptance rating is not more than 0.4, consistent with the acceptable outcome standard of the Built Form Policy.

As the applicant provided an ESD Statement that identified the above measures, the City did not require an ESD report to be provided as part of the development application. The proposal includes adaptive re-use of the existing commercial building which limits the whole of life environmental impact of the development. The DRP Chairperson advised that the proposed development, including the ESD report provided, is supported with regard to the 'Sustainability' design principle.

**CITY OF VINCENT
LOCAL PLANNING SCHEME NO. 2
SCHEME MAP 1 - LEEDERVILLE**

LEGEND

METROPOLITAN REGION SCHEME RESERVES

Note: The Western Australian Planning Commission (in care of the Department of Planning) should be consulted for full information on the actual land requirements for all Metropolitan Region Scheme Reserves.

- PARKS AND RECREATION
- R Restricted Public Access
- PRIMARY REGIONAL ROADS RESERVATION
- OTHER REGIONAL ROADS RESERVATION
- RAILWAYS
- PUBLIC PURPOSES
Particular use denoted as follows:
- H Hospital
- HS High School
- TS Technical School
- CP Car Park
- U University
- CG Commonwealth Government
- SU Special Use
- WSD Water Authority of Western Australia
- P Prison

CITY OF VINCENT LOCAL SCHEME RESERVES

- PUBLIC OPEN SPACE
- R Restricted
- PUBLIC PURPOSES
Particular use denoted as follows:
- PS Primary School
- CP Car Park
- CU Civic Uses
- HS High School
- I Institute for the Deaf
- W Water Supply Sewerage and Drainage
- TS Technical School

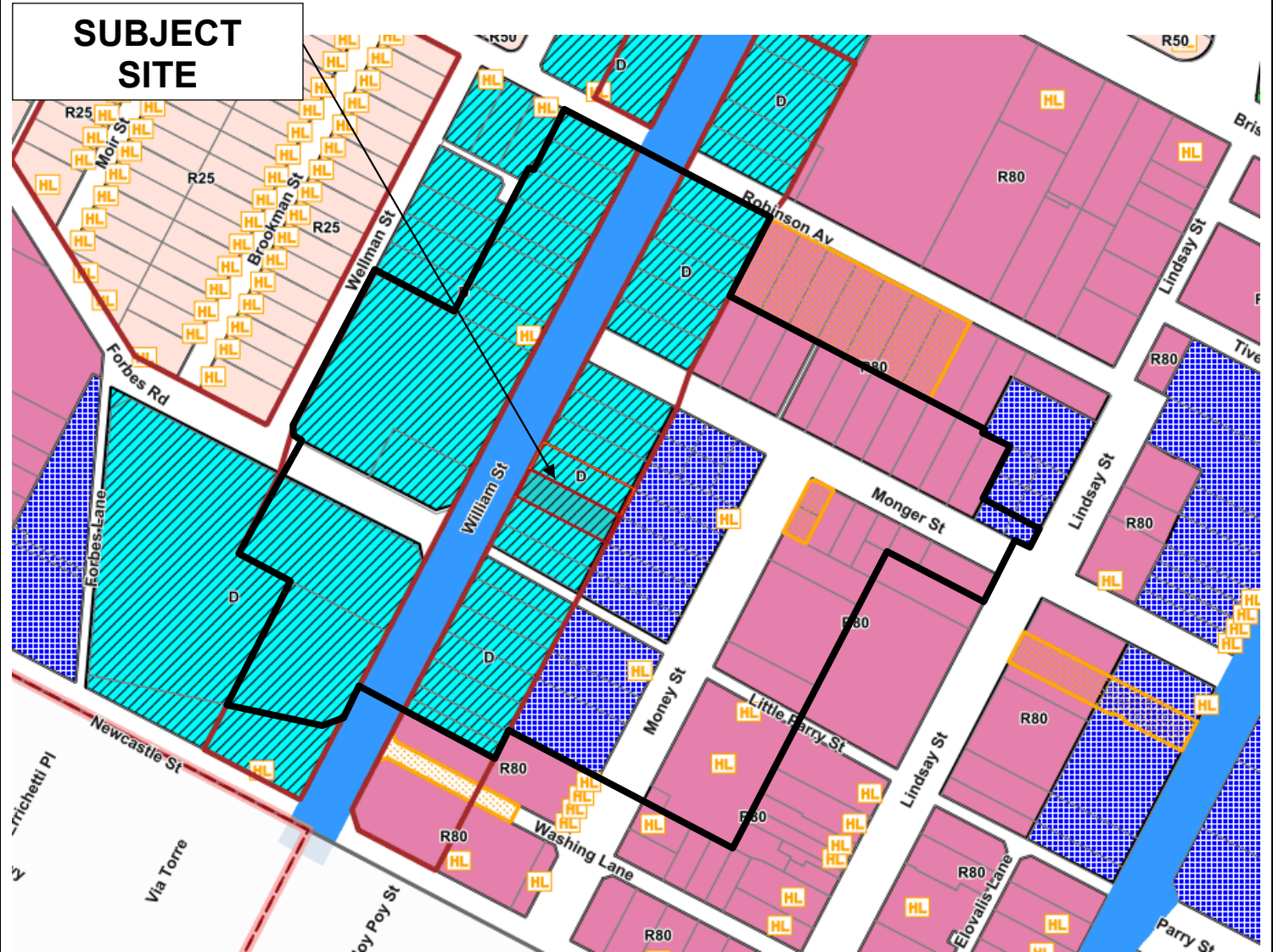
CITY OF VINCENT SCHEME ZONES

- RESIDENTIAL
- MIXED USE
- COMMERCIAL
- LOCAL CENTRE
- DISTRICT CENTRE
- REGIONAL CENTRE
- SPECIAL USE
Particular use denoted as follows:
- CP Car Park
- CU Community Use
- FC Function Centre
- HC Hall and Non Residential Club
- H Hotel
- PW Place of Worship
- S Service Station

ADDITIONAL USE

ADDITIONAL INFORMATION & RESIDENTIAL PLANNING CODES

- CODE AREA BOUNDARY
- SCHEME AREA BOUNDARY
- DENSITY CODE



CITY OF VINCENT

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Consultation and Location Map
Nos. 378 and 380 William Street, Perth

Extent of Consultation



**SUBJECT
SITE**

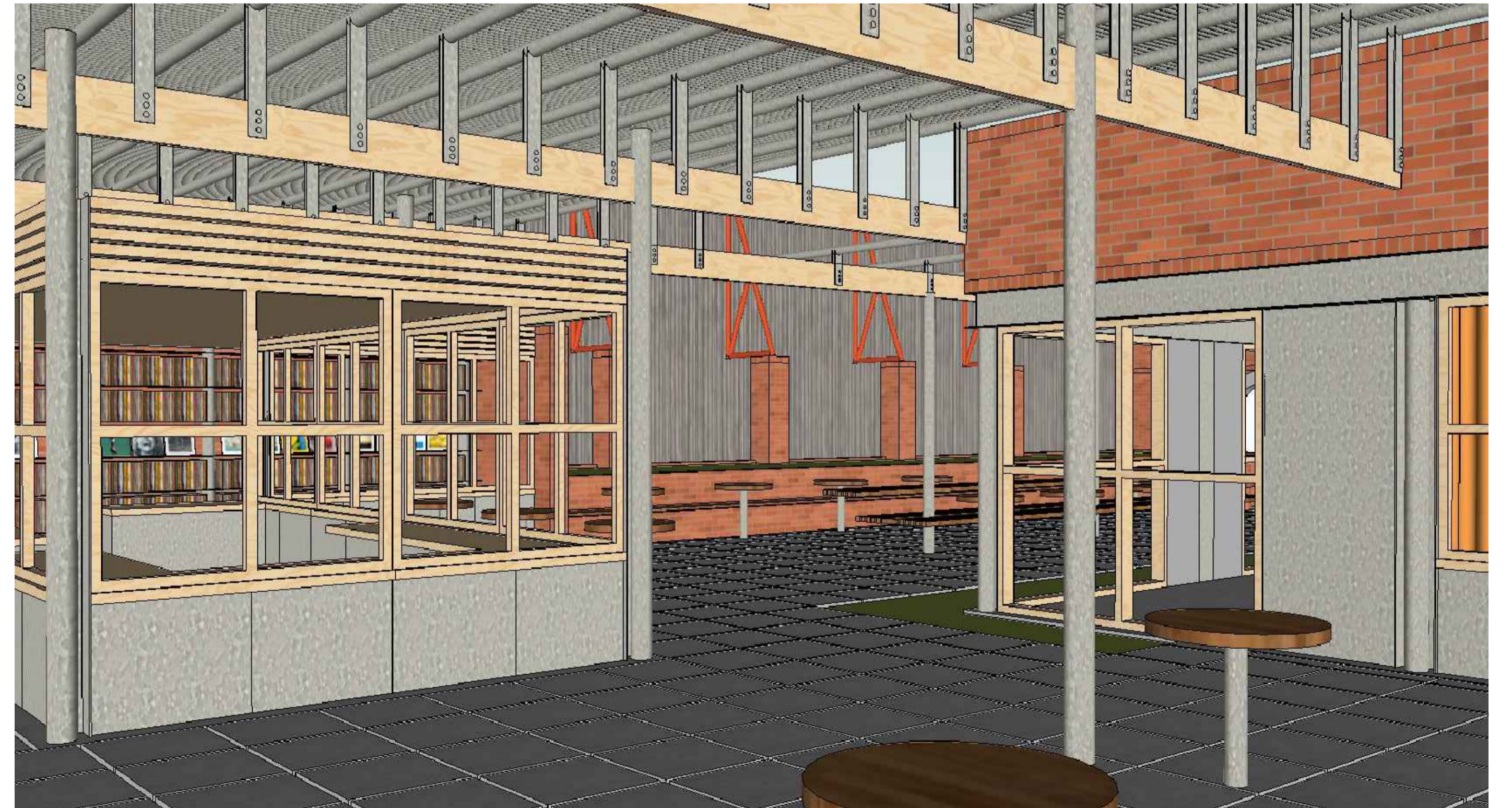
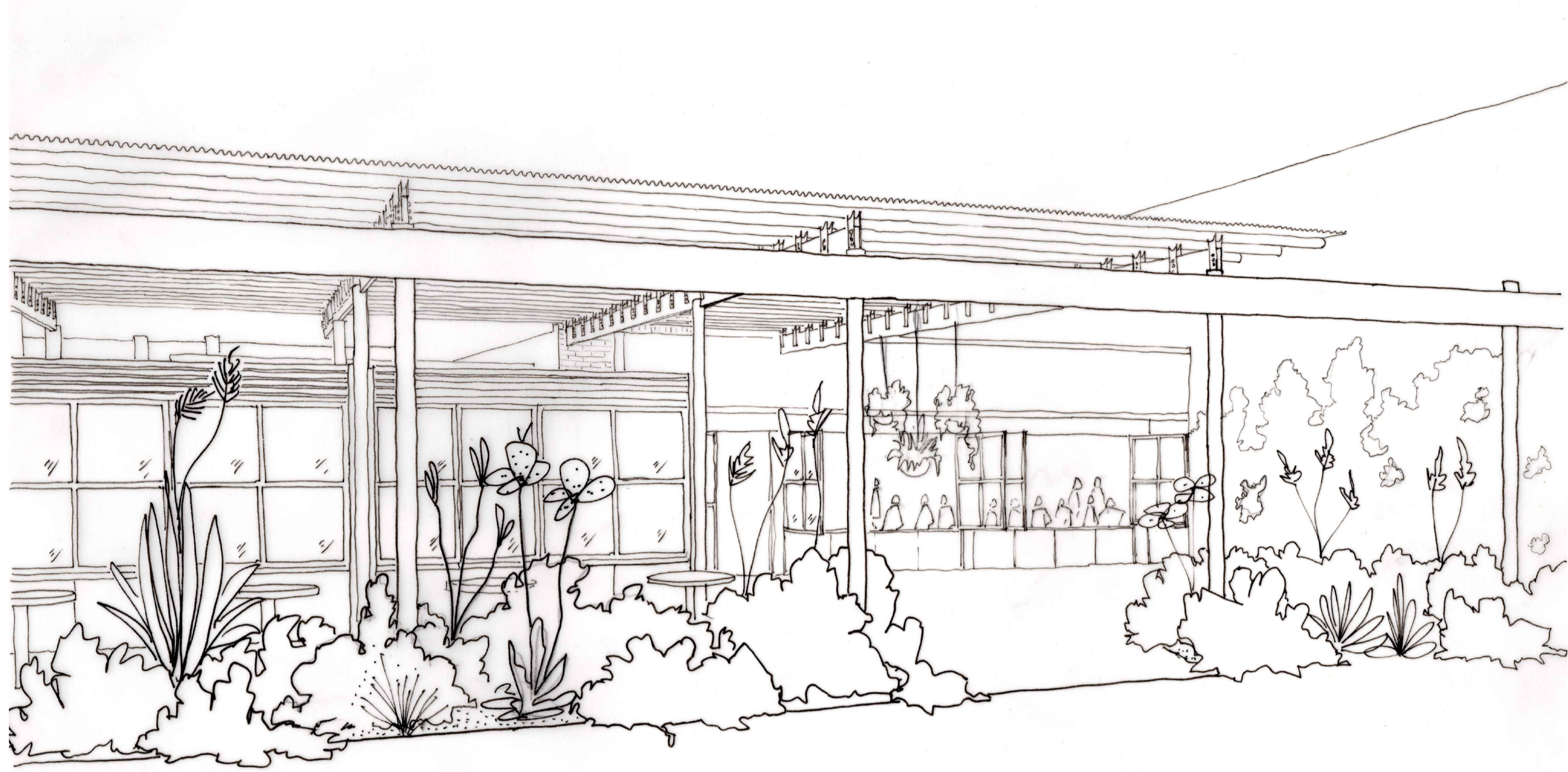


CITY OF VINCENT

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Nos. 378 and 380 William Street, Perth



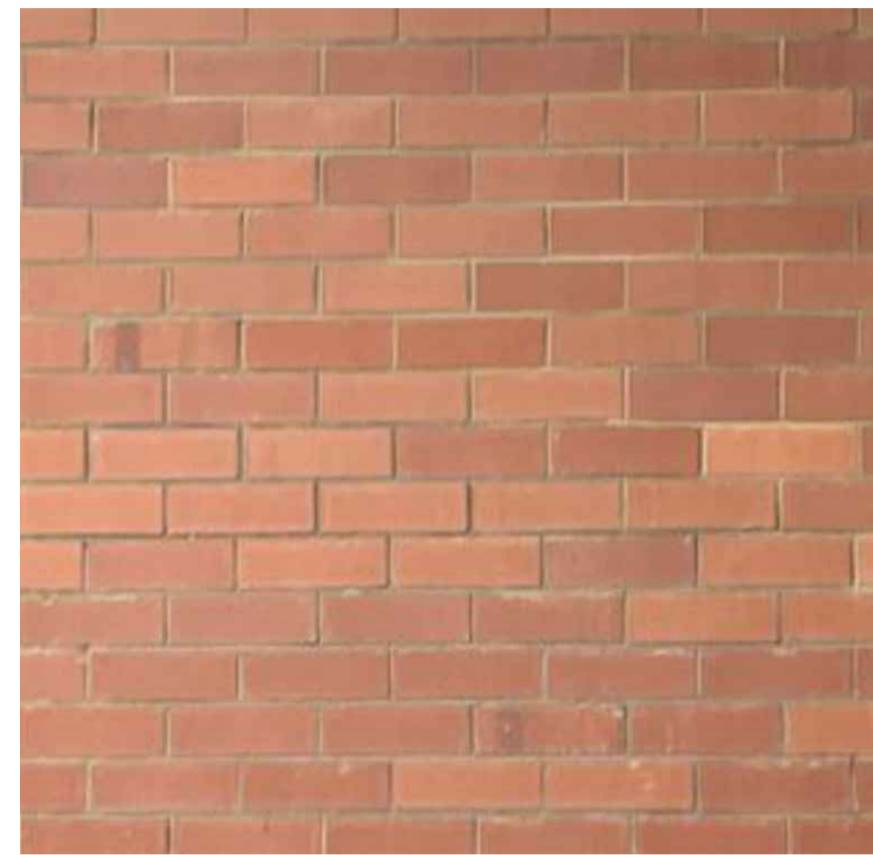


CITY OF VINCENT
RECEIVED
11 August 2023

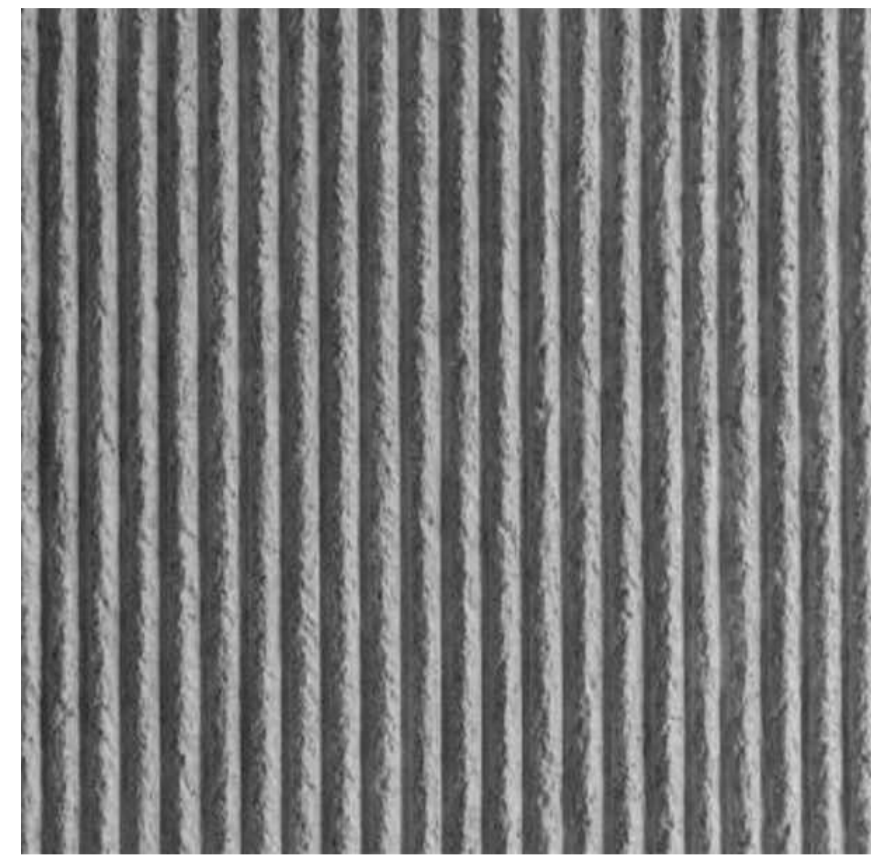
Material Schedule

Drawing Schedule

- A100 - Drawing + Material Schedule
- A101 - Existing Site Plan
- A102 - Existing Plan / Demolition Plan
- A103 - Proposed Plan
- A104 - Landscape Plan
- A201 - Existing Elevations
- A202 - Existing Elevations
- A203 - Proposed Elevations
- A204 - Proposed Elevations
- A205 - Proposed Elevations
- A301 - Proposed Sections
- A302 - Proposed Sections



BR1 - EXISTING BRICKWORK



CC2 - PRECAST TEXTURED CONCRETE



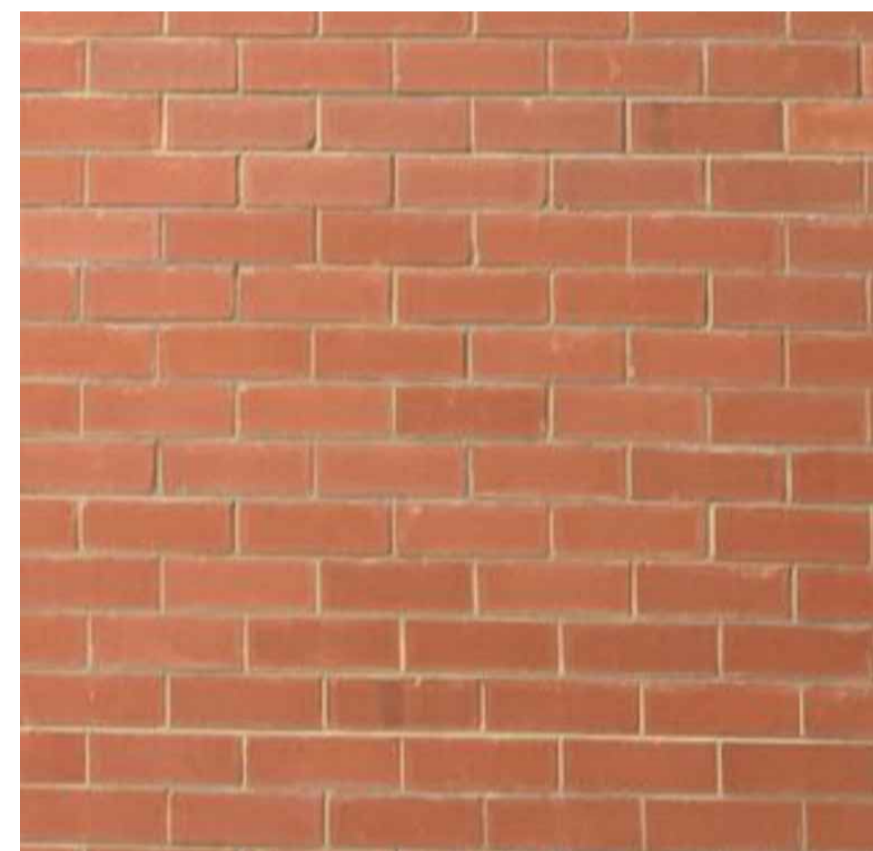
GS1-3 - GALVANISED STEEL
(STRUCTURAL, DETAILING AND CLADDING)



RF3 - COOLDEK ROOFING (CONCEALED)



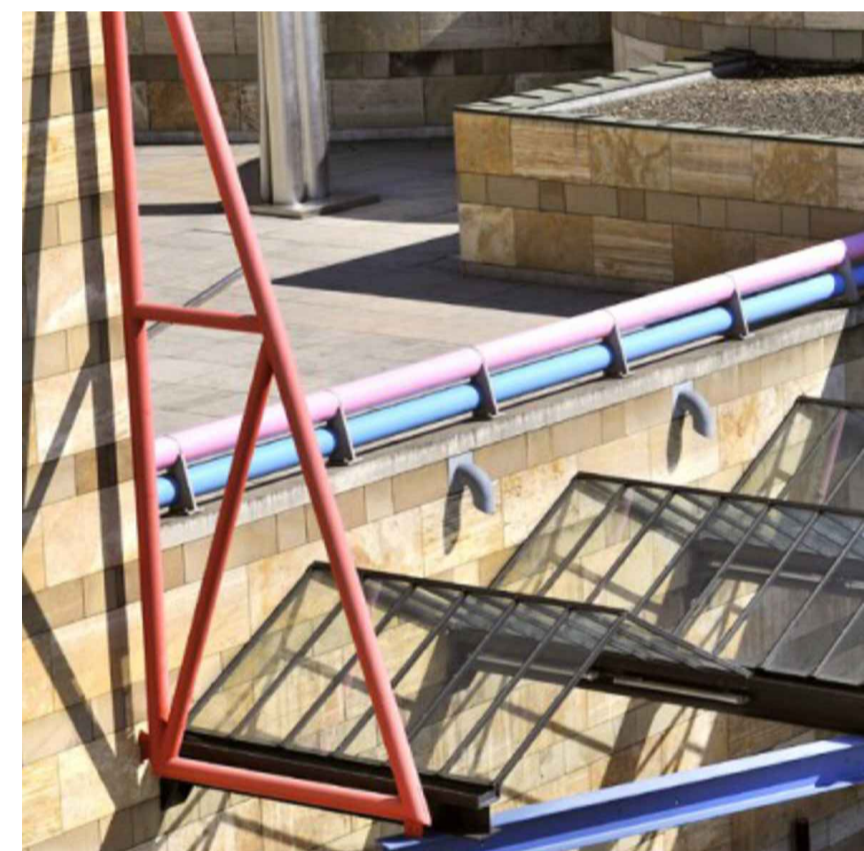
TM3 - MARINE GRADE HARDWOOD
PLYWOOD



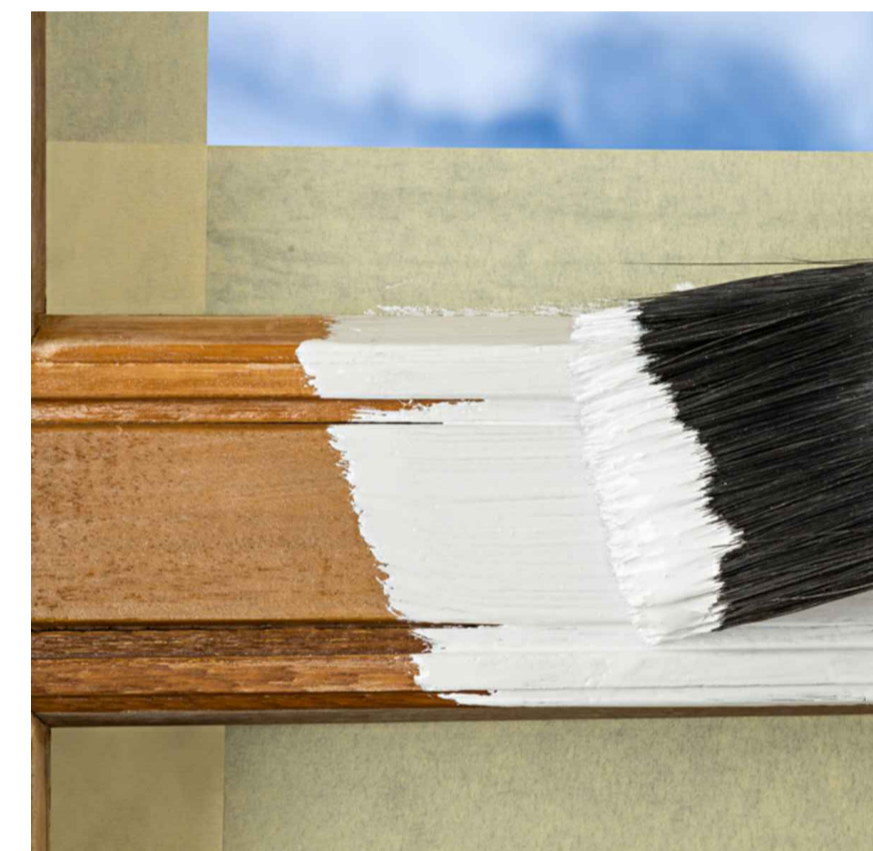
BR2 - NEW MATCHING BRICKWORK



CC3 - CONCRETE PAVERS SPACED
TO ALLOW FOR PLANTING



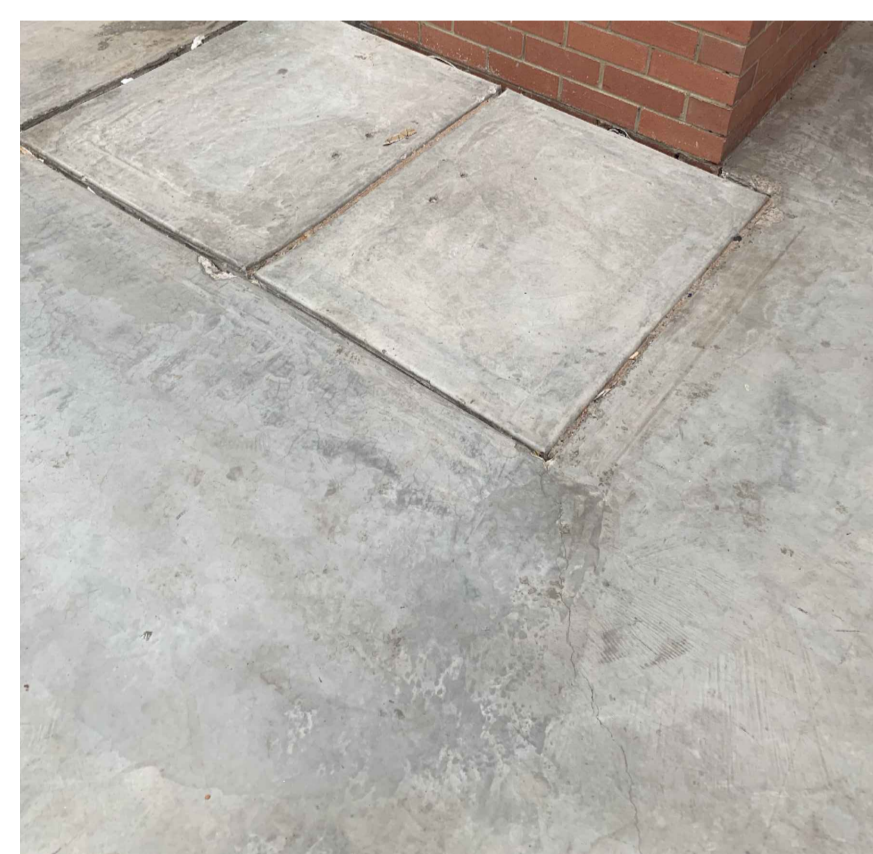
PS1 - PAINTED STEEL



TM1 - TIMBER WINDOWS (PAINTED)



TM4 - LVL ENGINEERED TIMBER



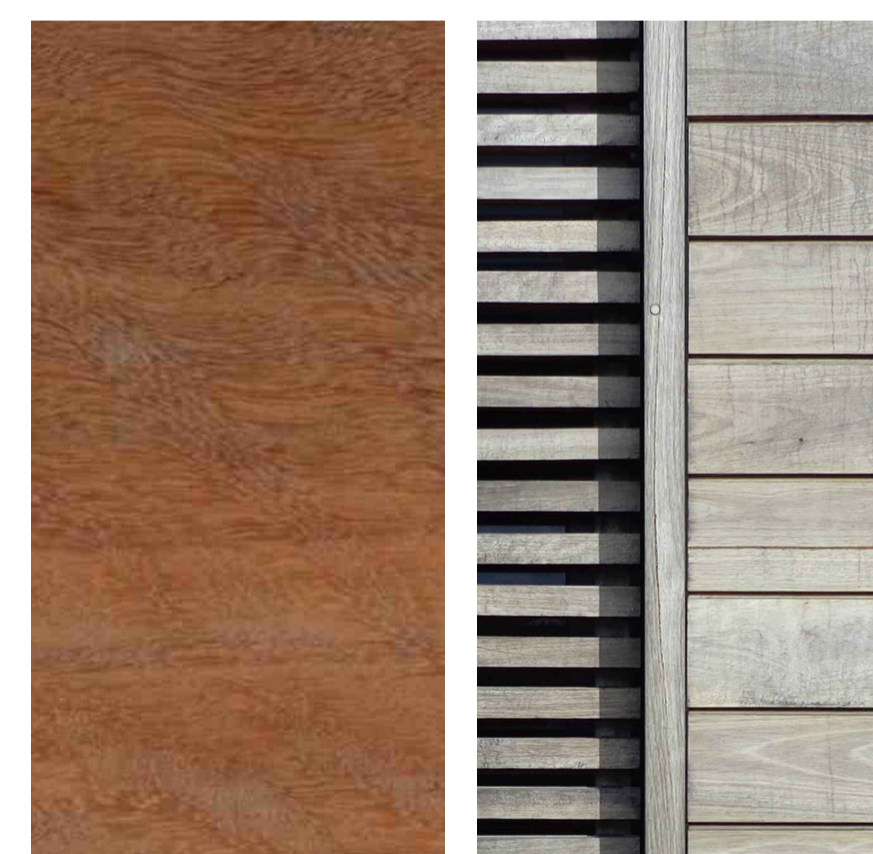
CC1 - EXISTING CONCRETE SLAB



CC5 - CUSTOM IN-SITU CONCRETE
DETAILING



RF2 - GALVANISED STEEL ROOF SHEETING



TM2 - JARRAH TIMBER LEFT TO GREY OFF



TR1 - STEEL TRELLIS

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MATERIALS KEY
BR1 Existing brickwork BR2 New matching brick
CC1 Ex. concrete slab CC2 Concrete boundary wall
CC3 Concrete pavers CC4 C'crete custom detail
GS1 Gal. steel (Structural) GS2 Gal. steel (cladding)
PS1 Painted Steel PS2 Ex. roof sheeting
RF1 Ex. steel roof sheet RF2 Gal. steel roof sheet
RF3 Stratco cooldek RF4 Roof flashings etc.
TM1 Timber (species tbc) TM2 Jarrah timber
TM3 Marine ply/wood ply TM4 Lam. veneer lumber
TR1 Gal. steel trellis

GENERAL LEGEND
ADJ DENOTES "ADJUSTABLE SHELF"
COLLUM (REFER STRUCT. ENG)
COS DENOTES "CONFIRM ON SITE" MEASUREMENT
DOWNPipe
FCL FINISHED CEILING LEVEL
FFL FINISHED FLOOR LEVEL
FW FLOOR WASTE
RL RELATIVE LEVEL
T HOSE COCK / PILLAR TAP

Revision:

Client
Iraia Andersen & Ben Taaffe

Project
Passenger

Project Address
380 William Street
Perth / Boorloo 6000
Western Australia
Drawing Name
Drawing+ Material
Schedule

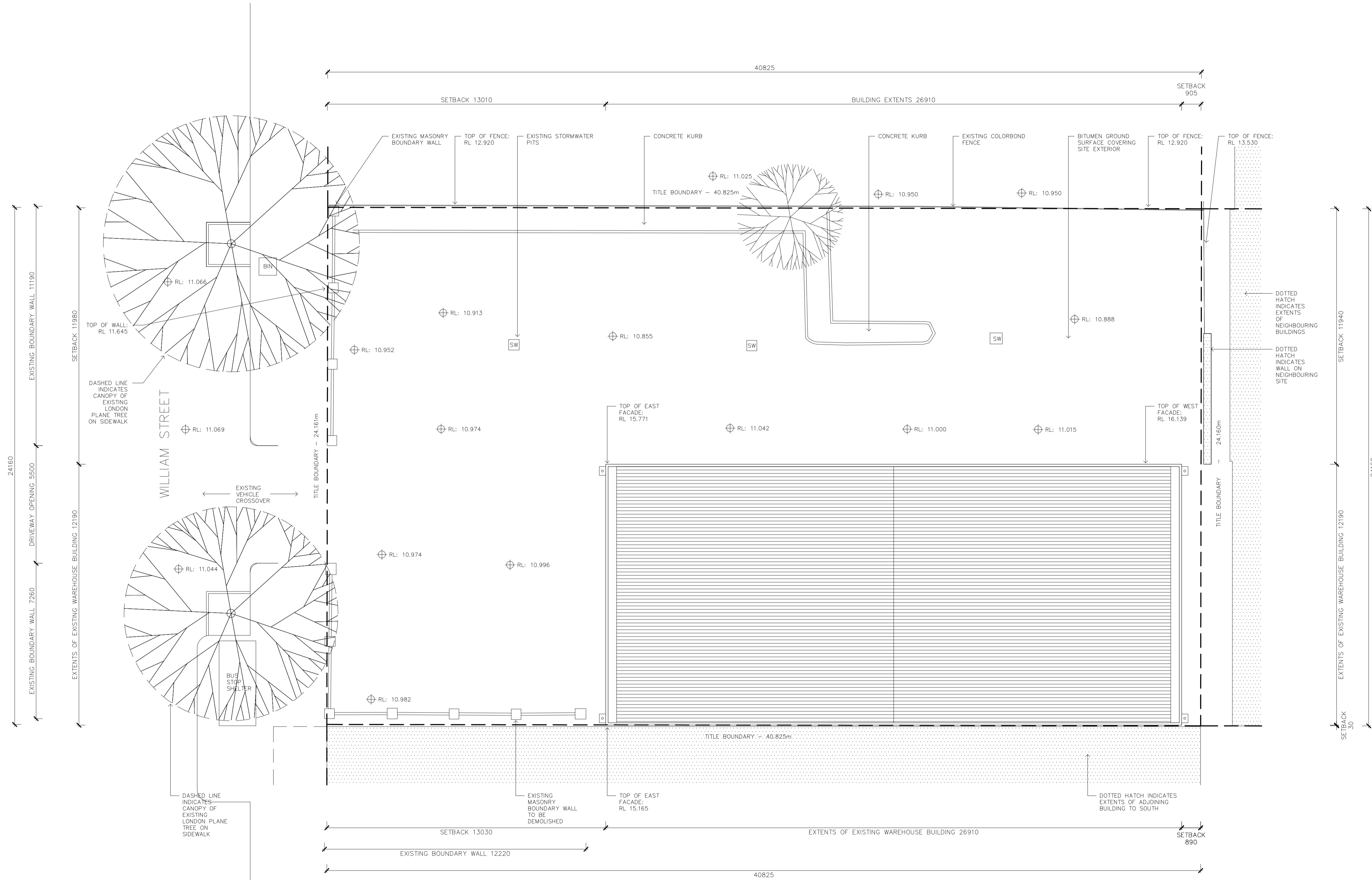
Project Stage
DA

Scale
n/a @ A1

Drawn
PFC

Project Number
2212

**Peter
Frederick
Cole
Architect**
Drawing
A100
Revision
DA1



Site Plan
1:100

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MATERIALS KEY

BK1 Existing brickwork	PS1 Painted Steel	TR1 Gal. steel trellis
BB2 New matching brick	RF1 Ex. roof sheeting	
CC1 Ex. concrete slab	RF2 Gal. steel roof sheet	
CC2 Concrete boundary wall	RF3 Stratco cooldek 50	
CC3 Concrete pavers	RF4 Roof flashings etc.	
CC4 C'crete custom detail	TM1 Timber (species tbc)	
GS1 Gal. steel (Structural)	TM2 Jarrah timber	
GS2 Gal. steel (cladding)	TM3 Marine ply/wood ply	
GS3 Gal. steel (cladding)	TM4 Lam. veneer lumber	

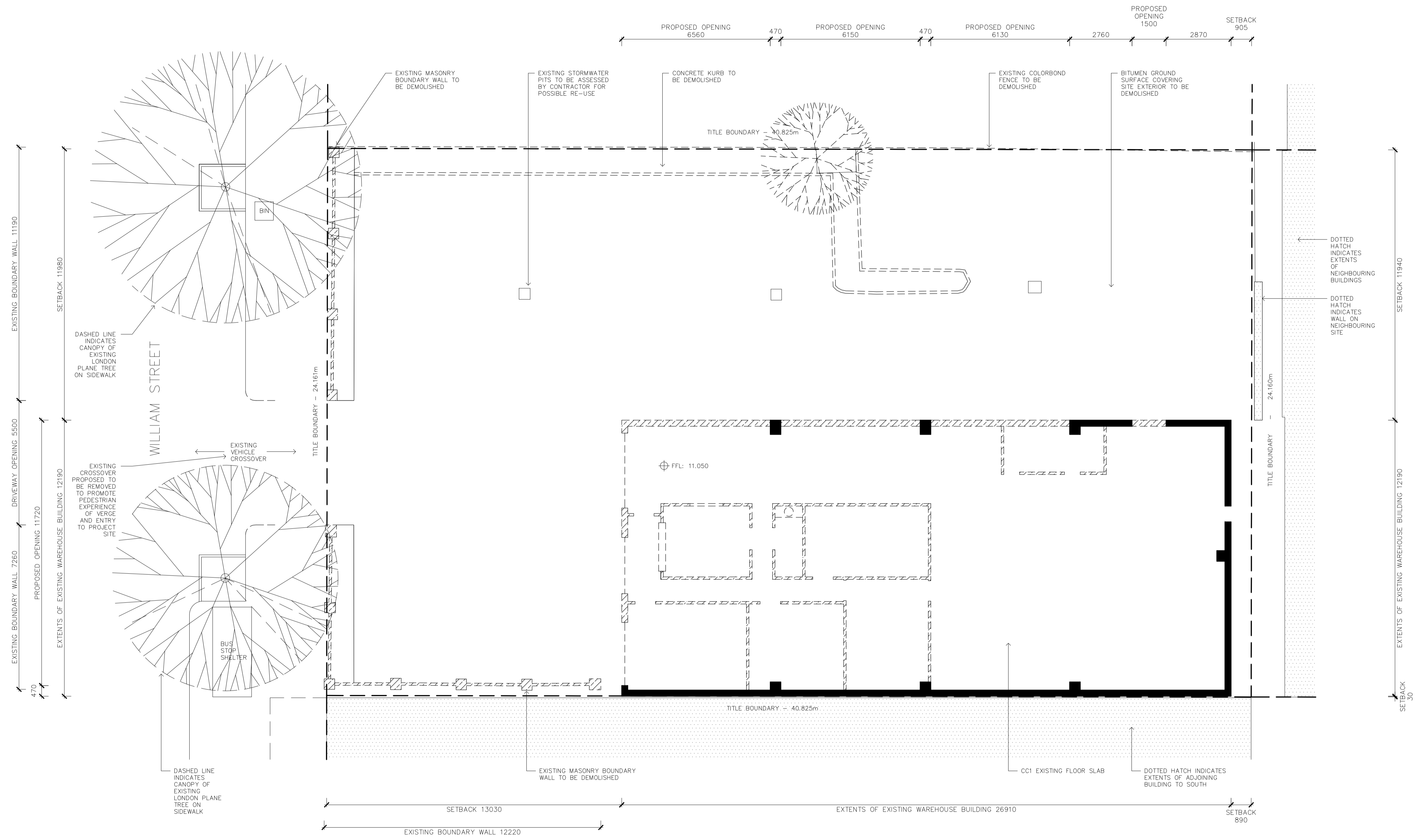
GENERAL LEGEND

ADJ	DENOTES "ADJUSTABLE SHELF"
C	COLUMN (REFER STRUCT. ENG)
COS	DENOTES "CONFIRM ON SITE" MEASUREMENT
D	DOWNPIPE
FCL	FINISHED CEILING LEVEL
FFL	FINISHED FLOOR LEVEL
FW	FLOOR WASTE
RL	RELATIVE LEVEL
T	HOSE COCK / PILLAR TAP

Revision:

Client Iraia Andersen & Ben Taaffe	Project Stage DA
Project Passenger	Scale 1:100 @ A1
Project Address 380 William Street Perth / Boorloo 6000 Western Australia Drawing Name Existing Site Plan	Drawn PFC
	Project Number 2212

Peter Frederick Cole Architect
Drawing A101
Revision DA10



Existing / Demolition Plan
1:100

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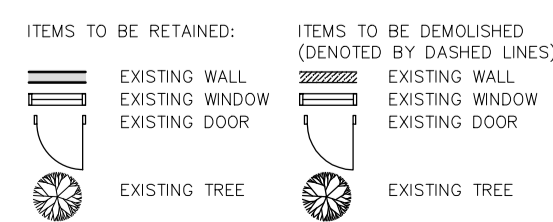
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MATERIALS KEY

BR1 Existing brickwork	PS1 Painted Steel	TR1 Gal. steel trellis
BR2 New matching brick	RF1 Ex. roof sheeting	
CC1 Ex. concrete slab	RF2 Gal. steel roof sheet	
CC2 Concrete boundary wall	RF3 Stratco cooldek 50	
CC3 Concrete pavers	RF4 Roof flashings etc.	
CC4 C'crete custom detail	TM1 Timber (species tbc)	
GS1 Gal. steel (Structural)	TM2 Jarrah timber	
GS2 Gal. steel (cladding)	TM3 Marine ply/wood ply	
GS3 Gal. steel (cladding)	TM4 Lam. veneer lumber	

GENERAL LEGEND

ADJ	DENOTES "ADJUSTABLE SHELF" COLUMN (REFER STRUCT. ENG)	EXISTING WALL	EXISTING WALL (DENOTED BY DASHED LINES)
COS	DENOTES "CONFIRM ON SITE" MEASUREMENT DOWNPIPE	EXISTING WINDOW	EXISTING WINDOW
FCL	FINISHED CEILING LEVEL	EXISTING DOOR	EXISTING DOOR
FFL	FINISHED FLOOR LEVEL	EXISTING TREE	EXISTING TREE
FW	FLOOR WASTE		
RL	RELATIVE LEVEL		
H	HOSE COCK / PILLAR TAP		

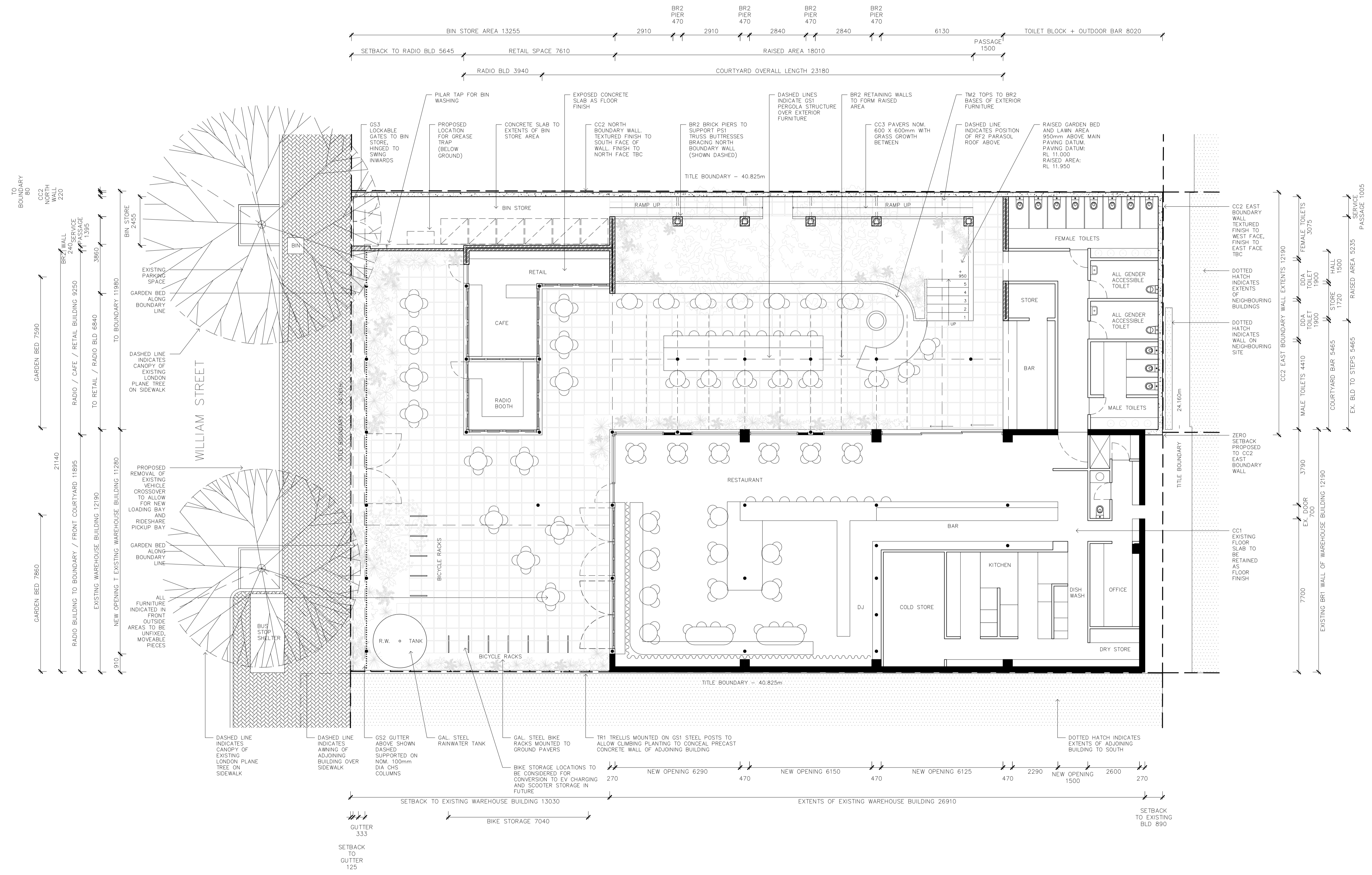


Revision:

Client
Iraia Andersen & Ben Taaffe
Project
Passenger
Project Address
380 William Street
Perth / Boorloo 6000
Western Australia
Drawing Name
Existing / Demolition Plan

Project Stage
DA
Scale
1:100 @ A1
Drawn
PFC
Project Number
2212

Peter Frederick Cole Architect
Drawing Name
A102 DA10
Revision
North



Proposed Plan
1:100

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MATERIALS KEY

BR1 Existing brickwork	PS1 Painted Steel	TR1 Gal. steel trellis
BR2 New matching brick	RF1 Ex. roof sheeting	
CC1 Ex. concrete slab	RF2 Gal. steel roof sheet	
CC2 Concrete boundary wall/RF3 Stratos cooler	RF4 Roof flashings etc.	
CC3 Concrete pavers	TM1 Timber (species tbc)	
CC4 C'crete custom detail	TM2 Jarrah timber	
GS1 Gal. steel (Structural)	TM3 Marine ply/wood ply	
GS2 Gal. steel (cladding)	TM4 Lam. veneer lumber	
GS3 Gal. steel (cladding)		

GENERAL LEGEND

ADJ	DENOTES "ADJUSTABLE SHELF" COLUMN (REFER STRUCT. ENG)
COS	DENOTES "CONFIRM ON SITE" MEASUREMENT DOWNPIPE
FCL	FINISHED CEILING LEVEL
FFL	FINISHED FLOOR LEVEL
FW	FLOOR WASTE
RL	RELATIVE LEVEL
T	HOSE COCK / PILLAR TAP

Revision:

Client
Iraia Andersen & Ben Taaffe

Project
Passenger

Project Address
380 William Street
Perth / Boorloo 6000
Western Australia
Drawing Name
Proposed Plan

Project Stage
DA

Scale
1:100 @ A1

Drawn
PFC

Project Number
2212

Peter Frederick Cole Architect

Revision
A103

North
DA10

SITE AREAS

SITE TOTAL: 986m² (100%)

TOTAL EXISTING BUILDING AREA: 328m² (32%)

TOTAL PROPOSED BUILDING AREA: 477m² (48%)
(INC. 328m² OF RETAINED EXISTING BUILDING AREA.)

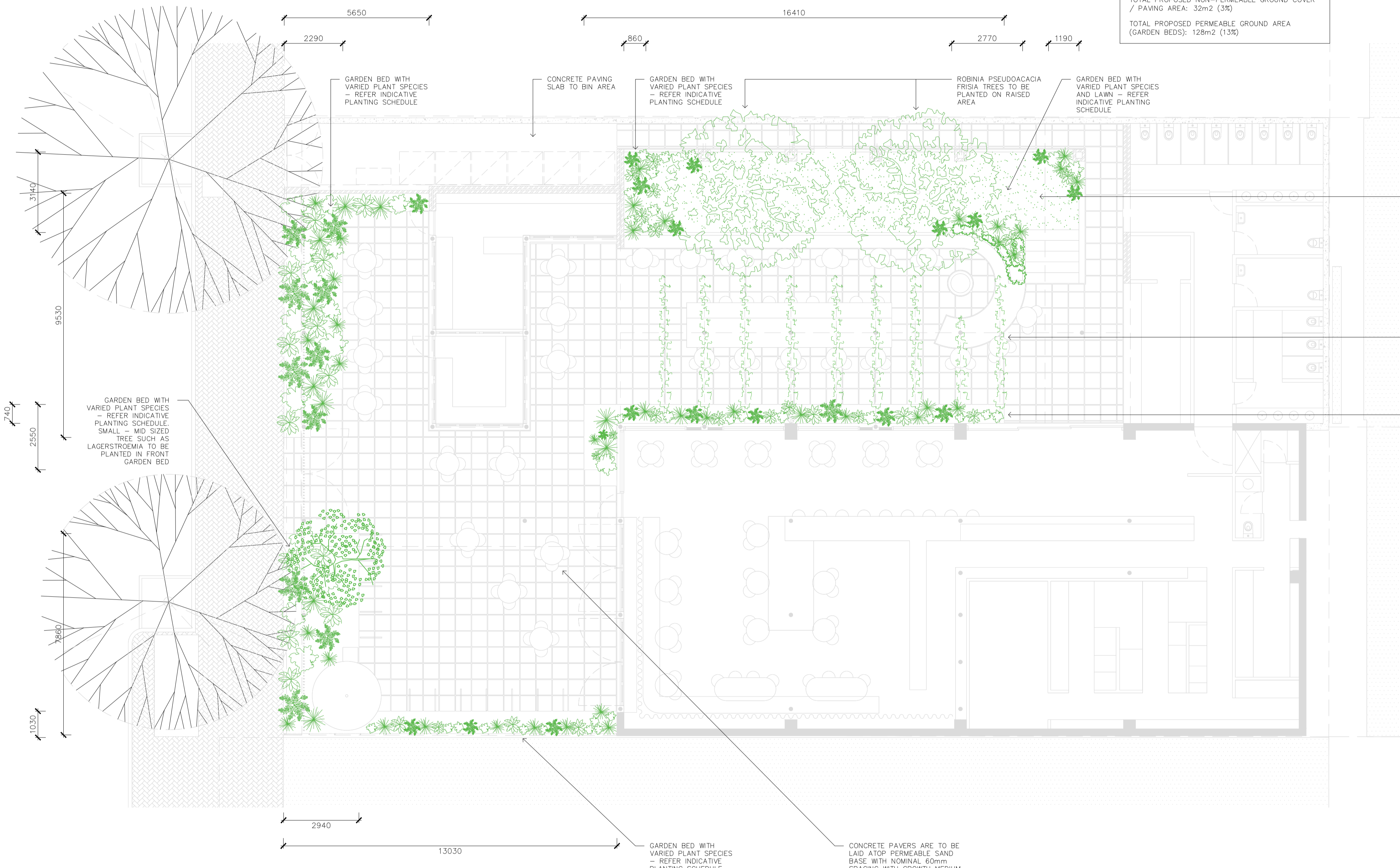
TOTAL EXISTING NON-PERMEABLE GROUND COVER / PAVING AREA: 626m² (63%)

TOTAL EXISTING PERMEABLE GROUND AREA (GARDEN BEDS): 32m² (3%)

TOTAL PROPOSED PERMEABLE PAVING AREA: 349m² (35%)

TOTAL EXISTING NON-PERMEABLE GROUND COVER / PAVING AREA: 32m² (3%)

TOTAL PROPOSED PERMEABLE GROUND AREA (GARDEN BEDS): 128m² (13%)



PLANTING SCHEDULE

	ZOYSIA TENUIFOLIA		MONDO GRASS		DICHONDRA REPENS
	VIOLA HEDERACEA - NATIVE VIOLET		LOMANDRA 'LITTLE CON'		DIANELLA TASMANICA
	TETRAPANAX		LAMBS EAR		DIANELLA CAERULIA
	SISYRNCHIUM - YELLOW EYED GRASS		LAGERSTROEMIA 'CREPE MYRTLE'		BOUGAINVILLEA VARIED
	ROBINIA PSEUDOACACIA FRISIA		KNIPHOFIA 'WINTER CHEER'		BLUE FESCUE GRASS
	RHINACANTHUS BEESIANUS		HOSTA 'GREEN LADY' 'WAGTAIL'		
	PROSTRATE ROSEMARY		GRAPE VINES TRAINED TO ARBOR		PERGOLA STRUCTURE
	PRATIA PEDUNCULATA		FOUNTAIN GRASS		
	POA LABILLARDIERI 'EKSDALEI'		FACIES NODOSA		
	PAMPAS GRASS		DICHONDRA 'SILVER FALLS'		

Landscaping Plan
1:100

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MATERIALS KEY

BRI Existing brickwork	PS1 Painted Steel	TRI Gal. steel trellis
BB2 New matching brick	RF1 Ex. roof sheeting	
CC1 Ex. concrete slab	RF2 Gal. steel roof sheet	
CC2 Concrete boundary wall	RF3 Stratos cooldek 50	
CC3 Concrete pavers	RF4 Roof flashings etc.	
CC4 C'crete custom detail	TM1 Timber (species tbc)	
GS1 Gal. steel (Structural)	TM2 Jarrah timber	
GS2 Gal. steel (cladding)	TM3 Marine ply/wood ply	
GS3 Gal. steel (cladding)	TM4 Lam. veneer lumber	

GENERAL LEGEND

ADJ	DENOTES "ADJUSTABLE SHELL" COLUMN (REFER STRUCT. ENG)
COS	DENOTES "CONFIRM ON SITE" MEASUREMENT DOWNPIPE
FCL	FINISHED CEILING LEVEL
FFL	FINISHED FLOOR LEVEL
FW	FLOOR WASTE
RL	RELATIVE LEVEL
T	HOSE COCK / PILLAR TAP

Revision:

Client
Iraia Andersen & Ben Taaffe

Project
Passenger

Project Address
350 William Street
Perth / Boorloo - 6000
Western Australia
Drawing Name
Landscaping Plan

Project Stage
DA

Scale
1:100 @ A1

Drawn
PFC

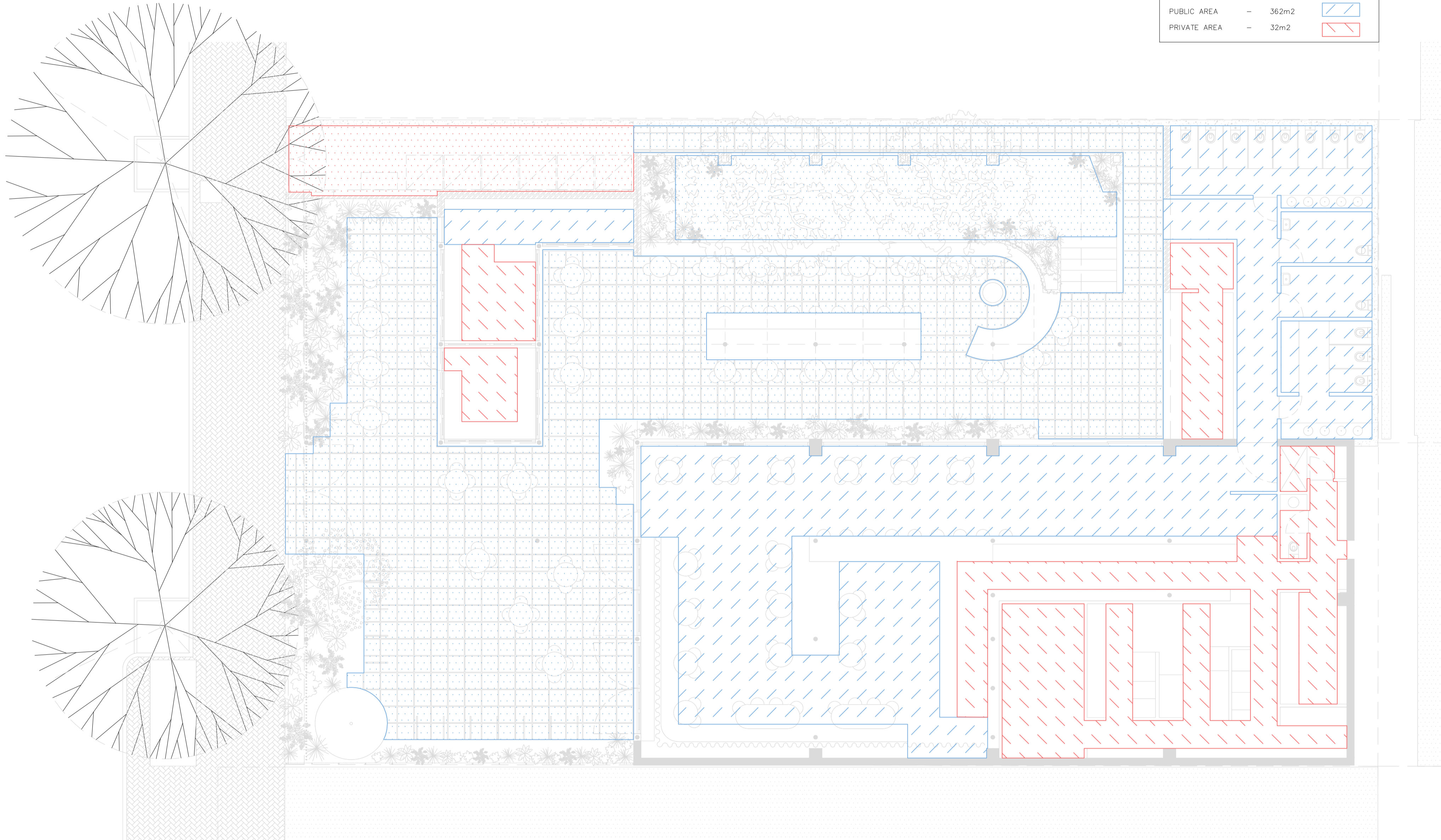
Project Number
2212

Peter Frederick Cole Architect
Drawing A104

Revision
DA10

North

SITE AREAS - PUBLIC VS. PRIVATE		
INTERIOR SPACES EXCLUDING FURNITURE AND STRUCTURE		
PUBLIC AREA	- 216m ²	
PRIVATE AREA	- 103m ²	
EXTERIOR AREAS EXCLUDING GARDEN BEDS, STRUCTURE AND FIXED FURNITURE		
PUBLIC AREA	- 362m ²	
PRIVATE AREA	- 32m ²	



Public / Private Areas Plan
1:100

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MATERIALS KEY

BRI Existing brickwork	PS1 Painted Steel	TRI Gal. steel trellis
BR2 New matching brick	RF1 Ex. roof sheeting	
CC1 Ex. concrete slab	RF2 Gal. steel roof sheet	
CC2 Concrete boundary wall	RF3 Stratco cooldek 50	
CC3 Concrete pavers	RF4 Roof flashings etc.	
CC4 C'crete custom detail	TM1 Timber (species tbc)	
GS1 Gal. steel (Structural)	TM2 Jarrah timber	
GS2 Gal. steel (detailing)	TM3 Marine ply/wood ply	
GS3 Gal. steel (cladding)	TM4 Lam. veneer lumber	

GENERAL LEGEND

ADJ	DENOTES "ADJUSTABLE SHELF"
C	COLUMN (REFER STRUCT. ENG)
COS	DENOTES "CONFIRM ON SITE" MEASUREMENT
DP	DOWNPIPE
FCL	FINISHED CEILING LEVEL
FFL	FINISHED FLOOR LEVEL
FW	FLOOR WASTE
RL	RELATIVE LEVEL
T	HOSE COCK / PILLAR TAP

Revision:

Client
Iraia Andersen & Ben Taaffe

Project
Passenger

Project Address
380 William Street
Perth / Boorloo 6000
Western Australia

Drawing Name
Public / Private
Areas Plan

Project Stage
DA

Scale
1:100 @ A1

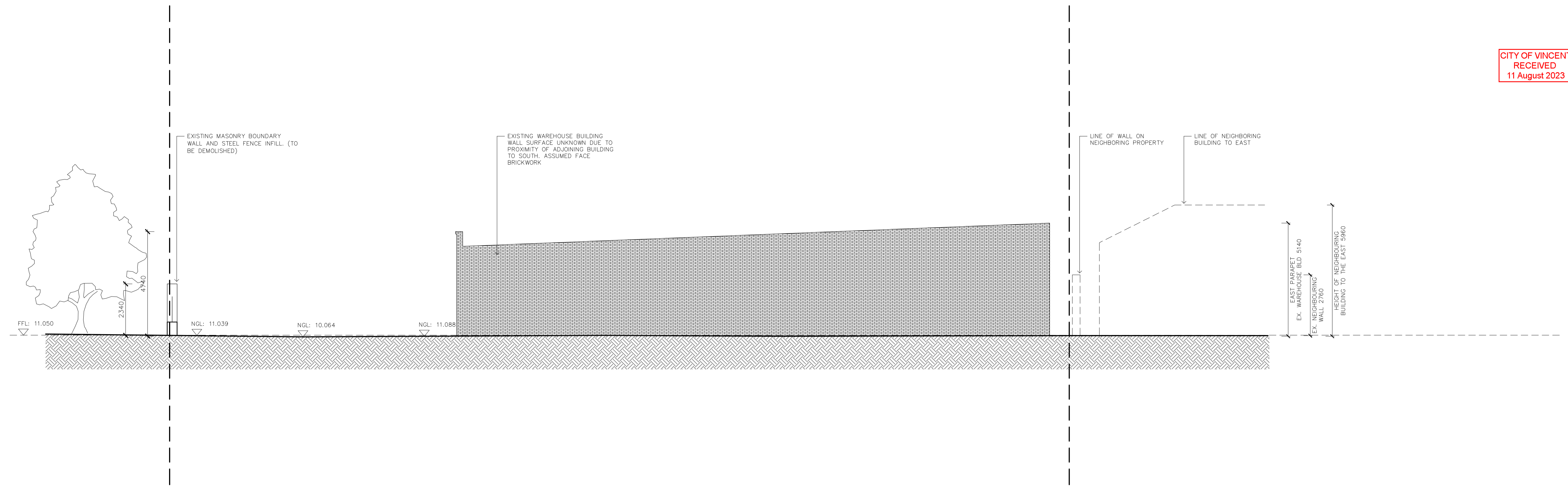
Drawn
PFC

Project Number
2212

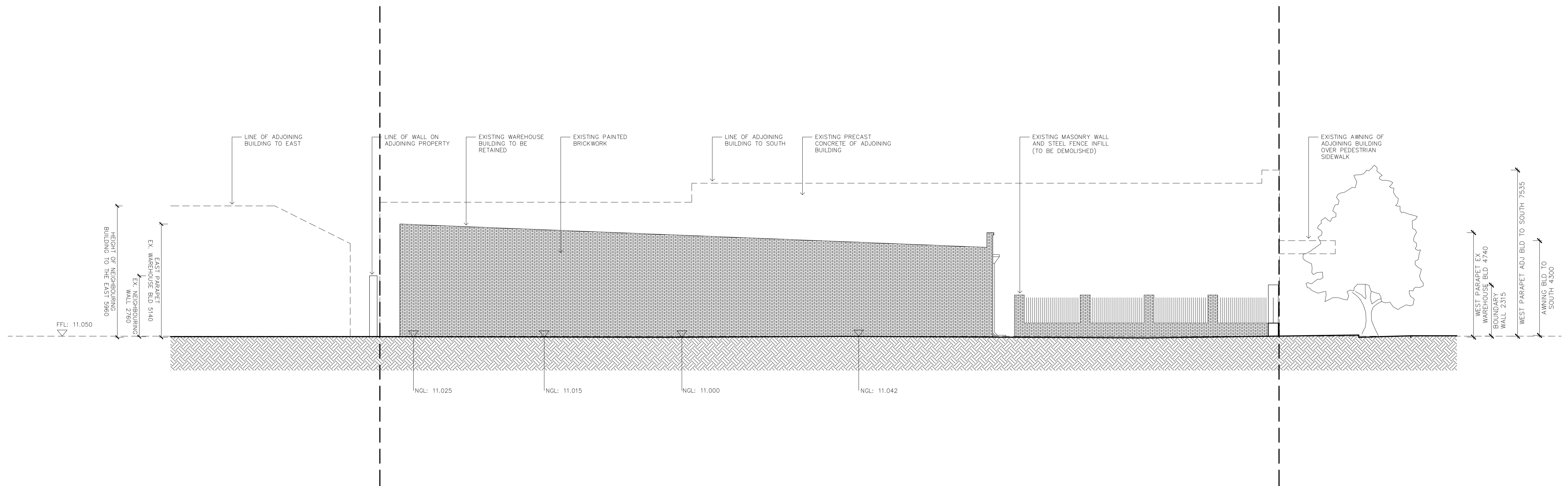
**Peter
Frederick
Cole
Architect**

Drawing
A105

Revision North
DA10



South Elevation
1:100



North Elevation
1:100

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Revision:

Client
Iraia Andersen & Ben Taaffe

Project
Passenger

Project Address
380 William Street
Perth / Boorloo 6000
Western Australia
Drawing Name
Existing Elevations

Project Stage
DA

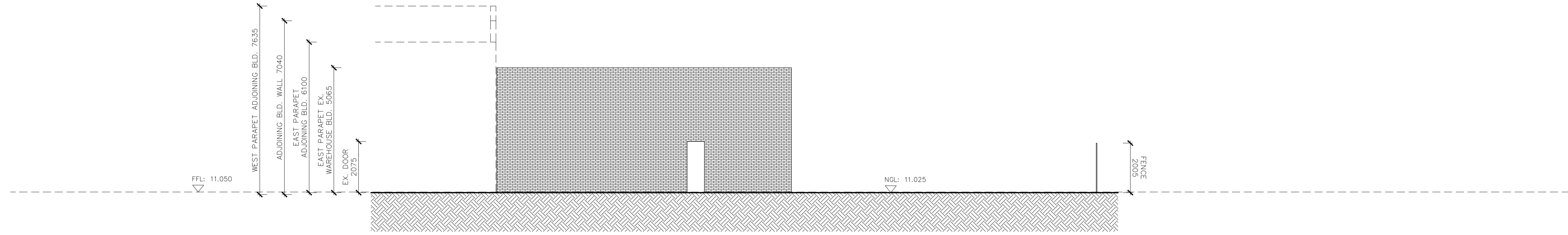
Scale
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Drawn
PFC

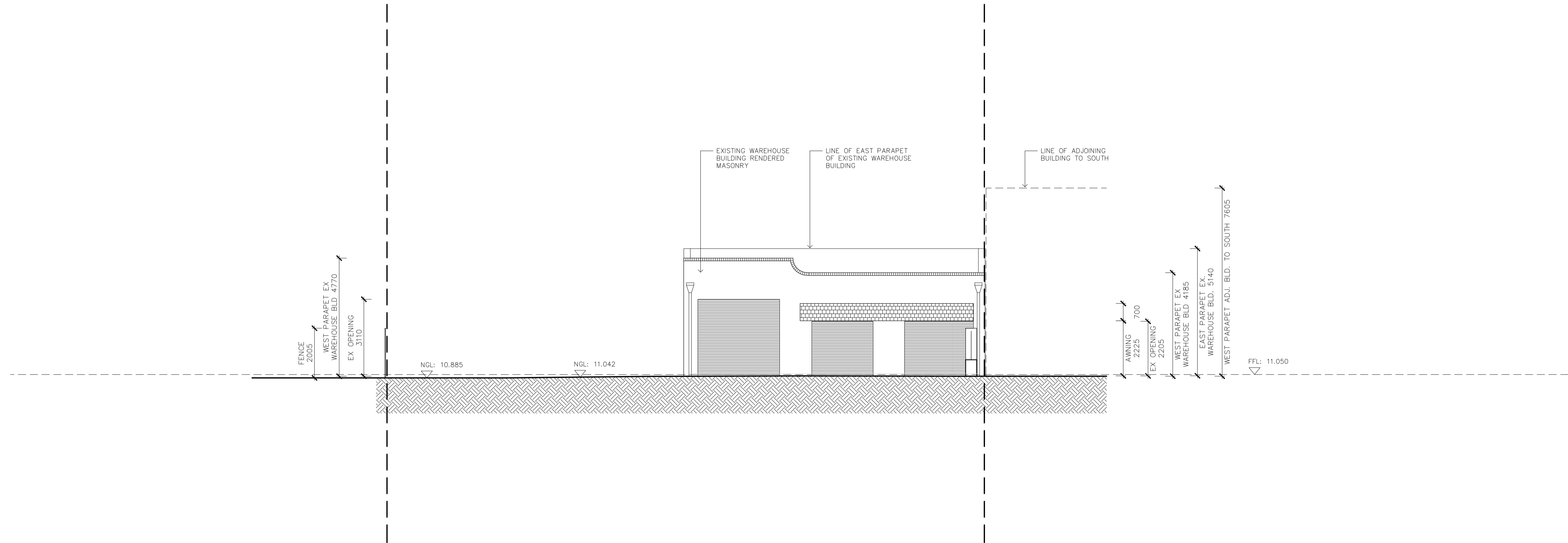
Project Number
2212

**Peter
Frederick
Cole
Architect**
Drawing
A201

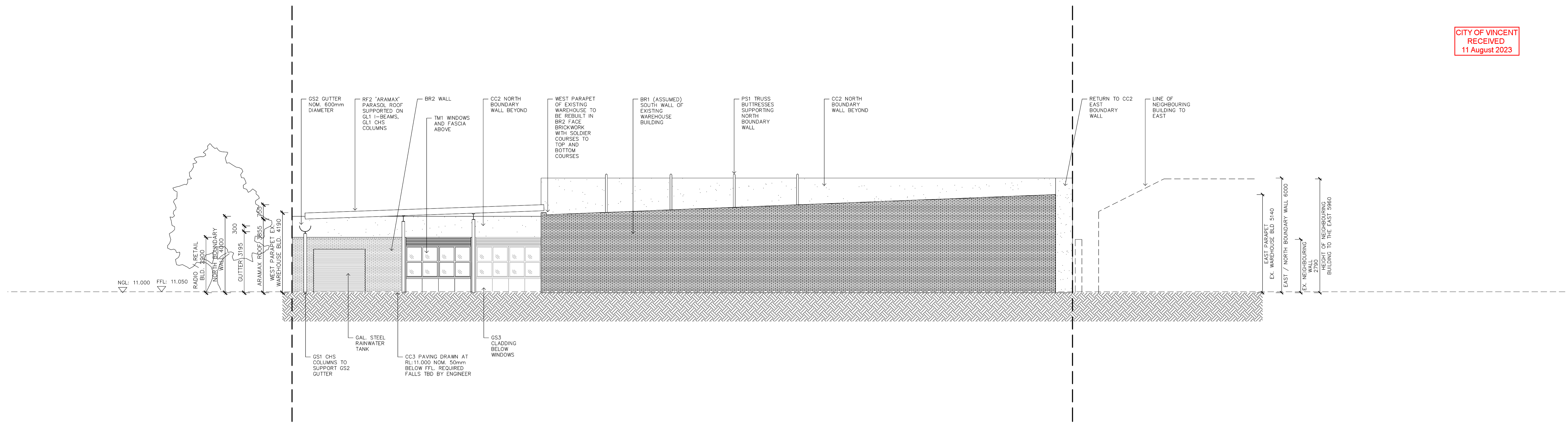
Revision
DA1



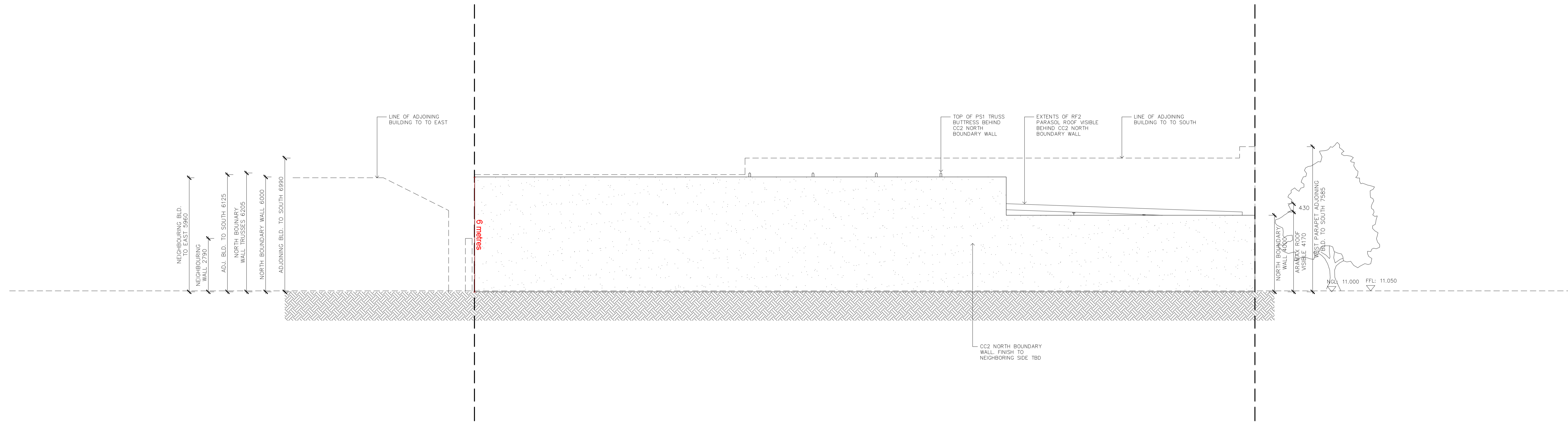
East Elevation
1:100



West Elevation
1:100



South Elevation
1:100



North Elevation
1:100

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Revision:

Client
Iraia Andersen & Ben Taaffe

Project
Passenger

Project Address
380 William Street
Perth / Boorloo 6000
Western Australia
Drawing Name
Proposed
Elevations

Project Stage
DA

Scale
1:100 @ A1

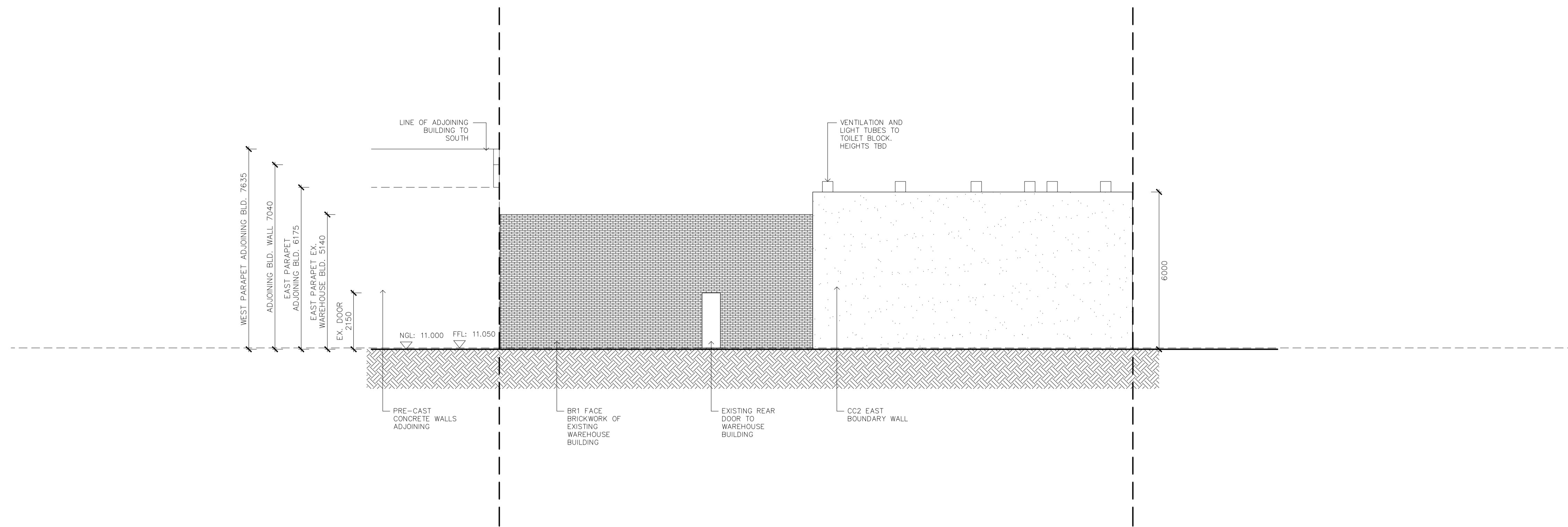
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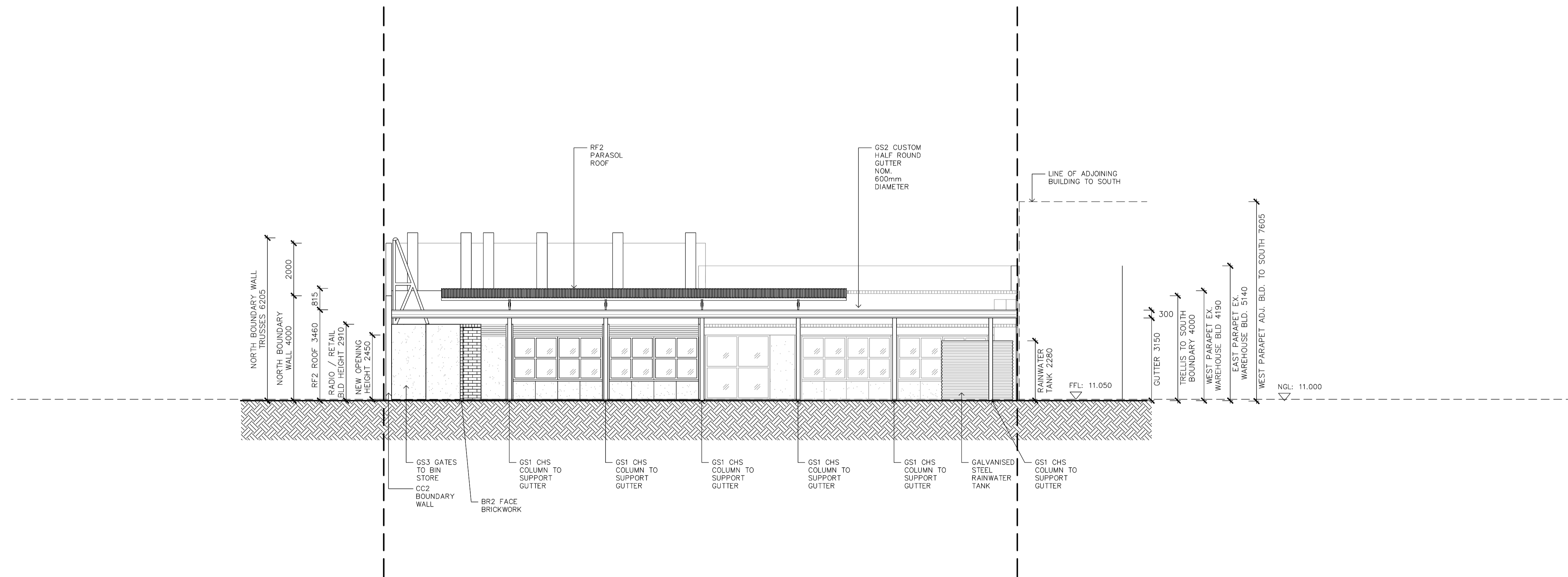
Peter Frederick Cole Architect

DA10

Revision North



East Elevation
1:100



West Elevation
1:100

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The Contractor shall verify all dimensions on site prior to commencing any shop drawings. Figured dimensions shall take precedence over scaled. Scaled dimensions shall be verified on site.

Revision:

Client
Iraia Andersen & Ben Taaffe

Project
Passenger

Project Address
380 William Street
Perth / Boorloo 6000
Western Australia
Drawing Name
Proposed
Elevations

Project Stage
DA

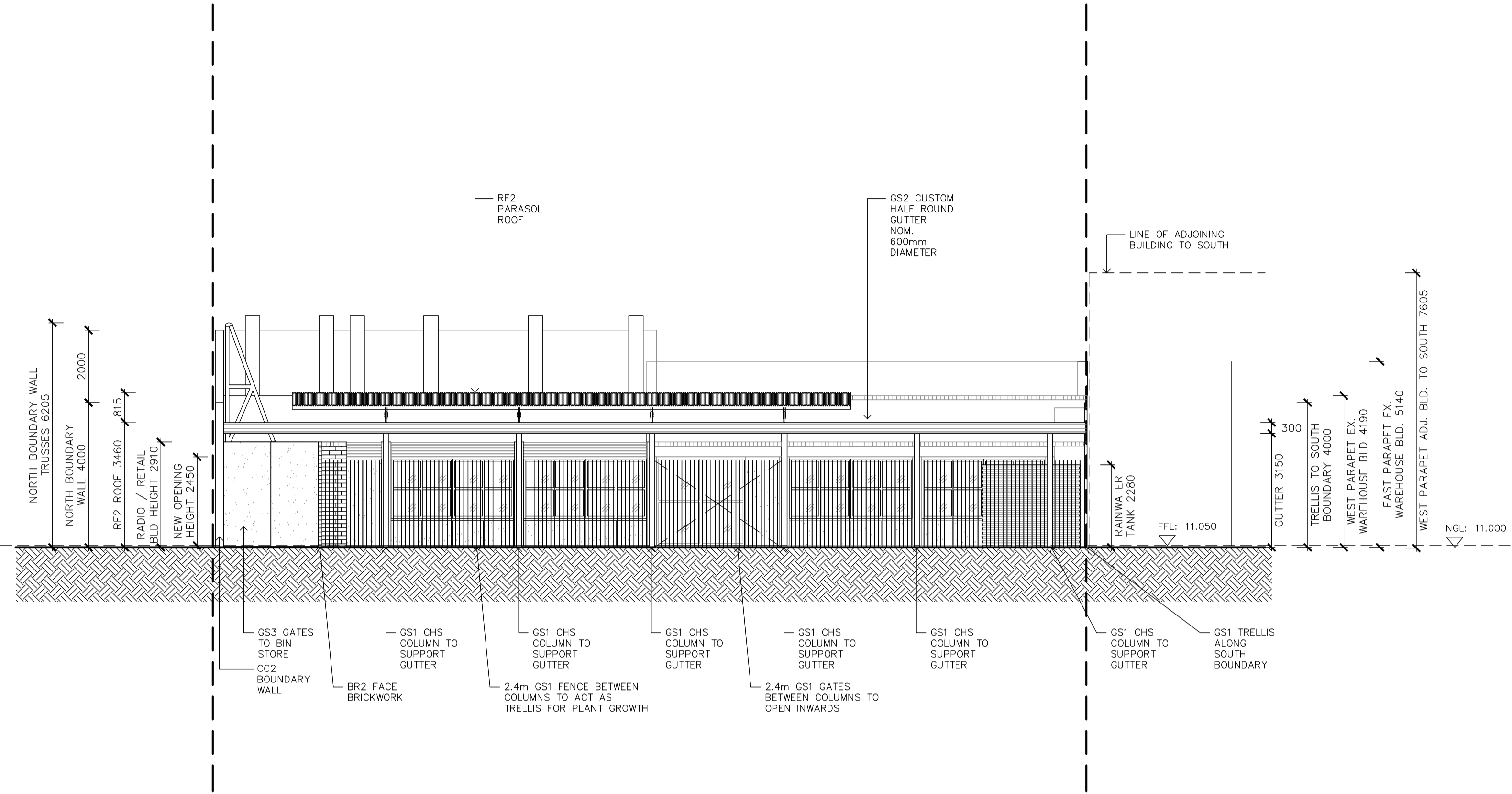
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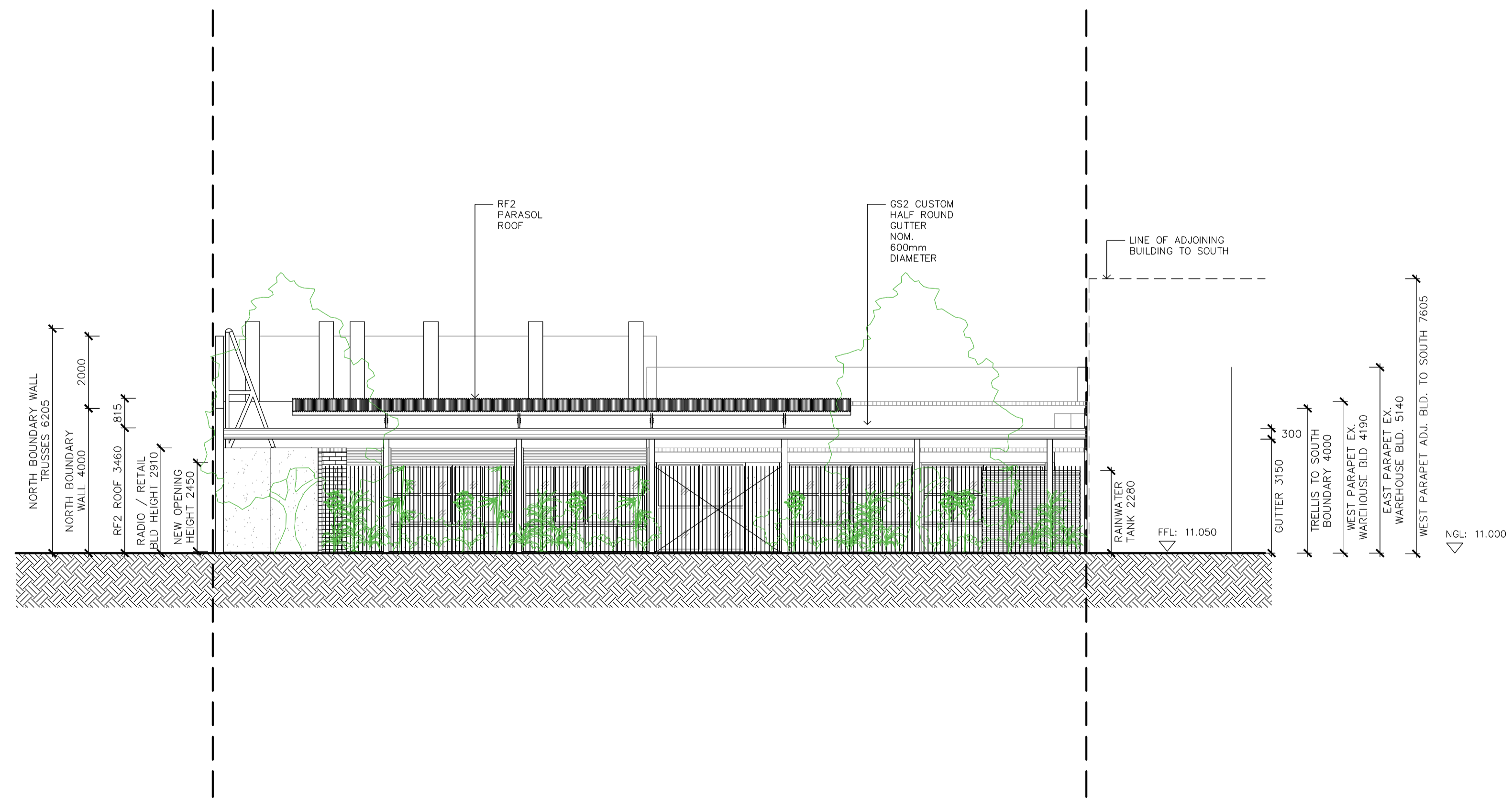
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**Peter
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Architect**
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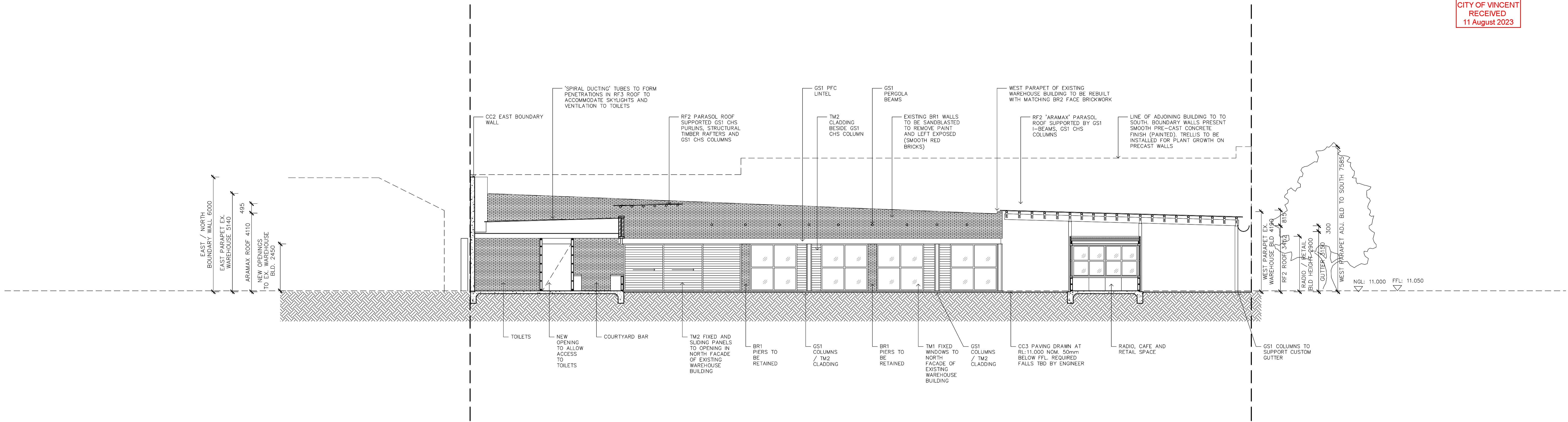
Revision
North



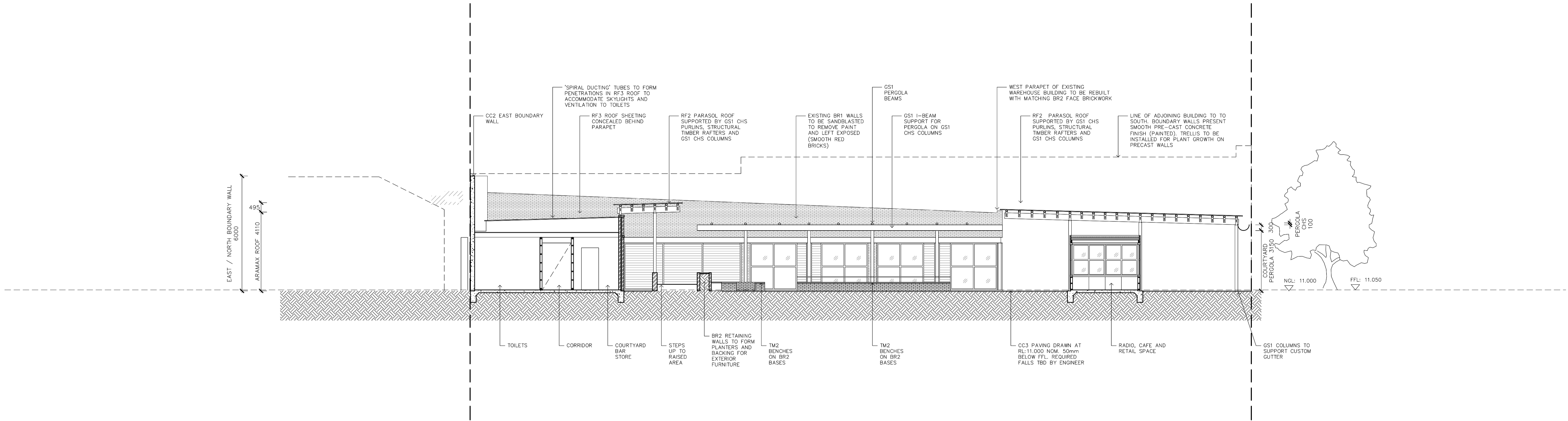
West Elevation – Including Fence
1:100



West Elevation – Planting along Boundary + Street Trees
1:100



Section xx
1:100



Section xx
1:100

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ABN: 92 161 606 254

Revision:

Client
Iraia Andersen & Ben Taaffe
Project
Passenger
Project Address
380 William Street
Perth / Boorloo 6000
Western Australia
Drawing Name
Proposed
Sections

Project Stage
DA

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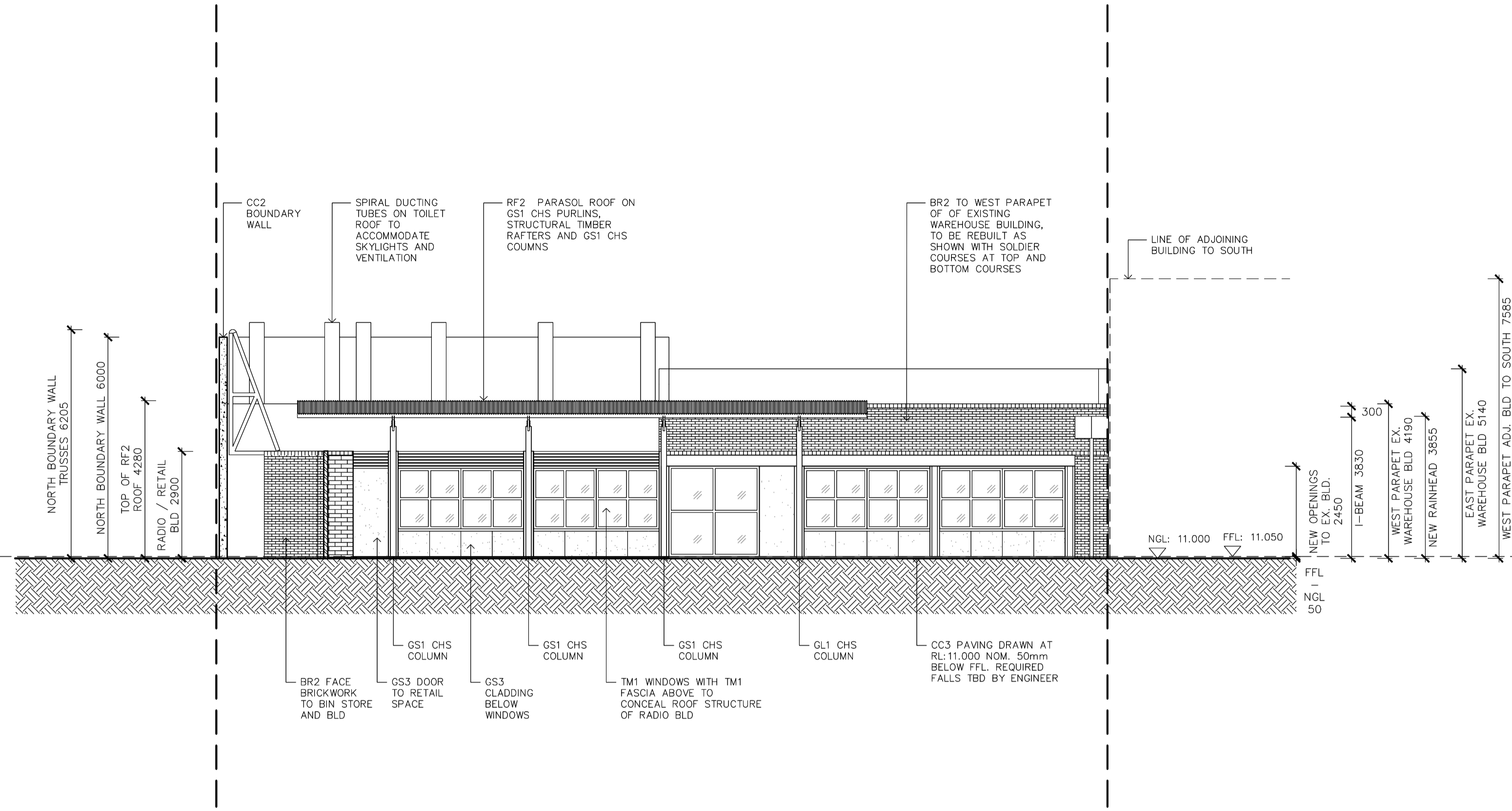
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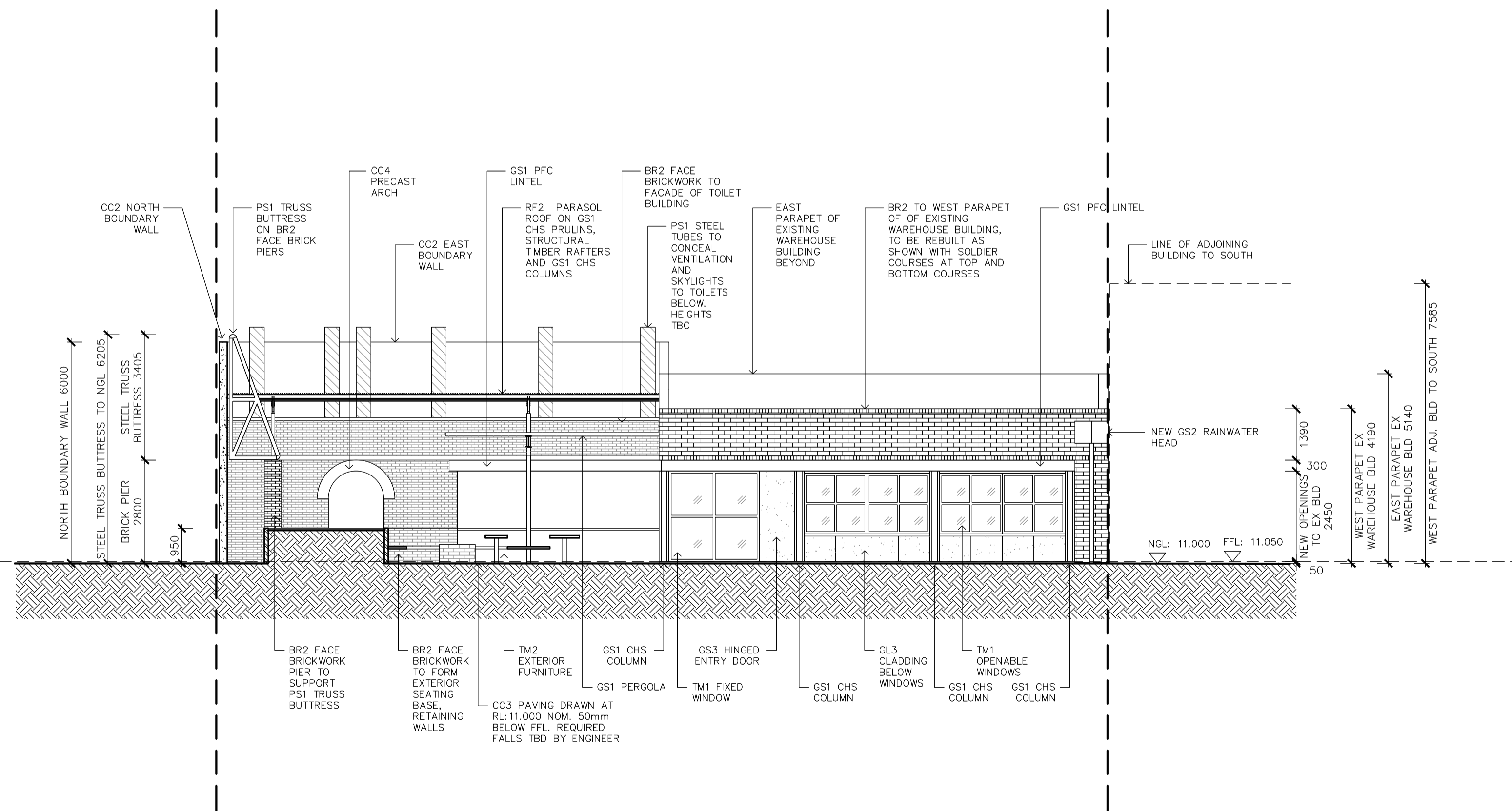
**Peter
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A301

Revision
DA10





Section xx
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Section xx
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The Contractor shall verify all dimensions on site prior to commencing any shop drawings. Figured dimensions shall take precedence over scaled. Scaled dimensions shall be verified on site.

Revision:

Client
Iraia Andersen & Ben Taaffe

Project
Passenger

Project Address
380 William Street
Perth / Boorloo 6000
Western Australia
Drawing Name
Proposed
Sections

Project Stage
DA

Scale
1:100 @ A1

Drawing
PFC

Project Number
2212

**Peter
Frederick
Cole
Architect**
Drawing
A302 DA10

Revision
DA10



Proposed Tavern, Internet Radio Studio, Café & Shop

378-380 William St, Perth

Prepared for Sound Hospitality Pty Ltd
10th August 2023

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1 Executive Summary

Method Planning presents for approval an application for a Tavern, Café, Shop and Internet Radio Studio at 378-380 William St, Perth. Method Planning presents Sound Hospitality Pty Ltd in regard to this proposal.

The project is to be a hospitality, retail, and community hub for music lovers in Perth, Western Australia. It offers a record store and cafe, a bar and kitchen, a beer garden and an online media/radio hub in the heart of Northbridge. The promise at the core of the business is to provide a home for a growing community of like-minded music lovers, DJs, fans, and enthusiasts to meet, listen, dance, eat and drink, perform, collect and share music. A local hub and hang-out for music heads and their friends with a diverse program of DJs and musicians connecting across the many thriving subcultures of the Perth music and arts landscape. The venue is to be called Passenger.

Sound Hospitality is a hospitality and events business founded by two established venue and events operators who have played a central role in bringing progressive culture and community focused spaces and activations into the Northbridge precinct, and on William Street specifically, through their successful business operations. Between them, directors Ben Taaffe and Iraia Anderson have over 23 years combined experience in designing, building and operating hospitality and events businesses.

The subject land is located within the Northbridge (William Street) District Centre with excellent exposure to passing trade along William Street and the greater Northbridge, Highgate, Perth area and surrounding suburbs.

The proposed land uses are compatible with the 'District Centre' zone and meets the objectives of the zone as follows:

- It provides a community focus point for people and leisure activities.
- The frontage and land uses are designed as a fully active and interactive frontage.
- The proposal retains and adapts the existing building.
- The operation of the development and waste management has demonstrably been shown to adopt best practise principles.
- The development will provide employment opportunities for both hospitality workers (15 at any one time), which matches the younger demographic that resides around this location, as well as employment for artists and performers to perform and play music or DJ at the venue.
- The development includes the promotion of uses including restaurant, cafe, and entertainment.
- The development ensures that the Northbridge District Centre is developed with due regard to State Planning Policy 4.2 - Activity Centres for Perth and Peel.

Based on the detailed planning assessment presented in this report, it has been demonstrated that the proposal is consistent with or exceeds the intent of relevant planning framework and is closely aligned with the City of Vincent's vision for the Northbridge District Centre and William St Place Plan area.

In light of the above, favourable consideration by City of Vincent and approval by the Elected Members is respectfully requested.

2 Introduction

Method Planning presents for approval an application for a Tavern, Café, Shop and Internet Radio studio at 378-380 William St, Perth (the 'subject land').

The proposed development has been designed having regard to the specific provisions of the City of Vincent Local Planning Scheme No. 2 (LPS2) and associated Design Guidelines and Local Planning Policies.

Pre-lodgement meetings were held with the City of Vincent Planning Team on the 24th February and 4th July 2023 to discuss the proposal and to seek clarification for the requirements of the application. Feedback received from these meetings has assisted with the refinement of this application.

This report will address the major planning and design issues pertinent to the subject land. Specifically, the report provides information on the following:

- Detail description of the project, the vision and background to proponents
- Location and a detailed description of the site context.
- Consultation with Council and the local community.
- Town planning considerations and description of the planning framework.
- Assessment of the proposed development with the relevant planning instruments.
- Justification for the development including detail regarding land use, built form, transport, waste and noise management.

We consider the information contained therein adequately demonstrates the appropriateness of the proposed development and request an approval at the earliest opportunity.

2.1 The Project Team

This report has been prepared by Method Planning in conjunction with a team of supporting professionals and experts as detailed in **Table 1** below. Their respective technical reports and plans are appended to this report:

Table 1 – Project Consultants

Technical Area of Expertise	Consultant
Architecture	Peter Frederick Cole Architect
Landscape Architecture	Peter Frederick Cole Architect
Acoustics	Acoustic Consultants Australia
ESD Consultant	Aimee Smith
Waste Management	Sound Hospitality
Urban Planning	Method Planning
Car Parking and Transport	Move Consultants
Venue Management Plan	Sound Hospitality
Public Interest Assessment	Sound Hospitality

3 The Project

3.1 Project Summary

The project is to be a hospitality, retail, and community hub for music lovers in Perth, Western Australia. It offers a record store and cafe, a bar and kitchen, a beer garden and an online media/radio hub in the heart of Northbridge. The promise at the core of the business is to provide a home for a growing community of like-minded music lovers, DJs, fans, and enthusiasts to meet, listen, dance, eat and drink, perform, collect and share music. A local hub and hang-out for music heads and their friends with a diverse program of DJs and musicians connecting across the many thriving subcultures of the Perth music and arts landscape. The venue is to be called Passenger, please see an **Example Cultural Program Schedule at Appendix A.**

To achieve this, the venue will consist of four complementary land uses creating a whole that is greater than the sum of its parts:

- Retail & Café - A world-class street-facing record shop & cafe; Shari Vari Records will stock a curated selection of vinyl records, music magazines, and clothing, and serves coffee and food, and activating the space and frontage from breakfast time and all day.
- Tavern - A lounge bar and kitchen offering high-quality affordable drinks and simple, fresh food, with options for casual drinking and dining or a more intimate listening experience.
- Tavern - A spacious beer garden boasting large shady trees, set behind the online media hub creating a leafy retreat from the grimy urban landscape beyond.
- Unlisted Use (Online Radio Studio) - An online community media/radio hub broadcasting diverse, progressive, and positive signals to the world while connecting virtual communities with local spaces and real life!

Please see plans of proposed development at **Appendix B.**

3.2 Detailed Description

The venue will consist of the following elements:

- 240m² Interior public lounge and bar area.
- 417m² of landscaped beer garden with 128m² of deep soil zone.
- 12m² retail shop and 12m² café.
- 12m² internet radio studio.
- Acoustic treatment and walls to the north and east to protect surrounding properties.
- Removal of the crossover and repurpose of the bitumen hardstand area on site to landscaped beer garden.
- 24 Long and Short-Term Bike bays, with some to be converted to EV charging for e-scooter and e-bike parking, pending demand.
- End of trip facilities including lockers and a shower for staff.
- Rainwater tank.

Please also refer to the ancillary information below regard operating hours, numbers of staff and customers as well as plant and equipment details:

- Operating hours – 7 days 7:00am – Midnight Monday to Saturday, 7:00am to 10:00pm. Sundays and Public Holidays – subject to liquor licensing approvals and as per Policy 7.5.7 clause 3.3.3 (a).
- Maximum employees at any one time will be 15.
- Capacity expected to be 350 people — subject to liquor licensing approvals.

- Plant and equipment to be used detailed in Table 3 below:

Table 2 – Plant and Equipment

Kitchen & Bar	Other
S/S kitchen benches	Soundsystem
S/S for beer station and bar counter	CCTV
Kitchen hand basin	Shipping container Radio
Stone bar tops	Outdoor Heating
Pass through dishwasher	Furniture Indoor
Pot sink	Furniture Outdoor
Glass washer	POS System
Upright Glass fridges	Coffee Machine
Under counter fridge	
Cool room	
Beer system	
6 Burner and grill plate	
Combi oven	
Hot griddle plate	
Exhaust canopy installed	
Deep fryer (twin basket)	

3.3 About the Company

3.3.1 Company Profile

Sound Hospitality is a hospitality and events business founded by two established venue and events operators who have played a central role in bringing progressive culture and community focused spaces and activations into the Northbridge precinct, and on William Street specifically, through their successful business operations. Between them, directors Ben Taaffe and Iraia Anderson have over 23 years combined experience in designing, building and operating hospitality and events businesses.

3.3.2 Iraia Andersen - Director, Hospitality & Facility

Iraia Andersen is a highly successful hospitality entrepreneur, with a rich background in the industry stemming from his grandfather's successful hotel businesses in Aotearoa. Having honed his skills from a young age, working in some of Melbourne's finest restaurants and bars, Iraia has become a champion of service and quality in the hospitality sector. He has since gone on to establish and manage his own highly acclaimed businesses, including Ezra Pound Bar, which won the Small Bar Association of WA's "Best Small Bar" people's choice award in 2013. Iraia also developed and managed Perth's first American BBQ restaurant and bar, 'Old Faithful Bar & BBQ', which was awarded "Best New Small Bar" for Western Australia in 2014. With a focus on creating unforgettable hospitality experiences, Iraia's success is driven by his keen eye for opportunity, driving professional staff culture, confident and comprehensive service knowledge, and focusing on delivering affordable prices to consumers.

3.3.3 Ben Taaffe - Director, Programming & Engagement

Ben Taaffe is a successful events producer and promoter, program coordinator, researcher, community worker and DJ whose love of music and connection to community has driven a successful twenty-one-year career across diverse fields of practice within the Boorloo (Perth) music industry and academic, arts and community sectors. Ben's engagement in these fields is distinguished by his commitment to the social, political and spiritual value of the arts and music. A belief in the potential to connect, engage, inspire and empower individuals and communities through arts, music and culture drives his practice. Ben is the director of music events company Move, which he established in 2007. Over his career he has built a reputation as a leading figure in the local events industry, producing high quality, safe and inclusive events at numerous venues and sites around Boorloo and collaborating successfully with leading cultural organisations – including Perth International Arts Festival, Western Australian Music Industry Association, Artrage, RTRFM, Fringe World, Bar Pop and Red Bull Music Academy.

Please see **Appendix C** for the Mission and Vision Statement from the Directors for what will become known as Passenger.

4 Site Description & Local Context

4.1 Site Description

The subject land is located within the Northbridge District Centre with excellent exposure to passing trade along William Street and the greater Northbridge, Highgate, Perth area and surrounding suburbs.

The subject land currently includes 2 lots, total area of 988m² with 24.18m frontage to William Street and will be subject to amalgamation as a condition of this planning approval. The lots are legally described as.

- Lot 68 on Plan 613 Volume 1082, Folio 902 (No. 378 William St).
- Lot 67 on Plan 613 Volume 2104, Folio 221 (No. 380 William St).

Please refer to **Appendix D – Certificate of Title**, as well as **Figure 1** below showing cadastral boundaries and **Figure 2 – Aerial Photo**.

Figure 1 – Cadastral Boundaries

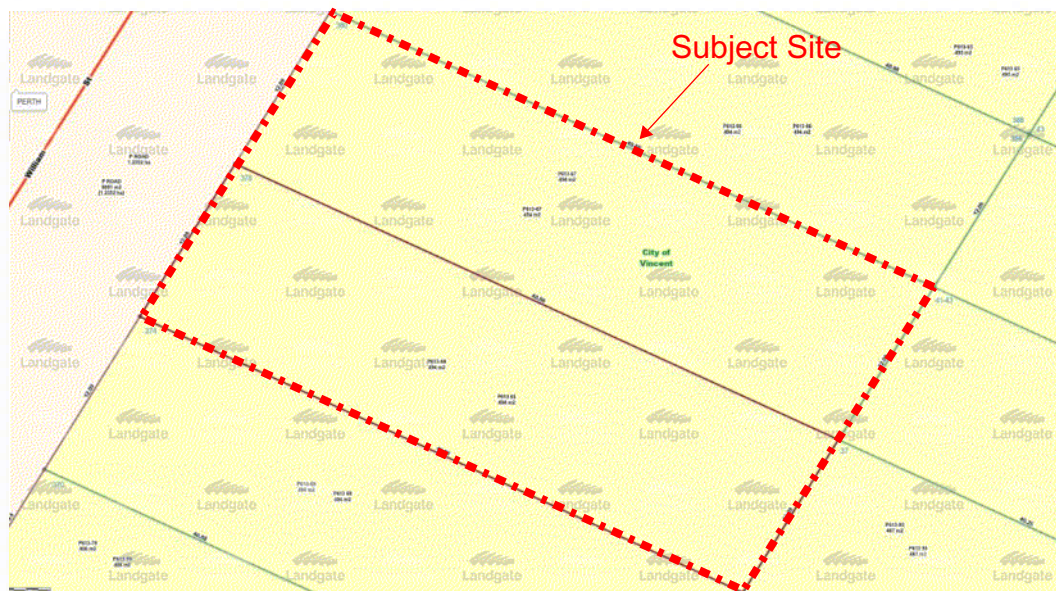


Figure 2 – Aerial Photo



4.2 Location

William Street is currently characterised by single and two storey, unassuming and largely unkempt buildings. With the exception of a few landmark historic buildings the majority of buildings lack any architectural style and contribute little to the streetscape. The land immediately surrounding the subject site comprises of a mix between entertainment, retail and personal services land uses. The broader area comprises a range of different uses including multi-storey residential, hotels, bars, cafés and restaurants as well as educational facilities. The site benefits from its location between the Northbridge and Beaufort Street areas. Stylish built form, strong urban design, rich cultural heritage, strategic placement to the Central Business District and distinctive topography are key characteristics of this area.

Please refer to **Figure 3** below and full map at **Appendix F** for more information.

Figure 3 - Site Context Map, and at Appendix E.



Some of the key characteristics of the site, building on from the William Street Design Guidelines are as follows:

- Proximity to the Perth Central Business District (CBD).
- Distinctive topography providing views to the CBD.
- Landmark buildings with cultural and heritage value.
- Inconsistent architectural styles.
- Degraded and some neglected streetscapes, with limited new built form along William Street.
- Street art and graffiti.
- Varied street setbacks.
- Large street trees and shade.
- Low density, single and two storey commercial development.
- New bars, restaurants and taverns including Wines of While and Vincent, catering to an older more refined audience.

4.3 Existing Use and Development

The site is currently run down with an old warehouse and associated car parking that has not been operational for several years. The entry currently is serviced by a centrally located gate with a bus stop to the south of the entry crossover. The site does not adjoin any residential properties or residential zoned land.

There is a significant amount of transport infrastructure including bike racks, bus stops, taxi and uber car bays and public car parking within close proximity to the subject land. A full analysis of the car parking and access will be undertaken in later sections of this report.

The bus stop immediately adjoining the development is a high frequency bus stop that services the 950 bus every 7-8 minutes during the week and every 10 minutes up until 7pm on Saturday evenings ([link to 950 bus timetable](#)). In addition to this the 980 bus route ([link to 980 timetable](#)) operates every 5 minutes in the peak hour, every 15 minutes in the week and on Saturday until later in the evening. There is effectively a bus at least every 6 minutes during the week and on Saturday until later in the evening that services the subject land from directly out the front.

Please see photos of the site and surrounds below.

Plate 1 – Existing Frontage



Plate 2 – Existing Derelict Building



Plate 3 - Existing Hardstand Car Parking



Plate 4- View North down William St

Plate 5 – View south down William St



Plate 5 – Inside Existing Warehouse



Plate 6 - Inside Existing Warehouse



5 Consultation

5.1 Statutory Authorities

The project team has had several discussions and meetings with senior members of the planning staff in formulating this proposal. This includes meetings on 24th February and 4th July 2023 where numerous issues were discussed across the planning framework, with discussion of pertinent issues to consider, in particular car parking shortfalls and recent decisions of the Development Assessment Panel and City of Vincent Elected Members.

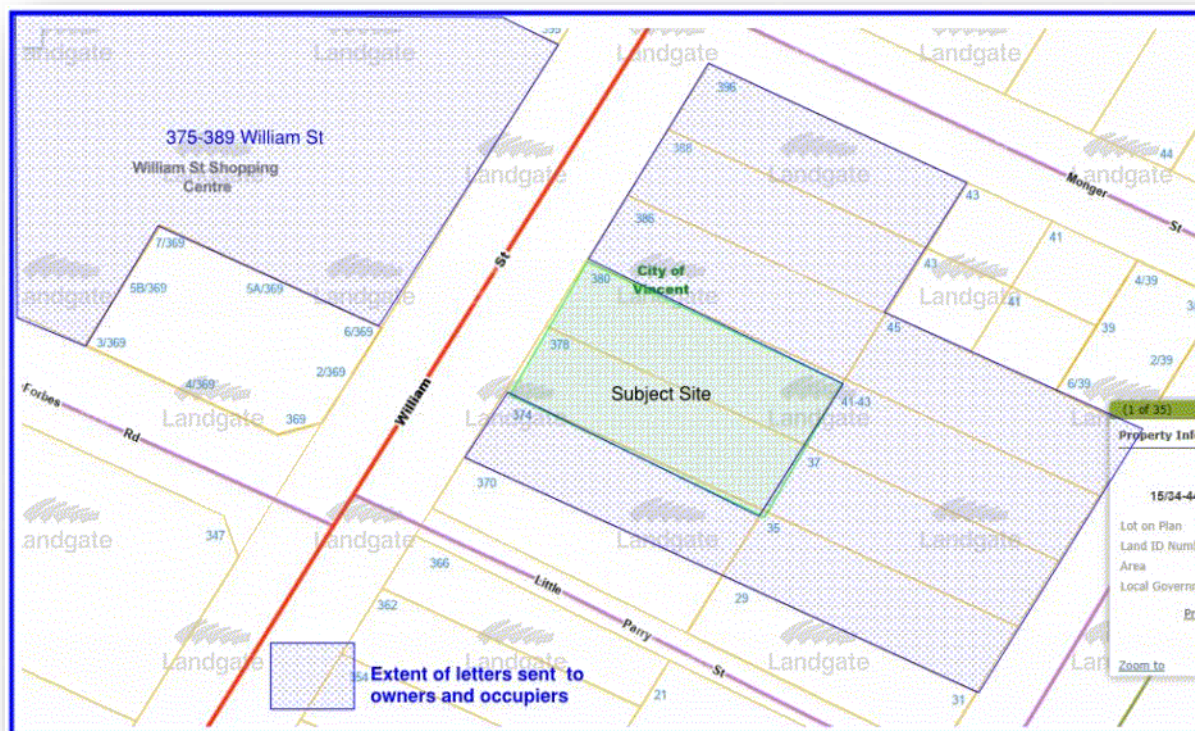
These meetings and correspondence with the City of Vincent planning staff have provided essential input into the Development Application and informed the ultimate outcome proposed.

5.2 Surrounding Properties

Method Planning and the project team acknowledges that early engagement with surrounding businesses and landowners is essential when proposing new active land uses in the area. On 28th March 2023 we sent a letter to all adjoining landowners (see letter at **Appendix G** and extent of mail out area in **Figure 4** below). The adjoining owners and occupiers were invited to a meeting with the project team to discuss the plans and address any issues they may have. The meeting will be held on 13th April 2023 at 5pm at the subject site.

A representative of all owners of the development at 375- 389 William Street (full details can be provided on request) attended the session. They were supportive of the proposal and noted that further activation of the dilapidated warehouse would be a welcome addition to William St and they “look forward to to new development”. There were no other attendees over the 1 hour meet and greet session, indicating ambivalence or general support for a land use that is compatible with the area.

Figure 4 – Extent of engagement with neighbours



6 Town Planning Framework

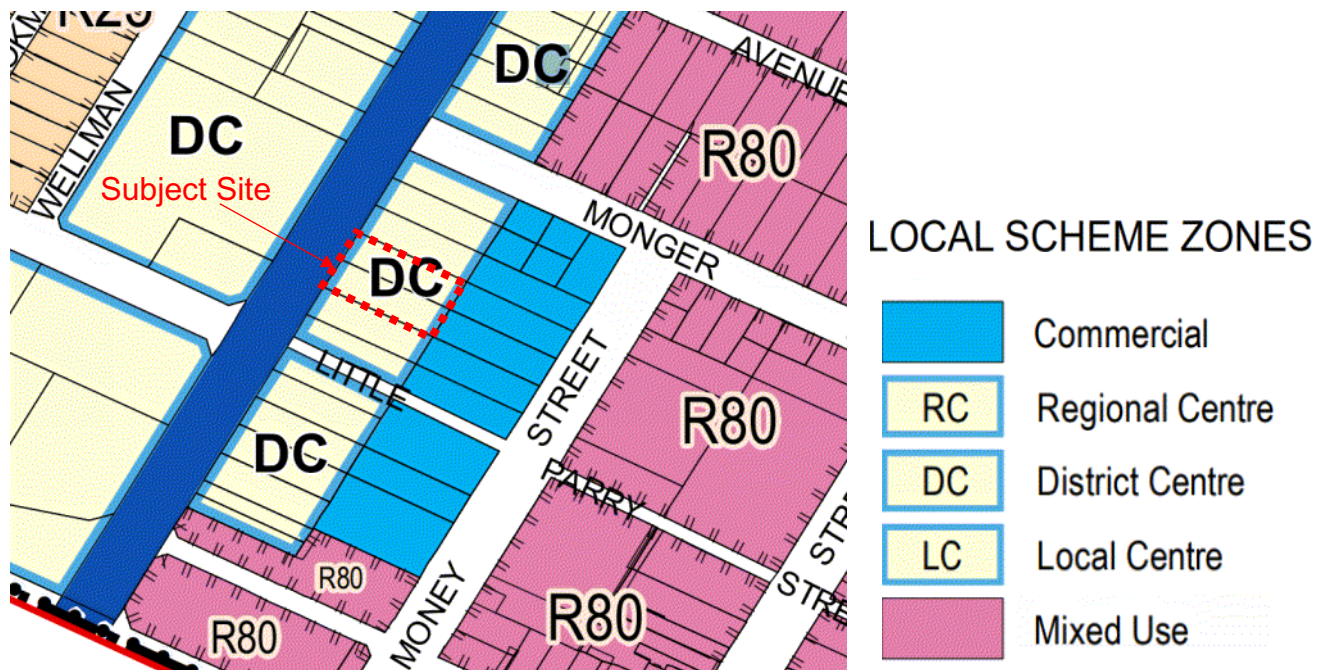
6.1 Metropolitan Regional Scheme

The land is zoned 'Urban' under the Metropolitan Regional Scheme.

6.2 Local Planning Scheme No. 2

The subject site is subject to the provisions set out under the City of Vincent Local Planning Scheme No. 2 (LPS2). Under LPS2, the subject site is zoned "District Centre" as shown in **Figure 5** below.

Figure 5 - City of Vincent LPS2 Zoning



The objectives of the "District Centre" Zone are as follows:

- *"To provide a community focus point for people, services, employment and leisure that are highly accessible and do not expand into or adversely impact on adjoining residential areas.*
- *To encourage high quality, pedestrian-friendly, street-orientated development that responds to and enhances the key elements of each District Centre, and to develop areas for public interaction.*
- *To ensure levels of activity, accessibility and diversity of uses and density is sufficient to sustain public transport and enable casual surveillance of public spaces.*
- *To ensure development design incorporates sustainability principles, with particular regard to waste management and recycling and including but not limited to solar passive design, energy efficiency and water conservation.*
- *To ensure the provision of a wide range of different types of residential accommodation, including affordable, social and special needs, high density residential and tourist accommodation, to meet the diverse needs of the community.*
- *To provide a broad range of employment opportunities to encourage diversity and self-sufficiency within the Centre.*
- *To encourage the retention and promotion of uses including but not limited to specialty shopping, restaurants, cafes and entertainment.*

- To ensure that the City’s District Centres are developed with due regard to State Planning Policy 4.2 - Activity Centres for Perth and Peel.” (LPS2)

6.2.1 Land Use

The proposed land uses under LPS, as per the land uses table are categorised as follows:

Land Use	Permissibility
Shop: means premises other than a bulky goods showroom, a liquor store – large or a liquor store – small used to sell goods by retail, to hire goods, or to provide services of a personal nature, including hairdressing or beauty therapy services;	P
Café: means premises primarily used for the preparation, sale and serving of food and beverages for consumption on the premises by customers for whom seating is provided, including premises that are licensed under the Liquor Control Act 1988, with a net lettable area of not more than 150 m ² .	P
Tavern: means premises the subject of a tavern licence granted under the Liquor Control Act 1988	A
Internet Radio Studio: not specifically referred to in the zoning table	See 4) below

“(2) The symbols used in the zoning table have the following meanings –

P means that the use is permitted if it complies with any relevant development standards and requirements of this Scheme.

A means that the use is not permitted unless the local government has exercised its discretion by granting development approval after giving special notice in accordance with clause 64 of the deemed provisions; “

“(4) The local government may, in respect of a use that is not specifically referred to in the zoning table and that cannot reasonably be determined as falling within a use class referred to in the zoning table -

a) determine that the use is consistent with the objectives of a particular zone and is therefore a use that may be permitted in the zone subject to conditions imposed by the local government.

As per clause 4 above and the objectives of the current ‘District Centre’ zone it is contended that an Internet Radio Studio, with positive impact on the amenity of the area and ability to create activation and vibrancy, is a “Permitted” use. This is best demonstrated by RTRfm on Beaufort Street and the positive impact it has on the surrounding area.

Therefore, under the Town Planning Regulations the only use subject to approval is the proposed ‘Tavern’ use and associated works, notwithstanding that all uses sans the ‘Tavern’ are ancillary land uses.

6.3 Local Planning Policies/Structure Plans

The following local planning policies, adopted under LPS2 are relevant to the planning application:

- Local Planning Policy 7.1.1 - Built Form.
- Local Planning Policy 7.5.7 – Licensed Premises.
- Local Planning Policy 7.7.1 – Non-Residential Development Parking Requirements.
- Design Guidelines for William Street.

Pertinent provisions of each policy have been addressed under the development assessment and justification sections below.

7 Development Assessment

7.1 Land Use & Orderly and Proper Planning

In addition to the assessment and relevant justification provided, the principles of orderly and proper planning require that the land uses and development is consistent with the planning vision for the area. The 'Tavern' land use is compatible with the 'District Centre' zone and meets the objectives of the zone as follows:

- It provides a community focus point for people and leisure activities, particularly local creatives and musicians and is setback from the residential area and acoustically treated to ensure it does not adversely impact on residential areas.
- The frontage and land uses are designed as a fully active and interactive frontage, from breakfast to late at night, improving safety and ensuring a high quality, pedestrian-friendly, street-orientated development, as well as enhancing the use of public transport and other integrated transport options, and reducing the use of private vehicle trips.
- The proposal retains and adapts the existing building which, in addition to maintaining the existing streetscape rhythm, has the benefit of retaining the structures' embodied energy. The adaptation of existing buildings reduces climate impact over the newly built comparison.
- The operation of the development and waste management has demonstrably been shown to adopt best practise principles.
- The development will provide employment opportunities for both hospitality workers (15 at any one time), which matches the younger demographic that resides around this location, as well as employment for artists and performers to perform and play music or DJ at the venue. This aligns with the objective to provide a broad range of employment opportunities to encourage diversity and self-sufficiency within the Centre.
- The development includes the promotion of uses including restaurant, cafe and entertainment.
- The development ensures that the Northbridge District Centre is developed with due regard to State Planning Policy 4.2 - Activity Centres for Perth and Peel.

7.2 Built Form Policy Compliance

Built form is to comply with the Built Form Policy, in particular the Town Centre Built Form Area Commercial Section 1, and the William St Design Guidelines. The following responds to the pertinent policy provisions of the Built Form Policy.

- Clause 1.1 - The building heights are within the policy thresholds (6 storeys).
- Clause 1.2 and 1.3 - The primary street setback is above the nil required, being 5.645m to the radio studio and 13.255m to the main existing building and food service area. Importantly the design meets objectives O1.2.4 – O1.2.6 by ensuring:
 - Street setback facilitates the provision of useable open space, alfresco dining and landscaping which contributes to canopy coverage.
 - The street setback reinforces and complements the character of the street, by adaptive reuse of the existing building and forecourt.
 - The street setback and land uses enable passive surveillance and outlook to the street.
- Clause 1.4 - Building orientation has no impact on habitable rooms, open space and solar collectors of neighbouring properties during mid-winter and the building layout responds to the streetscape and site attributes, while addressing acoustic reporting requirements.
- Clause 1.5 - Over 12% deep soil provided (128m², see Landscape plan A104), with significant planting and canopy area to the beer garden and frontage.

- Clause 1.7 - The transition between the private and public realm has been designed to enhance safety and although the bin store is within the primary street setback it is concealed at the north end of the site. It has been integrated into the design of the development and does not detract from the amenity and visual appearance of the overall design or active and interactive frontage, especially considering it facilitates the removal of a vehicle crossover.
- Clause 1.8 - The development meets all the acceptable outcomes of Clause 1.8 with an active and interactive frontage that has direct access to the street, with future plans for a parklet next to the bus stop.
- Clause 1.9 – There is no vehicle access directly to the site to disincentive people from driving with private vehicle to a Tavern and to improve the pedestrian experience by removing a crossover.
- Clause 1.10 - Secure bicycle parking is provided over and above what Local Planning Policy 7.7.1 – Non-Residential Development Parking Requirements would require, this will help to incentive alternative transport modes, noting the heat maps below showing strava data on people walking and / or cycling towards and around William Street and the subject site. Further detail is provided in the Transport Impact Statement and Car Parking Management Plan on how car and bicycle parking is managed.

Figure 7 – Strava Pedestrian Heat Map - William Street (accessed 11 May 2023)

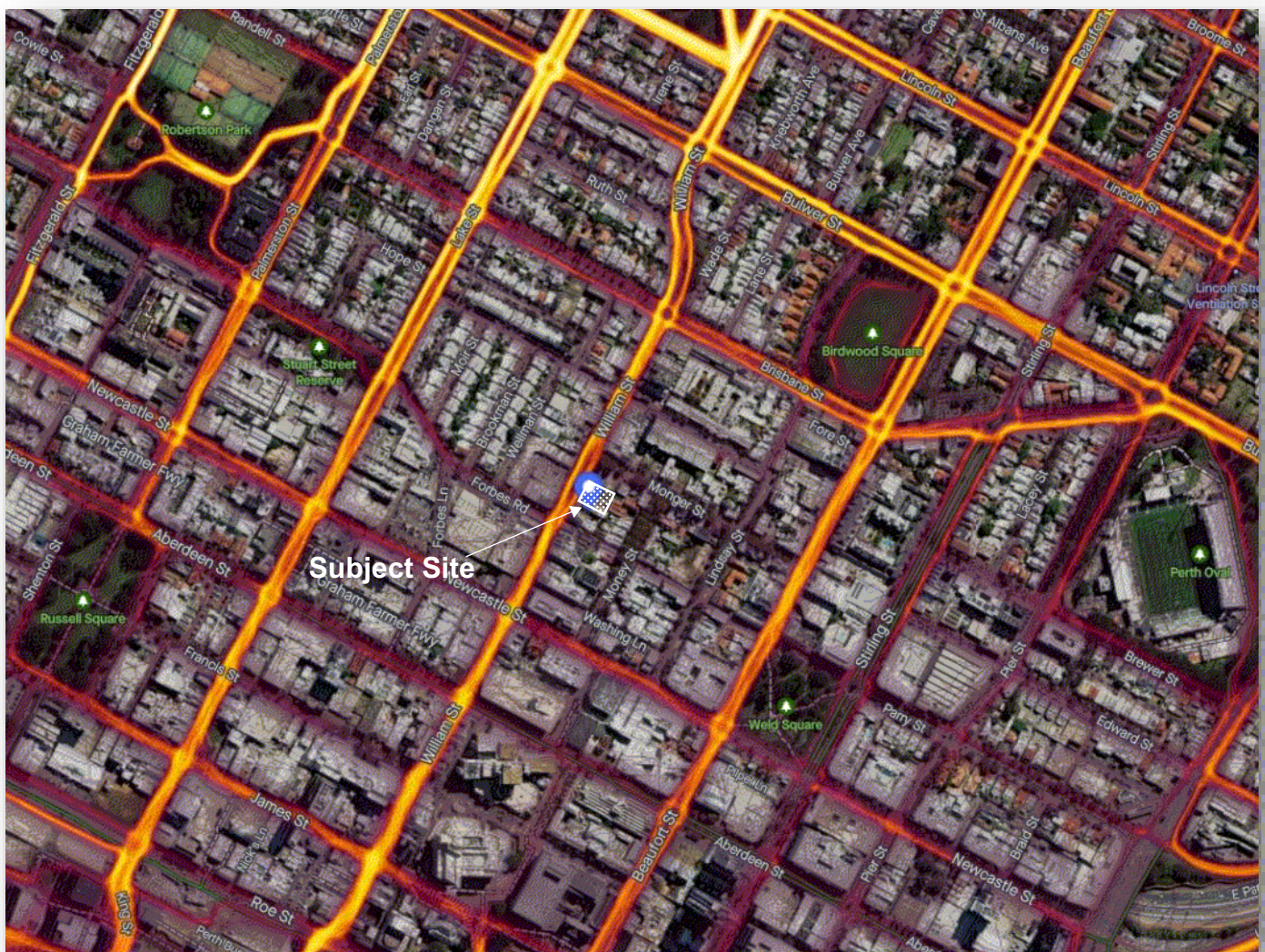
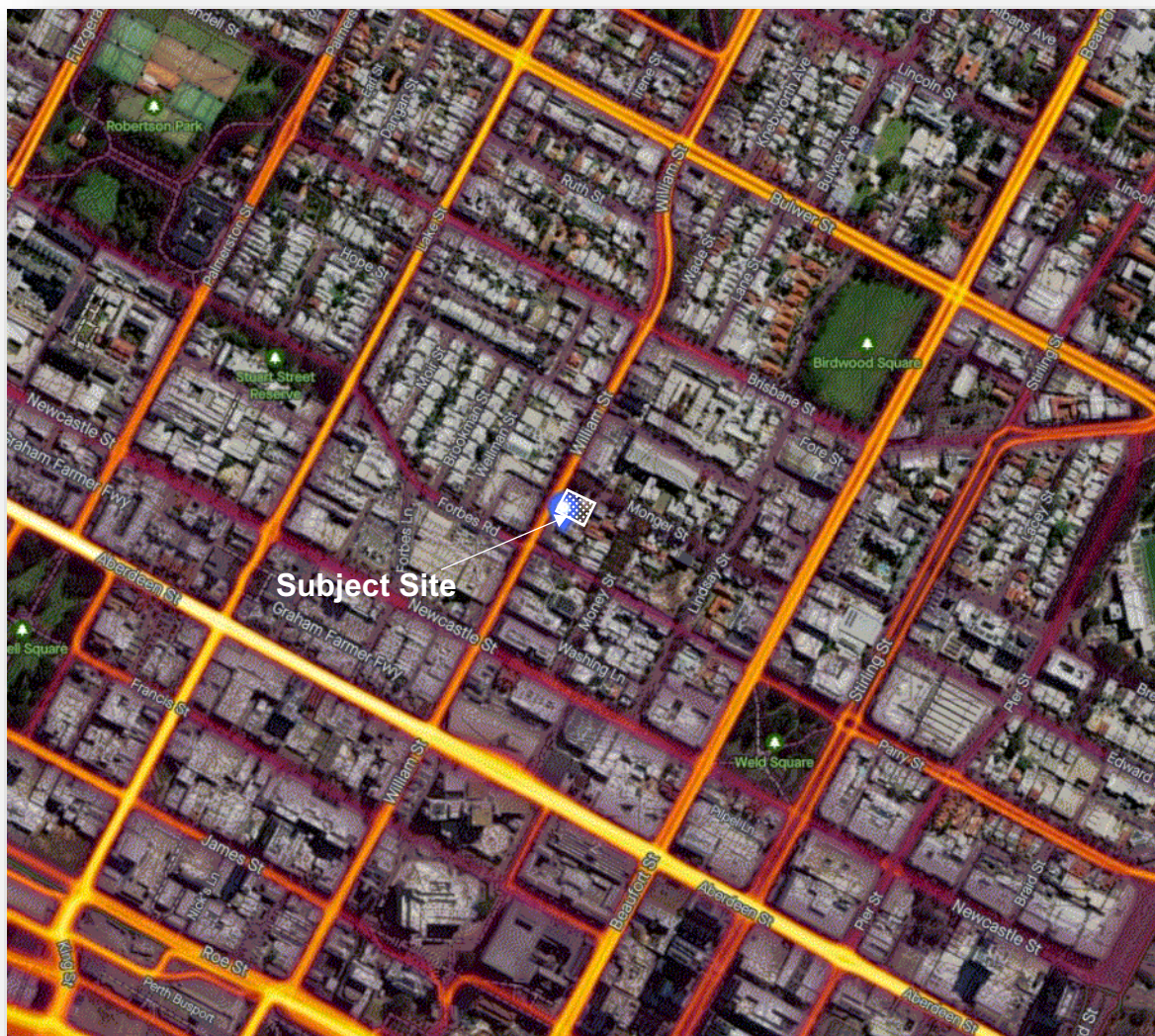
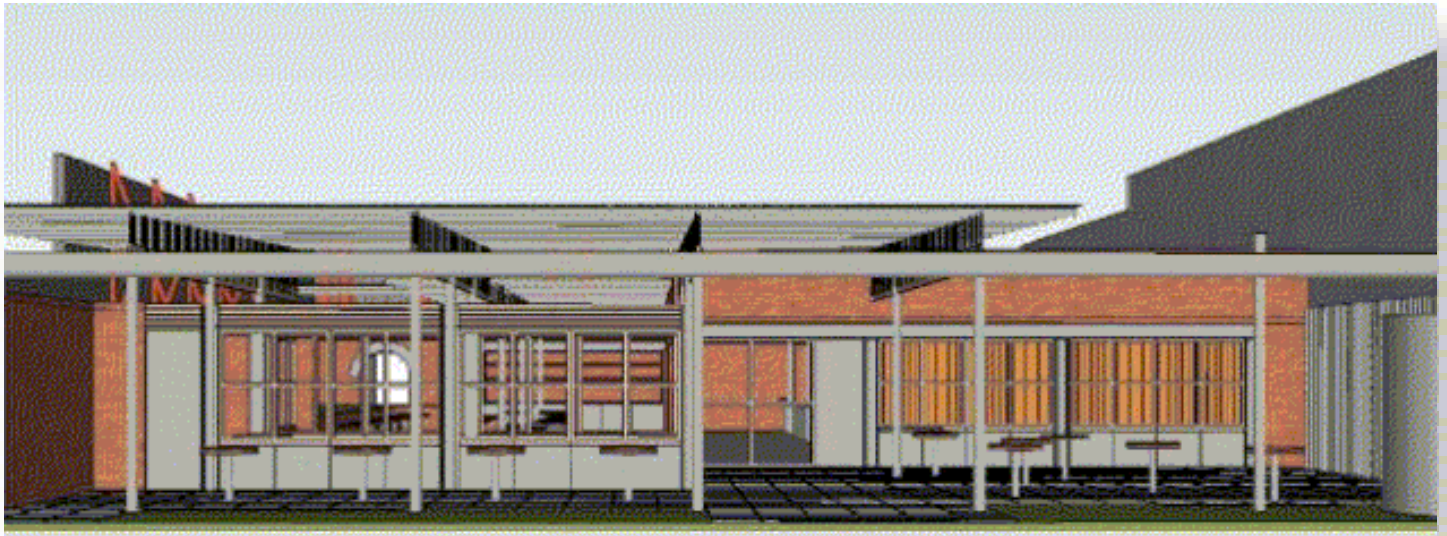


Figure 8 – Strava Cycling Heat Map - William Street (accessed 11 May 2023)



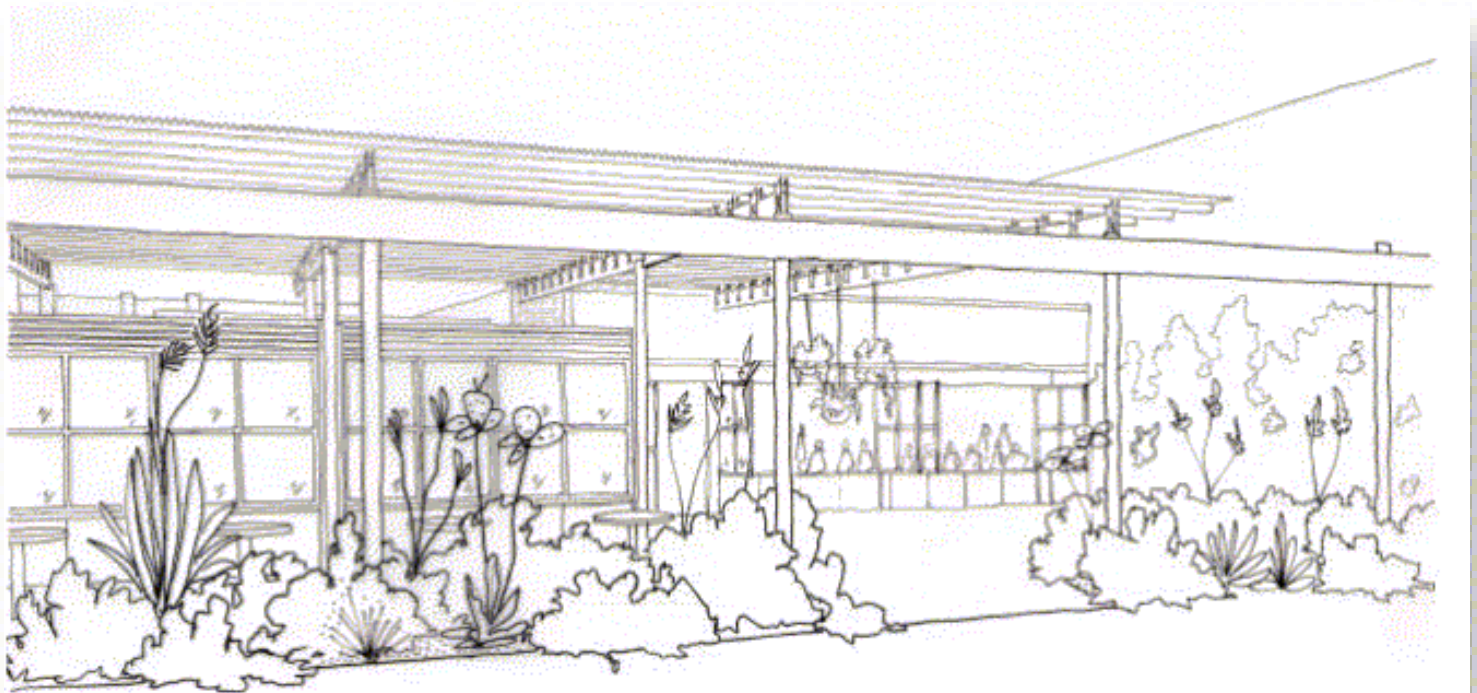
- Clause 1.11 – There are no adjoining residential properties, however an acoustic report has been completed and the measures in it will be adopted, see detail under Licensed Premises Policy No. 7.5.7 section.
- Clause 1.12 – The development is set on one level with UA facilities and therefore meets the needs of people living with disabilities.
- Clause 1.13 – Facade Design – The development presents an active frontage including glazing, openings and operable windows to ensure activity, interaction and surveillance of the street – See **Figure 9** below. Some other aspects in addressing the façade and adaptive reuse of an existing building include:
 - Adding openings to the existing warehouse, and adding retail and café uses as well as alfresco areas to the frontage.
 - There are no faux materials, and the development demonstrably responds to the local area and streetscape by the adaptive reuse of an existing building.
 - No blank, monotonous, repetitious, or dominant walls face William Street.
 - The proposal includes the adaptive reuse of an existing warehouse structure where the height and width of the building is already defined.
 - The inclusion of alfresco areas and interactive uses ensures the development is transparent and visually permeable to allow views inside the building and enable internal light sources to be seen from the street.

Figure 9 – Proposed Frontage



- Clause 1.14 – Roof Design – the proposed roof form is well integrated into the building design and responds positively to the street.
- Clause 1.15 – Landscape Design – the Landscape design enhances streetscape and pedestrian amenity and improves the visual appeal of the development. This includes water efficient irrigation systems, and incorporates water harvesting and water re-use technologies, please see proposed water tank. Please see **Figure 10** below detailing landscaping within the development.

Figure 10 – Proposed Landscape Design



- Clause 1.16 – Adaptive Reuse and Clause 1.17 Environmentally Sustainable Design (ESD) – The proposal retains and adapts the existing building which, in addition to improving the existing streetscape rhythm, has the benefit of retaining the structures’ embodied energy. When compared to construction of an entirely a new premise, the adaptive reuse of the existing masonry structure not only ensures the maximum use of existing material lifespans but also reduces (construction) waste and associated emissions. With respect to a life cycle analysis used to review new construction energy impacts, with few exceptions, comparing similar uses, types and locations, the adaptation of existing buildings reduces climate impact over the newly built comparison. Findings suggest that even sustainably constructed newly built structures do not recoup energy outlays for approximately 30 years when measured against a renovated existing building. See also statement from ESD expert at **Appendix H**, detailing the ESD principles underpinning the design and operation of the development.
- Clause 1.18 – Water management and Conservation There is a rainwater tank on site. Stormwater runoff generated from small rainfall events is managed on-site.
- Clause 1.19 - Waste Management - Sufficient area is provided to accommodate the required number of bins as detailed in the Waste Management Plan (WMP) in Section 7.6 below. It is sited and designed to be screened from view from the street and the waste is to be collected as per the WMP.
- Clause 1.17 – Utilities - The site is serviced with power, water, gas, wastewater, fire services and telecommunications/broadband services that are fit for purpose.

7.3 William Street Design Guidelines Compliance

The pertinent objectives from the William Street Design Guidelines relating to the reuse of the existing building for tavern, shop, café and radio studio land uses and development are as follows:

- “i) To maximise the opportunities afforded by the area’s proximity to the Perth Central Business District, major public transport routes, road networks and gateway to the City of Vincent.*
- ii) To provide clear guidance to landowners and developers with respect to development requirements for William Street.*
- iii) To provide car parking requirements which are cognisant of the unique nature and range of uses existing and those attracted to the area.*
- vii) To maximise opportunities for redevelopment of undercapitalised/underdeveloped properties.*
- ix) To encourage the principles of sustainability and ‘green building’ techniques.*
- x) To create premier examples of robust building forms of good quality and design, catering to a variety of uses within a unique inner-urban environment.*
- xi) To build on the sense of place evidenced by the area’s history and cultural diversity.*
- xii) To ensure the provision of awnings, along William Street in any new or redeveloped property.*

The report has stepped out how the proposal meets these objectives with the following additional justification against the detailed build form guidelines under section 7 of the policy:

- The policy details the *encouragement of shops... restaurants*, for the area between Newcastle and Brisbane Streets, which we are providing.
- We are developing an under-utilised property by reusing the existing warehouse, which means a single storey development. The development would be unviable if required to be 2 storeys, especially with current building costs, the land uses proposed and reusing the current structure.
- Building colours and materials are to be lively and at a uniformly high quality and reference the existing brick finish and concrete flooring that exists on site and includes a parasol roof to match the style of roofing in the area while meeting environmental objectives to access breeze – See articulation of roofing and frontage below at **Figure 11**.

Figure 12 – West Elevation



- There are no nil setbacks across the front but instead an alfresco area that ensures *William Street being interconnected with the streetscape to ensure active street frontages and encouragement of pedestrian activity*.
- Glazing is *in the form of smaller windows in a regular pattern and the windows and doors are of a vertical, rectangular format*.
- No awning is provided as the development is to an existing building and covered alfresco areas are provided in the front setback area. There is also significant weather protection from the existing plane trees, which were planted around the time of the policy adoption (2008).
- Bin storage is not visible from the street, it is screened behind gates.
- As detailed on drawing A104, high quality landscaping is provided to help *enrich William Street and contribute to the creation of a sense of place in the local environment*.
- The key principles of Crime Prevention through Environmental Design (CPTED) are employed through the use of large open areas with active land uses along the frontage, and providing passive surveillance from breakfast through to later in the evening.
- Sound attenuation strategies will be implemented as part of the acoustic report recommendations.

7.4 Transport & Access

7.4.1 2021 Census Analysis

The premises will be easily accessed on foot and on bicycle, scooter or other forms of active transport by local residents, students and business employees in the neighbourhood. The venue will be a locally focused and frequented premises that will be a neighbourhood venue for those living in and close to the locality, catering to residents within the locality who are more likely to walk, ride or use public transport than drive a car. 2021 ABS census data indicates that people residing in the Northbridge area are over 5 times more likely to ride to work and 10 times more likely to walk to work. 2021 ABS census data on car ownership and preferred methods for travelling to work for the suburbs in the 2km radius surrounding the venue show a strong trend towards residents living close by to the venue preferring active and public modes of transport over motor vehicles in their choice of mode of transport to work. Residents in this area are:

- over 3 times more likely to live in a household without a car (16% of all households) compared to the average respondent in the Greater Perth area (4.8%).
- over 3 times more likely to cycle to work (1.8% of all employed residents) compared to the average respondent in the Greater Perth area (0.6%).
- over 7 times more likely to walk to work (11.7% of all employed residents) compared to the average respondent in the Greater Perth area (2%).
- more than twice as likely to take public transport to work (19.4% of all employed residents) compared to the average respondent in the Greater Perth area (8.4%).

Looking only at the areas closest to the venue shows these trends become significantly stronger as the distance to the venue decreases. For example; residents in the neighbouring suburb of Northbridge are over 14 times more likely to choose to walk to their place of employment as their primary mode of transport (23.7% of residents), compared to the average respondent in the Greater Perth area (1.6%); or residents in the vicinity of City of Vincent's "Northbridge Town (District) Centre" are over 4 times more likely to choose to cycle to their place of employment as their primary mode of transport (2.6% of residents), compared to the average respondent in the Greater Perth area (0.6%).

In light of these figures, several features of the venue can be shown to support alternative, active and public transportation choices for clientele and as such align with City of Vincent strategic goals for enhanced neighbourhood connectivity, sustainable transport and healthy and active residents:

- A Transperth bus stop for route 680 is situated directly at the front of the premises (with buses every 6 minutes, refer above) , providing a highly convenient access point for those arriving to or leaving the premises by public transport.
- City of Vincent has identified William Street as one of its endorsed cycle routes as a "primary route" and a priority cycleway for scheduled improvements in their "Bike Plan" strategy document. As stated in the document;
Bike riding, along with walking, is the most sustainable form of transport, contributing to creating great places, cleaner local environments, healthier lifestyles and providing economic benefits to local neighbourhoods. The City wants to increase the number of people riding and walking to work from 15% of residents in 2020 to 17% of residents by 2025 and 20% of residents by 2030. The City wants bike riding and walking to be the preferred mode of transport for short trips to, from and within the city and a viable, safe and efficient option for longer trips. By encouraging more people to choose bike riding and walking for everyday trips, we will improve neighbourhoods and the environment, reduce car use and traffic congestion and improve the general health of our community.

- The venue will ensure secure bicycle parking, in excess of LGA requirements, will be available on the premises during operational hours, in the secure front area of the venue; and end-of-trip facilities will be installed for employees of the venue to encourage active transport choices.
- The venue is also conveniently located within a 5-minute walk from North Metropolitan TAFE, a 10-minute walk from the new Perth City campus of Edith Cowan University and the Western Australian Academy of Performing Arts (currently under construction), including their school of music, and a 15-minute walk from SAE Creative Media Institute. Staff and students from these important creative and educational institutions within the locality are considered to be part of the Venue's primary target audience group alongside local residents and employees.
- Marketing and promotional activities will be used to target and promote the venue to local residents specifically, such as "locals night" food specials, local letter drops with special offers and of course the venue's locally focused cultural programming, see **Appendix 1**.

7.4.2 Local Planning Policy No. 7.7.1 Non -Residential Parking Requirements

Please see below for the calculations for car and bicycle parking against the policy provisions of LPP 7.7.1.

Table 3 – Car Parking Calculations

Non-Residential Car Parking Standards				
Land Use	Town Centre Requirement	Yield	Required Car Bays	Provided Car Bays
Tavern	0.15 Per person	350 + 15 = 365 persons	54.75	0
Café	Ancillary Use, Part of Tavern	Nil	0	0
Shop	Ancillary Use, Part of Tavern	Nil	0	0
Restaurant	Ancillary Use, Part of Tavern	Nil	0	0
Internet Radio Studio	Ancillary Use, Part of Tavern	Nil	0	0
Subtotal			54.75	0
Local supply public bays, scooter bays and integrated transport use				54.75
Actual shortfall				0

Table 4 – Bike Parking and End of Trip Calculations

Land Use	Town Centre Requirement	Yield	Required Bicycle Bays	Provided Bicycle Bays
Tavern - Short Term	0.019 per person	350 persons (patrons)	6.65	23
Tavern - Long Term	0.042 per person	15 persons (staff)	0.63	1
Café	Ancillary Use, Part of Tavern	Nil	0	0
Shop	Ancillary Use, Part of Tavern	Nil	0	0
Restaurant	Ancillary Use, Part of Tavern	Nil	0	0
Internet Radio Studio	Ancillary Use, Part of Tavern	Nil	0	0
Subtotal			7.28	24
Surplus				16.72
EOT Facilities	1 per 5 long term bays	1	0.2	1

Where a development application does not satisfy the policy provisions, the proposal is to be assessed against the objectives of the LPP 7.7.1. In addressing the objectives of the Policy we detail the Transport Impact Statement and Parking Management Plan undertaken by Move Consultants (Traffic Engineers and consultants) and detailed below.

7.4.3 Transport Impact Statement

It should be acknowledged at the outset that 55 car bays or up to 1,485m² of bitumen hardstand does not meet the City's policy objectives or best practice sustainability principles. Importantly the expert analysis and survey of available car bays and integrated transport usage by Move Consultants, as detailed in the Transport Impact Statement at **Appendix I**.

Driving to a licensed premise is no longer the norm and the majority of patrons seek out alternative modes of transport. It is not feasible or practical to provide such parking numbers due to the location of the subject site, its size, and the available developable area.

Hence, the operation of the proposed development will rely on walking, electric scooter riding, pedal and electric bike riding, public transport (both train and bus), taxi/Uber services and ride-share options for patrons and employees in lieu of more traditional private vehicle transport to and from the site, which in return reduces the demand for parking. Accordingly, the proposed development does not contemplate provision of any formal car parking for employees or patrons.

Instead, as a result of the proposed closure of the site's existing crossover to William Street, improving the pedestrian amenity of the area an opportunity is presented for creation of on-street dropoff/pick-up bays – consistent with what was approved at The Beaufort in Mt Lawley. Sound Hospitality proposes to fund this through a cash -in -lieu payment equivalent to 1-2 car bays.

The site's frontage to William Street is 22m long of which approximately 5-6m is allocated to a Bus Stop which leaves 16-17m of frontage available to accommodate the drop-off/pick-up bays. This length would be suitable to accommodate 2 bays for such purpose. It is recommended that the proponent, through liaison with the City of Vincent, prepares a concept plan for such facility in the subsequent stages of the project.

It is considered that the available public parking opportunities in the vicinity of the site are sufficient to accommodate any potential parking demand from the subject development. Accordingly, Move undertook a parking utilisation survey within convenient 5 minutes walking distance from the site (principally a 400m radius) to investigate the parking availability during the anticipated peak operation times of the proposed hospitality venue.

The parking utilisation survey for the existing public parking facilities was undertaken at the following peak hours times of the subject land uses:

- Friday 21st July 2023 between 12noon and 2PM and 5PM and 10PM.
- Saturday 22nd June 2023 Between 5PM and 10PM

These peak operation periods for the proposed venue generally coincide with the weekday lunchtime and dinner and Saturday dinner peak activity periods for surrounding restaurants, bars and cafés. The parking survey was recorded in 30-minute time intervals with full details of the zones and available parking detailed in the Transport Impact Statement at **Appendix G**.

7.4.4 Parking Management Plan

As part of the proposal this report includes a Parking Management Plan (PMP) please refer **Appendix J**. The PMP outlines the follow which address the policy objectives and site responsive design.

- The subject site is located within the Northbridge District Centre and William Street entertainment precinct and the proposal allows for multi-purpose trips for people visiting the site and the entertainment precinct.
- The subject site is located within close proximity to high frequency public transport via the Perth Train Station, which is approximately 600 metres away, and bus stops for the No. 980 bus route (directly out the front of the subject site) which provides connections to Mount Lawley and the Central Business District.
- Maximum staff numbers of 15 any one time, with various shift starting and finishing times meaning one EoT facility is considered sufficient. All bike bays are secured as they can be viewed by patrons and staff, and also covered by the venues CCTV security system.
- The subject site is located less than 300 metres from the free Blue CAT bus, that runs along Aberdeen Street.
- In total there are up 342 public car bays within 400m of the site, and a significant amount of these are available at peak times of the venue as follows:
 - Fridays 10am – 2pm – 53-73 bays available.
 - Fridays 5:00pm – 10:00pm – 58-189 bays available.
 - Saturdays 5:00pm – 10:00pm – 48-109 bays available
- The subject site is near a significant number of public car parks is, opposite the subject site and others as detail in the parking survey undertake by Move Consultants.
- Nature of land use, being a Tavern and a licensed premises, is such that it is expected that a large proportion of patrons would choose not to drive to the venue in private vehicles and to instead opt for taxi, ride-share, public transport, walking, cycling or other alternate modes of transport.
- The subject site is located within the William Street Activity Centre which is a high amenity area. And this location provides the opportunity for multi-purpose trips for people visiting the site and other premises within the entertainment precinct. This reduces dependence on single person private vehicle trips, consistent with the objectives of the Parking Policy.
- Consistency with other venues and Council's and the Development Assessment Panel's application of LPP 7.7.1, where limited, if any, staff and patron parking is provided on-site for other venues, this includes:
 - The Beaufort Hotel (600+ patrons and no on-site vehicle parking for patrons).
 - Bar 399 (no on-site vehicle parking for patrons).
 - Brisbane Hotel (no on-site vehicle parking for patrons).
 - Vincent Wine Bar (no on-site vehicle parking for patrons).
 - Wines of While (no on-site vehicle parking for patrons).
 - Toms Providore and Wine Bar (no on-site vehicle parking for patrons or staff).
 - Ellington Jazz Club (no on-site vehicle parking for patrons).
- Arguably the subject site has the best access to alternative transport modes and best addresses the Policy objectives of all these approved venues with the trifecta of:
 - 1) Not adjoining the residential area.
 - 2) Best access to Perth Train Station of all listed venues and bus stops out the front of the venue.
 - 3) A public car park on its doorstep with a new car park (Lindsay St Secure Park) proposed to be opening shortly with 40 new bays in the area.

7.5 Licensed Premises Policy No. 7.5.7

The following information and reporting details the appropriateness of the proposed development under the provisions of Local Planning Policy - Licensed Premises Policy No. 7.5.7.

7.5.1 Street Frontage

As detailed above in the Build Form Guidelines justification, the frontage currently has fencing and zero activation in front of an unused warehouse. The new development will include removal of a crossover and alfresco areas, a café, shop and internet radio studio, demonstrably improving the passive surveillance and creating an active and interactive frontage, from the am hours until later into the evenings.

7.5.2 Number of Patrons

There are no restrictions on the number of patrons for taverns. The number of patrons is estimated at 350 plus 15 staff, final numbers will be given when the building works are complete.

7.5.3 Hours of Operation

Trading hours as per the policy as follows:

Day (s)	Trading Hours	
	Indoor areas	Outdoor areas
Monday - Saturday	7.00am to midnight	7.00am to midnight
Sunday and Public holidays	7.00am to 10:00am	7.00am to 10:00am
Extended Trade Permits	As approved	

7.5.4 Management Plan

Please see Venue Management Plan attached at **Appendix K**, addressing the numerous matters detailed in this report and the accompanying technical reports and Public Interest Assessment. This will be refined to address conditions of the Liquor License and Planning Approval.

7.5.1 Public Interest Assessment

As per clause 3.8.1 please see attached the Public Interest Assessment at **Appendix L**, plus details in this report, including **Appendix F – Site Context Mapping**, that address such matters as distribution and mix of land uses, including residential, shops, restaurants, community facilities, public open spaces and other licensed premises, within 400 metres of the premises.

7.5.2 Acoustic Report

The attached Acoustic Report presents the findings of the noise assessment conducted by Acoustics Consultants Australia. A copy of the report is at **Appendix M**:

The aims of the assessment are:

- To identify the main sources of noise from the proposal and the nearest noise sensitive receivers.
- to conduct an objective noise assessment based on a 3D noise model calibrated with measurements conducted at similar venues during busy operations; and
- To provide recommendations that will set basis for noise management, where required.

A summary of the recommendations are detailed below:

- Music to be played at background levels only. Indicatively, not to exceed LAeq,5min 77 dB at listeners' locations.
- Loudspeakers shall be installed with appropriate resilient mounts to stop vibration or resonances being transmitted to the building structure. A distributed network of speakers should be sought in place of large stereo system.
- Fit sound absorption panelling to the internal bar to achieve reverberation time of 0.6 seconds.
- Install short barriers on the rooftop next to the mechanical plant. The barriers shall be installed in such a way to break the line of sight between the residential receivers and the mechanical units.
- 6m high boundary barrier to the northeast of the site (see elevations).
- Building and roof penetrations: ensure all penetrations (mechanical, electrical or hydraulic services) are treated to fill any gaps.
- Set up a noise/measuring management device.
- Limit waste disposal to daytime hours.
- Limit goods' delivery to daytime hours.

7.6 Waste Management Plan

Sound Hospitality has developed a comprehensive waste management plan to optimise recycling efforts; establish in-house composting facility and practices; train and support staff to foster a green employee culture; work with suppliers to minimise packaging waste and promote sustainable practices. A copy of the full Waste Management Plan is contained at **Appendix N**.

Projected waste has been calculated using information available from Waste Authority WA. The table below is the total projected waste generated weekly at Passenger from a 3 Bin system:

Table 5 – Waste Generation

Venue category	Size (Patron space sqm)	General Waste /L	Recycling /L	FOGO /L
Indoor Bar (Tavern)	150	4200	3150	630
Outdoor Bar (Tavern)	350	9800	7350	1470
Dining Room (Restaurant)	100	3500	2100	840
Retail	80	280	140	

Passenger has engaged with Cleanaway for all waste disposal services. The following bin quantities and collection frequency will be provided.

Table 6 – Bin Quantities and Collection Frequency

Type	Size / L	Quantity	Collection Frequency p/week
General Waste	1100	3	4
Recycling Comingle	1100	3	4
FOGO	1.5m3	1	2

7.7 Amalgamation

As detailed above the development straddles two lots, which are to be amalgamated prior to occupation of the development and expectation it will be conditioned as such on any approval issued.

7.8 Stormwater Management

All stormwater will be designed to be contained on site, with a stormwater management plan to be submitted with the Building Permit application.

7.9 Signage

Signage will be minimised and located and sized to comply with the Signs and Advertising policy.

7.10 Staging

Due to the current uncertainty with building costs and supply of materials and equipment, the proponent is requesting the development potentially be staged in 2 stages. This would mean building the toilets and amenities for the ultimate capacity as part of Stage 1 but some of the kitchen and fit out equipment may be delayed until stage 2.

8 Conclusion

This report has been prepared by Method Planning on behalf of Sound Hospitality Pty Ltd in support of an application to reuse an existing warehouse for a tavern, shop, café and internet radio studio at a dilapidated site on 378-380 William St, Perth.

Designed by a collective team of experts in hospitality, inner city and sustainable design, the proposed development will regenerate a dilapidated building on William St and provide a framework for integrated transport usage. The proposal positively engages with and addresses William Street and the Northbridge District Centre in an exemplary and appropriate way through the design of the building and public interface.

Based on the detailed planning assessment presented in this report, it has been demonstrated that the proposal is consistent with or exceeds the intent of the relevant planning framework and is closely aligned with the City of Vincent's vision for the Northbridge District Centre and William St area.

This report provides appropriate justification where variations are sought to the planning framework, and the report notes that where alternative solutions are sought, the development is still consistent with the vision and the relevant objectives of the planning framework.

In light of the above, favourable consideration by City of Vincent administration and approval by the Elected Members is respectfully requested.

Appendix A - Example Cultural Program Schedule

PASSENGER - CULTURAL PROGRAMMING SCHEDULE

(EXAMPLE OF MONTHLY SCHEDULE)

WEEK 1

Day	Event & Time
Monday	Art Exhibitions Opening - Lounge and Dining areas (5:00 PM - 9:00 PM)
Tuesday	"Chess Club" - Relaxed Music and Casual Community Chess Club - Lounge (6:00 PM - 9:00 PM)
Wednesday	Live Radio Programming (4:00 PM - 9:00 PM) Radio Presenting & Podcasting 101 Workshop - Media Studio (1:00 PM - 3:00 PM)
Thursday	Live Radio Programming (4:00 PM - 9:00 PM)
Friday	Local Music Release - "Listening Party" Lounge (7:00 PM - 11:00 PM)
Saturday	Live Radio Programming (4:00 PM - 9:00 PM), Guest Selector Session - Listening Lounge (9:00 PM - Midnight)
Sunday	Live Acoustic Jazz - Garden Bar (11:00 AM - 2:00 PM), Community Fundraiser (2:00 PM - 4:00 PM)

WEEK 2

Day	Event & Time
Monday	Creative Workshop Series - Eg: Live Art Class.
Tuesday	Listening Event - Music Feature - Classic Artist Showcase - Listening Lounge.
Wednesday	"Meet Your Maker" - Local Food or Beverage Producer / Supplier Activation: Garden Bar (6:00 PM - 9:00 PM), Radio Programming (4:00 PM - 9:00 PM)
Thursday	Live Radio Programming (4:00 PM - 9:00 PM) Radio Presenting & Podcasting 101 Workshop - Media Studio (1:00 PM - 3:00 PM)
Friday	Resident Selector Session - Listening Lounge (6:00 PM - Midnight)
Saturday	Live Radio Programming (4:00 PM - 9:00 PM), Guest Selector Session - Listening Lounge (9:00 PM - Midnight)
Sunday	Live Acoustic Jazz - Garden Bar (11:00 AM - 2:00 PM), Community Plant and Record Sales (12:00 PM - 3:00 PM)

WEEK 3

Day	Event & Time
Monday	Creative Workshop Series - Eg: Live Art Class.
Tuesday	"Chess Club" - Relaxed Music and Casual Community Chess Club - Lounge (6:00 PM - 9:00 PM)
Wednesday	Live Radio Programming (4:00 PM - 9:00 PM) Radio Presenting & Podcasting 101 Workshop - Media Studio (1:00 PM - 3:00 PM)
Thursday	Live Radio Programming (4:00 PM - 9:00 PM)
Friday	Resident Selector Session - Listening Lounge (6:00 PM - Midnight)
Saturday	Live Radio Programming (4:00 PM - 9:00 PM), Guest Selector Session - Listening Lounge (9:00 PM - Midnight)
Sunday	Live Acoustic Jazz - Garden Bar (11:00 AM - 2:00 PM)

WEEK 4

Day	Event & Time
Monday	Art Exhibitions Closing - Lounge and Dining areas (5pm-9pm)
Tuesday	Listening Event - Music Feature - Classic Artist Showcase - Listening Lounge.
Wednesday	Live Radio Programming (4:00 PM - 9:00 PM)
Thursday	Live Radio Programming (4:00 PM - 9:00 PM) Radio Presenting & Podcasting 101 Workshop - Media Studio (1:00 PM - 3:00 PM)
Friday	Resident Selector Session - Listening Lounge (6:00 PM - Midnight)
Saturday	Live Radio Programming (4:00 PM - 9:00 PM), Guest Selector Session - Listening Lounge (9:00 PM - Midnight)
Sunday	Live Acoustic Jazz - Garden Bar (11:00 AM - 2:00 PM)

INTERNET RADIO - INDICATIVE PROGRAM SCHEDULE

(EXAMPLE OF ONLINE CONTENT ONLY - PRODUCED ONSITE).

Monday to Thursday (2 PM - 11 PM)

Time	Program	Description
2:00 - 5:00 PM	"Urban Beat Lounge"	3 hours of curated down beat and electronic vibes to kickstart the afternoon.
5:00 - 7:00 PM	"Voices"	Showcasing brand new releases, local talent & interviews with artists.
7:00 - 9:00 PM	"Rhythmatism"	A journey through culturally diverse music, celebrating global culture and emerging sounds with rotating resident selectors and special guests.
9:00 - 11:00 PM	"Vinyl Revival"	A discerning trip through classic vinyl records and timeless tracks curated by the city's finest selectors

Friday to Saturday (9 AM - Midnight)

Time	Program	Description
9:00 - 12:00 PM	"Good Morning Grooves"	Start the day with upbeat tracks and weekend vibes from a rotation of local selectors
12:00 - 2:00 PM	"Discovery"	A platform for independent and underground artists across various genres like rock, pop, folk, and alternative. This would be a space for new and undiscovered talents to showcase their music, coupled with interviews and insights into the local indie scene.
2:00 - 5:00 PM	"Urban Tribes"	Exploring the heart of city cultures and subcultures, highlighting diverse music, art, and lifestyles. Tune in to discover the unique tribes that define urban life, from underground dance scenes to vibrant street art communities.
5:00 - 8:00 PM	"Local Gems" - Guest Mix	is a guest mix show dedicated to unearthing and showcasing the remarkable music

		collections of local collectors, selectors, DJs, and musicians. Guests share their personal favourites, rare finds, and untold stories behind their music, providing a unique and intimate look into the rich and diverse musical landscape of the community.
8:00 - 10:00 PM	"Resident Advisors"	Resident DJs do their thing, every Friday and Saturday night.
10:00 PM - Midnight	"Midnight Mix"	DJ sets from local and international DJs, live from the venue.

Sunday (12 PM - 10 PM)

Time	Program	Description
12:00 - 3:00 PM	"Long Lunch"	Smooth jazz, Contemporary Beats and laid-back conversations for a relaxing Sunday.
3:00 - 5:00 PM	"Cultural Mosaic"	Exploring diverse local happenings and cultural activities through music, food, and arts.
5:00 - 7:00 PM	"Disconnection"	inviting listeners to lose themselves in the glittering world of disco, boogie, funk and all of its groovy variations - a genre that transcends time and space. Whether you're a disco aficionado or new to the groove, join us as we map out the multiple dimensions of this captivating music phenomenon.
7:00 - 9:00 PM	"Vinyl Revival"	A discerning trip through classic vinyl records and timeless tracks curated by the city's finest selectors
9:00 - 10:00 PM	"Soulful Sundays"	A soothing mix of soul, R&B, and blues to wind down the week.

***These program concepts aim to create a dynamic and diverse lineup that offers something for everyone. By combining music genres, cultural explorations, community engagement, and topical discussions, the Passenger internet radio programming can create a rich and vibrant listening experience. Importantly, the actual Passenger Radio programming will be developed and curated in collaboration with the local community, giving voice to diverse and evolving local culture and creative scenes, and so may differ from what has been outlined here.

Appendix C - Mission and Vision Statement - Passenger

PASSENGER

Mission

Create a thriving hospitality business that supports and contributes to progressive culture, creative economy and community in Northbridge and beyond.

Vision

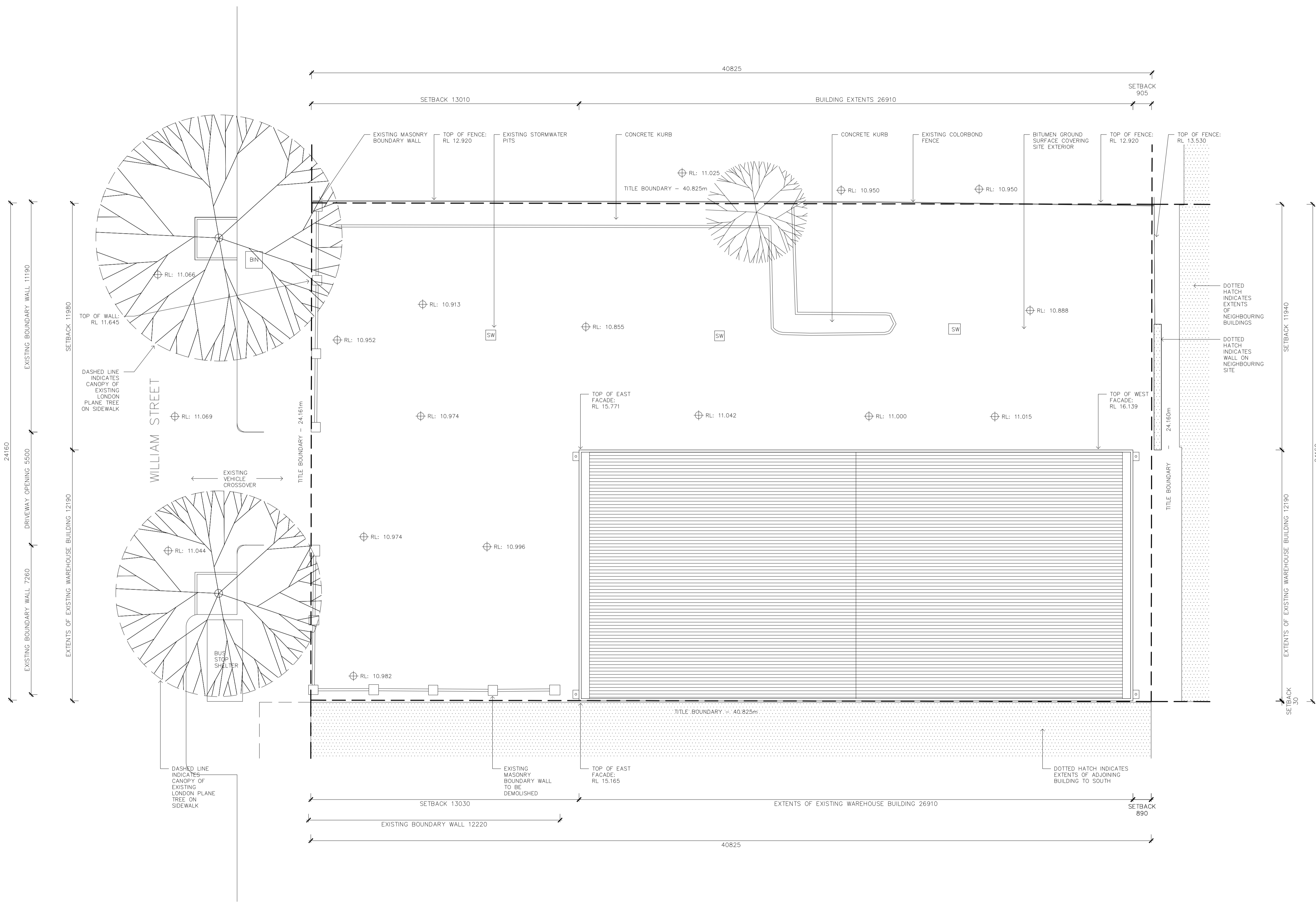
Passenger is a low-key, high-quality, indoor/outdoor hospitality, radio and retail hub for creative folks from different strokes. It is a homely and hospitable space where all are welcome and quality, service, music, connection and community are central to everything they do. The concept draws on the broad experience and vision of two long-standing operators in the Perth hospitality and music scenes, Iraia Anderson (Ezra Pound, Freedom Time) and Ben Taaffe (Move, Freedom Time, Planet X), combining their shared passions for the arts, music and sound, hospitality and community, quality and simplicity with an all-are-welcome and no bullshit approach.

The venue design is inspired by iconic listening and lounge bars from around the world and a new generation of contemporary garden bar and beer garden venues. With this inspiration the directors have designed a uniquely Western Australian space with large outdoor spaces woven into the colourful and well-worn fabric of the Northbridge area. The 350 person capacity venue will consist of four complementary components creating a whole that is greater than the sum of its parts; a world-class record shop & cafe space; an expansive outdoor garden bar space with luscious shady landscaping and large seated social areas; an intimate music lounge made for listening; and an innovative online community media/radio hub broadcasting diverse, progressive and positive signals to the world, while connecting virtual communities with local spaces and real life!

A humble and unpretentious place for the celebration of local stories, creative culture, high-quality music and hi-fi sound, lo-fi drinks, simply delicious food and good friends (new and old); inviting long visits, intimate interactions, creative and cultural exchanges and a healthier hospitality culture. The promise at the core of this business is to provide a home for a growing community of like-minded artists, creative professionals, music enthusiasts to meet, listen, connect, eat and drink, perform, collect and share music - a local creative hub and hang-out with a diverse cultural program connecting across the many thriving subcultures of the Perth music and arts landscape.

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Appendix E - Site Feature Survey



Site Plan
1:100

Peter Frederick Cole
Architect

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peter@pfarchitect.com
+ 61 402 781 258

ABN: 92 161 606 254

This copy of this drawing is issued for the sole use of the recipient and the acceptance of the same constitutes an agreement that it will not be published, reproduced or given to any other party without the permission of Peter Frederick Cole Architect and it shall remain their property subject to return upon request.

This drawing is to be read in conjunction with the Secondary Consultants drawings, details and specifications and any other written instructions issued during the course of the Contract.

The Contractor shall verify all dimensions on site prior to commencing any shop drawings. Figured dimensions shall take precedence over scaled. Scaled dimensions shall be verified on site.

MATERIALS KEY

BK1 Existing brickwork	PS1 Painted Steel	TR1 Gal. steel trellis
BB2 New matching brick	RF1 Ex. roof sheeting	
CC1 Ex. concrete slab	RF2 Gal. steel roof sheet	
CC2 Concrete boundary wall	RF3 Stratco cooldek 50	
CC3 Concrete pavers	RF4 Roof flashings etc.	
CC4 C'crete custom detail	TM1 Timber (species tbc)	
GS1 Gal. steel (Structural)	TM2 Jarrah timber	
GS2 Gal. steel (cladding)	TM3 Marine ply/wood ply	
GS3 Gal. steel (cladding)	TM4 Lam. veneer lumber	

GENERAL LEGEND

ADJ	DENOTES "ADJUSTABLE SHELF" COLUMN (REFER STRUCT. ENG)
COS	DENOTES "CONFIRM ON SITE" MEASUREMENT DOWNPIPE
FCL	FINISHED CEILING LEVEL
FFL	FINISHED FLOOR LEVEL
FW	FLOOR WASTE
RL	RELATIVE LEVEL
T	HOSE COCK / PILLAR TAP

Revision:

Client
Iraia Andersen & Ben Taaffe

Project
Passenger

Project Address
380 William Street
Perth / Boorloo 6000
Western Australia
Drawing Name
Existing Site Plan

Project Stage
DA

Scale
1:100 @ A1

Drawn
PFC

Project Number
2212

Peter Frederick Cole Architect

DA10

Revision
DA10

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Appendix F - Site Context Mapping



378 - 380 William St, Perth

Method Planning

Trent Durward · Director
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APRIL 2023



Appendix G - Letter to Neighbours Sent 28th March 2023

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28th March 2023

Ref: 005WIL

Dear Neighbour,

**PROPOSED TAVERN, RETAIL SHOP, CAFE, AND INTERNET RADIO STUDIO
378 WILLIAM ST, PERTH.**

We hope this letter finds you well. Method Planning represents Sound Hospitality Pty Ltd regarding this correspondence.

Sound Hospitality, represented by Ben Taaffe, are excited to announce that a new tavern, retail shop, cafe, and internet radio studio is being planned for 378 William Street, Perth, near your commercial property.

We understand that this may raise some questions for you, which is why we would like to invite you to a meeting with the project team to discuss the plans and address any issues you may have.

The meeting will be held on 13th April 2023 at 5pm at 378 William St, Perth. We believe that this is a great opportunity for us to come together and build a strong community relationship.

We hope you can attend and look forward to seeing you there.

Should you have any queries or questions regarding this letter, or wish to confirm attendance please do not hesitate to contact me on 0421567180 on trent@methodplanning.com.au

Yours faithfully

A handwritten signature in black ink, appearing to read 'Trent Durward', written over a horizontal line.

TRENT DURWARD
Director, Method Planning

Proposed Mixed-Use Development

378 William Street, Perth

TRANSPORT IMPACT AND CAR PARKING ASSESSMENT

FINAL REPORT – V2

Prepared for: Sound Hospitality

Prepared by: Move Consultants



Move consultants

Moving People Moving Commerce

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August 2023

DOCUMENT ISSUE AUTHORISATION

Issue	Rev	Date	Description	Checked	Approved
1	0	04/08/2023	FINAL	HH	HH
2	1	10/08/2023	REV	HH	HH

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1. INTRODUCTION

1.1 OVERVIEW

This Transport Impact and Car Parking Assessment has been prepared by Move Consultants on behalf of Sound Hospitality with regard to a proposed retail and bar development to be located at 378 William Street, Perth in the City of Vincent. The subject land is currently occupied by a vacant building.

1.2 SITE LOCATION

The site is located on the east side of William Street, approximately 165m north-east of the signalised intersection with Newcastle Street and 225m south-west of the signalised intersection with Brisbane Street. The site is located along the William Street mixed-use corridor within the broader Mount Lawley/Highgate Town Centre. Existing uses in place in the vicinity of the site are mainly mixed-use commercial, short-stay accommodation, higher density multiple dwellings and hospitality uses. Further to the west and east of William Street are generally single-family dwellings mixed with higher density residential uses. There is a vacant single storey commercial building on the site with a single crossover leading to William Street leading to an at-grade parking area.

The location of the site is shown in **Figure 1**.



Figure 1: Site Location

The locational context of the property in relation to adjacent and proximate uses is shown in **Figure 2**

The general metropolitan context is shown in Figure 2.

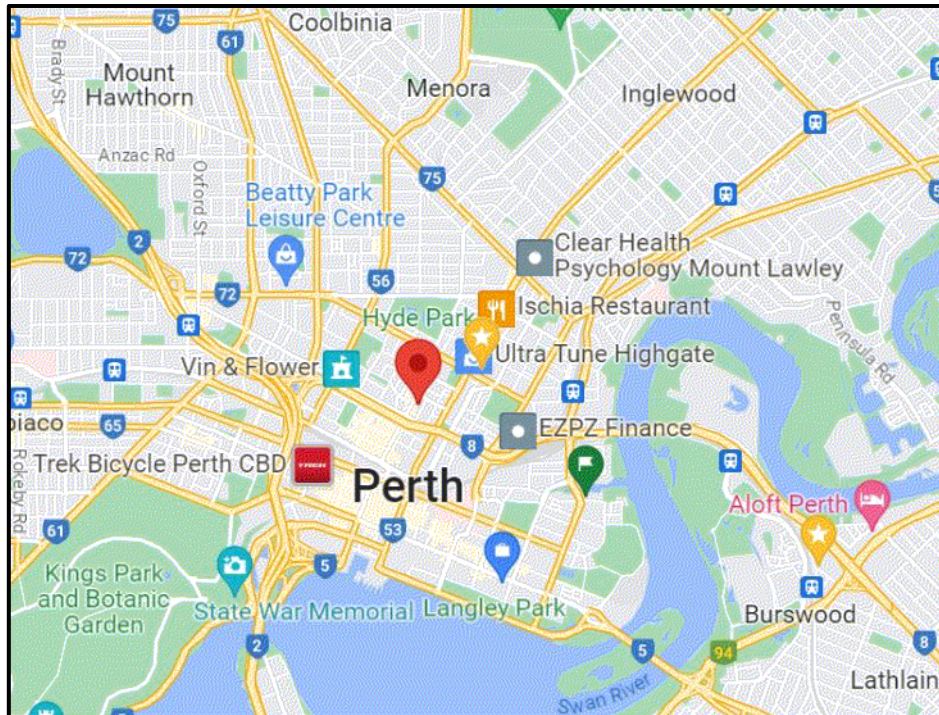


Figure 2: Metropolitan Context

1.3 SCOPE OF ASSESSMENT

This report has been prepared in accordance with the Western Australian Planning Commission's *Transport Assessment Guidelines for Developments: Volume 4 – Individual Developments* as well as the City of Vincent's *Policy 7.7.1.: Non-Residential Car Parking Requirements*.

Specifically, this report aims to assess the impacts of the proposed development on the local boundary road network to identify any modifications, to site or road layout, which may be required to serve the proposed site. In addition, the assessment considers the proposed access, circulation, and egress arrangements to and from the site as well as parking demands associated with the proposal.

2. EXISTING SITUATION

2.1 ROAD INFRASTRUCTURE

The proposed development is to be constructed on a property previously used for commercial purposes and is now vacant. A single full movements crossover to the east side of William Street is currently in place. Existing on-street paid parallel parking is in place along the both sides of William Street to the north of the site with 2-hour time limits in place during typical weekday and weekend peak periods.

William Street, which flanks the site along its north-western boundary is a secondary north-south connecting road providing direct access into and out of the Perth CBD as well as further to the north to Mount Lawley and Highgate. It has been constructed as a single undivided carriageway. William Street has been classified as a *District Distributor A* road under the *Main Roads WA Functional Hierarchy* and have been defined as roads which "...carry

traffic between industrial, commercial and residential areas and generally connect to Primary Distributors; are likely to be truck routes and provide only limited access to adjoining property; and are managed by Local Government.” It functions as a parallel reliever route to Fitzgerald Street to the west and Beaufort Street to the east. William Street operates under a localised area speed zoning of 40kph in the vicinity of the site and is owned, operated and maintained by the City of Vincent. It is also designated as an *Other Regional Road* or *Blue Road* in the *Metropolitan Region Scheme*.

Newcastle Street, which is located some 165m to the south-west of the site, is a primary east-west connecting road providing direct access into and out of the Mount Lawley, Highgate and Northbridge precincts as well as connecting further to the west to Fitzgerald Street and Charles Street to access the Mitchell Freeway and the Leederville Town Centre. It functions as a parallel reliever route to Walcott Street and Vincent Street to the north of the site. It has been constructed as a dual divided carriageway with a raised central median, south-west of the site. Bulwer Street has also been classified as a *District Distributor A* road under the *Main Roads WA Functional Hierarchy* and Newcastle Street also operates under a speed zoning of 50kph in the vicinity of the site and is owned, operated and maintained by the City of Vincent.

Brisbane Street, to the north of the site and east of William Street, which is located approximately 225m north-east of the site, is a secondary east-west connecting road providing direct access into and out of the Mount Lawley and Highgate precincts. It has been constructed as a wide single undivided carriageway on both sides of William Street flaring wide on the westbound approach to the signalised intersection to incorporate a dedicated left-turn pocket. On-street parallel parking is in place on both sides of the road both east and west of William Street. Brisbane Street, east of William Street, has been classified as a *District Distributor A* road under the *Main Roads WA Functional Road Hierarchy* and an *Access Road* west of William Street. *Access Roads* are roads which “...provide access to abutting properties with amenity, safety and aesthetic aspects having priority over the vehicle movement function which are bicycle and pedestrian friendly and are managed by Local Government.” The speed zoning on Brisbane Street is 60kph east of William Street and 40kph west of William Street.

A number of local roads in close proximity to the site include Robinson Avenue, Monger Street, Money Street, Little Parry Street, Forbes Road, Lindsay Street and Wellman Street have all generally been constructed as typical single undivided carriageways with on-street parallel parking in place. These have all been classified as *Access Roads* under the *Main Roads WA Functional Road Hierarchy* and have been defined as roads which “...provide access to abutting properties with amenity, safety and aesthetic aspects having priority over the vehicle movement function which are bicycle and pedestrian friendly and are managed by Local Government.” These are roads which operate under the localised area speed zoning of 40kph and are also owned, operated and maintained by the City of Vincent.

The intersections of William Street/Newcastle Street and William Street/Brisbane Street operate under 4-way signalised control. The balance of the intersections in close proximity to the site operate under unsignalised control.

Figure 3 shows the road hierarchy in the vicinity of the site.

A detailed site visit was conducted on Thursday 27th July 2023 to collect information relating to existing road geometry, speed limits, and sightlines and to observe existing traffic operations on the adjacent boundary road network.



Figure 3: MRWA Functional Road Hierarchy – Local Road Network

Existing traffic volumes were obtained via data from Main Roads Western Australia for the major roads in the vicinity of the site. The anticipated daily traffic on the minor or local roads in the area have been derived upon a review of existing land uses, spatial distribution of activities, access to higher road network and existing road connection. Documented and estimated traffic volumes in the vicinity of the site are shown in **Table 1**.

Table 1: Existing Traffic Volumes

Road Link	Existing Traffic Volumes (vpd)	Source	Practical Capacity (vpd)
William Street (North of Newcastle Street)	8,900 vpd	Main Roads WA (2021/22)	15,000 vpd
Newcastle Street (East of William Street)	11,900 vpd	Main Roads WA (2018/19)	15,000 vpd
Newcastle Street (West of William Street)	10,700 vpd	Main Roads WA (2021/22)	15,000 vpd
Brisbane Street (East of William Street)	4,100 vpd	Main Roads WA (2023)	7,000 to 10,000 vpd
Brisbane Street (West of William Street)	2,300 vpd	Main Roads WA (2023)	3,000 vpd
Robinson Avenue, Money Street, Lindsay Street, Monger Street, Wellman Street, Forbes Road	500 to 1,100 vpd	N.A. (Interpolation)	3,000 vpd
Little Parry Street	<500 vpd	N.A. (Interpolation)	1,200 vpd

2.2 PUBLIC TRANSPORT, PEDESTRIAN, AND CYCLIST FACILITIES

The site is located directly on a high frequency bus routes (Route 950 – Morley Bus Station – QEII Medical Centre via Beaufort Street and University of WA and Route 980 – Morley Bus Station via William Street) as well as on several other line haul routes (Routes Street, Perth and UWA) with additional line haul bus routes (Route 66 – Perth to Morley via Beaufort Street, Route 67 – Perth to Mirrabooka via Grand Promenade and Route 68 – Perth to Mirrabooka via Lennard Street). A bus stop for southbound services along William Street is located along the southern boundary of the site with a bus stop for northbound services are located approximately 30m to the northwest on the west side of William Street within a 1-to-2-minute walking distance. These combined bus services provide 5-minute services during weekday peak periods and 15 to 30-minute services service during the midday and on weekends. The East Perth Railway Station is also located approximately 725m to the south and is within a 10-minute walking distance to and from the site. Figure 4 and Figure 5 show the existing public transport services in the area.

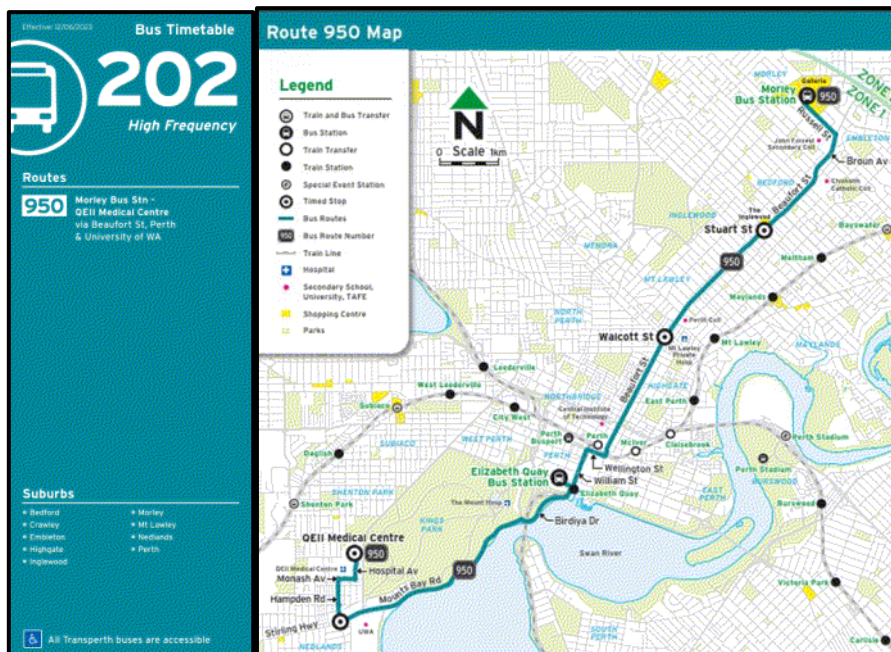
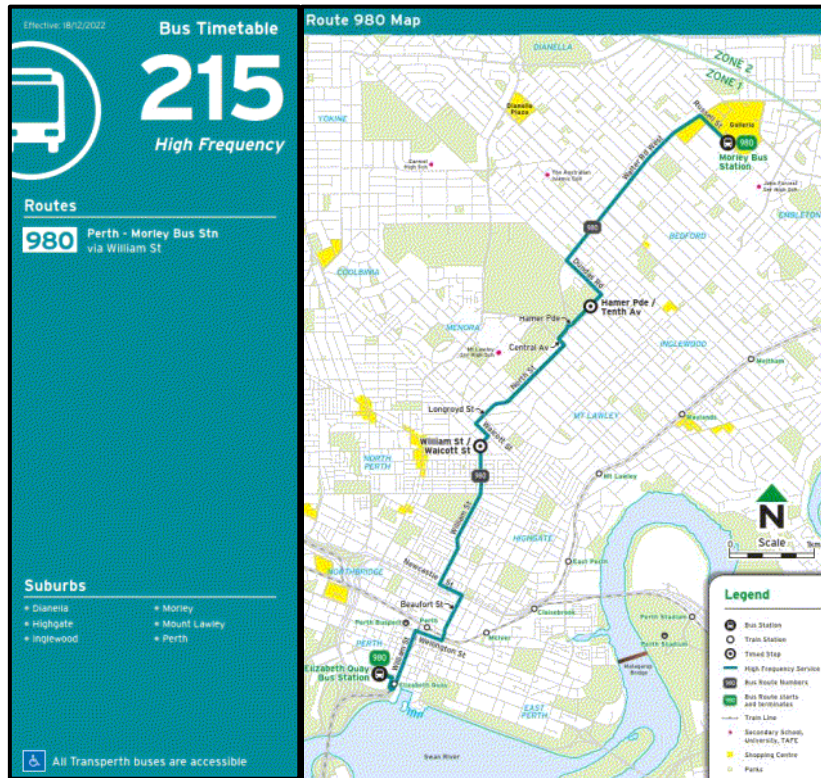


Figure 4: Existing Public Transport High Frequency Bus Services

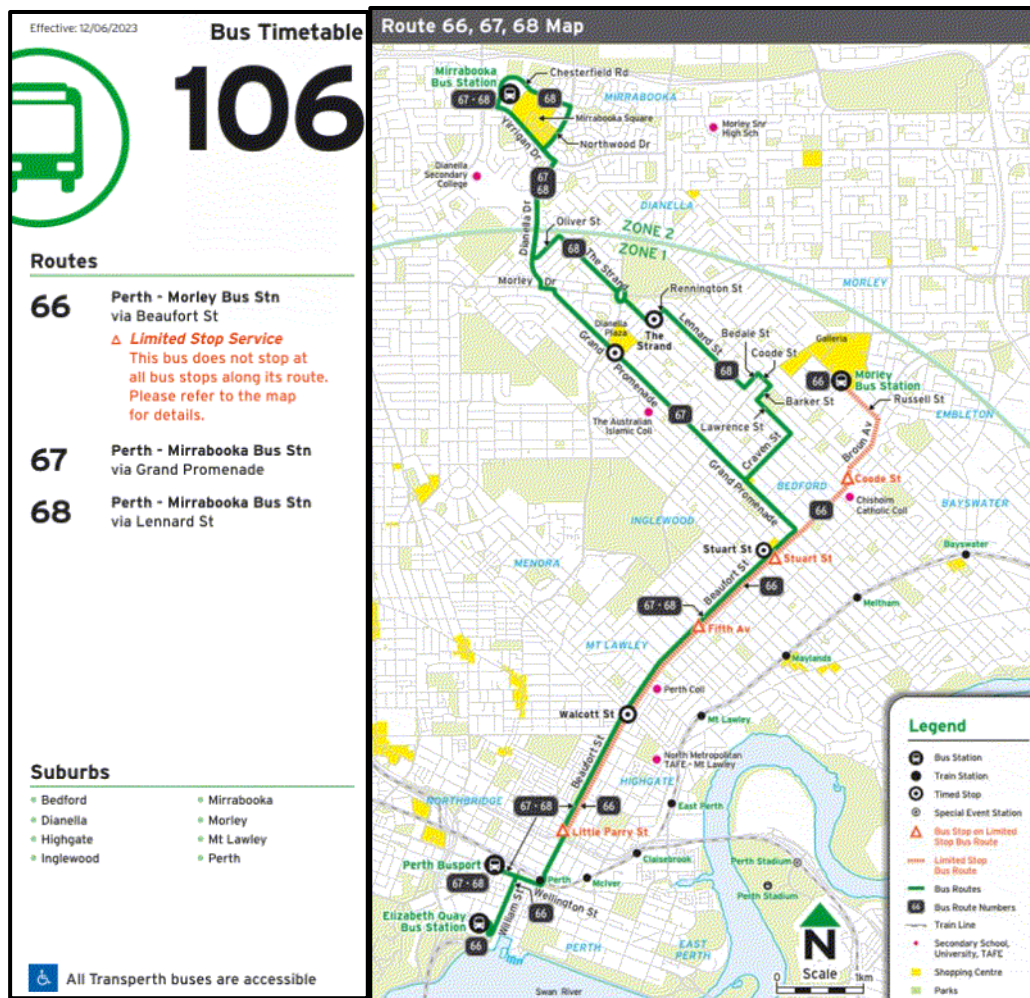


Figure 5: Existing Public Transport Services – Line Haul Bus Services

Footpaths are in place on both sides of William Street with footpaths and a Principal Shared Path on both sides of Forbes Road, west of William Street, Forbes Road is also designated as a *Good Riding Environment*. These cycling facilities provide a direct connection into the *Principal Shared Path Network* of the *Perth Bicycle Network* providing direct access into the Perth CBD via the off-road 'veloway' in place adjacent to the railway line to the south.

Figure 6 shows the cycling and pedestrian infrastructure in the vicinity of the site.

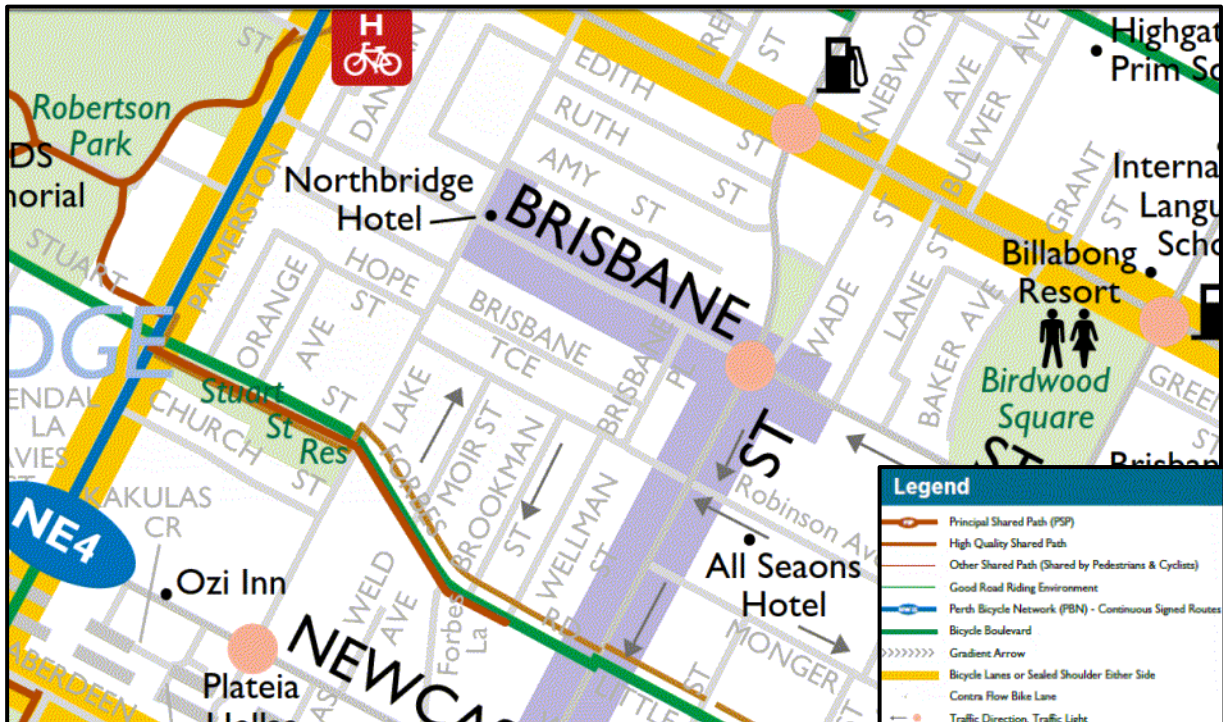


Figure 6: Existing Cycling and Pedestrian Infrastructure

3. PROPOSED DEVELOPMENT

A site plan of the proposed development has been prepared by Peter Frederick Cole Architect V99CV 9. A copy of the site plan is contained in **Appendix A**.

3.1 PROPOSED LAND USES

The proposal seeks the development of the following:

- Café/Retail – 36m²
- Indoor Public Bar Area – 240m²
- Indoor Back of House – 304m²
- Outdoor Public Area – 417m²

3.2 PROPOSED ACCESS AND PARKING ARRANGEMENTS

No vehicle access is proposed for the site with parking demands associated with the site proposed to be accommodated off-site within the public supply. This development is not expected to operate a typical hospitality establishment due to the constraints associated with the inner-city location which is afforded high quality public transport and pedestrian and cycling infrastructure as well as size of the proposal as well as due to the changing public attitudes associated with drinking and driving and the increased popularity of and accessibility of ride share services such as Uber, Lyft and Didi.

Bicycle parking for visitors as well as bicycle parking and end-of-journey facilities for staff will also be provided on the site in accordance with the City's planning policy.

Rubbish collection will be undertaken via kerbside collection on William Street in a line haul manner with commercial rubbish collection arrangements confirmed in consultation with the City of Vincent in a separate Waste Management Plan prepared during the detailed design stages of the project.

3.3 END OF TRIP FACILITIES

End-of-trip facilities (including 24 bicycle racks) are proposed to be provided on the site within bicycle parking provided at ground level along the southern boundary of the site. The provision of these facilities is compliant with City of Vincent and Austroads guidelines for the development with a dedicated shower and lockers for staff.

4. TRANSPORT ANALYSIS

A traffic generation and distribution exercise has been undertaken to assess the potential traffic impacts associated with the proposed development. The aim of this exercise was to establish the traffic volumes which would be generated from the proposed development and to quantify the effect that the additional traffic has on the surrounding road network,

4.1 TRIP GENERATION

The traffic generated by the proposed development has been predicted by reviewing documented trip generation rates for the proposed uses associated with similar recently approved developments such as the Indian Gastro Pub and The Beaufort. This trip generation was then modified to reflect the location of the proposal on a high frequency bus route and within a significant urban catchment which would attract a high proportion of walk-in trade. As a result, the traffic generation has been based upon a maximum patronage capacity of 350 seats; however, as the venue is not expected to approach 100% capacity at any given time due to the staggered nature of the operating times, it is therefore expected that the maximum occupancy at any given time would be in the order of 75%. The peak patron arrival/departure patterns during the anticipated peak periods would be as follows:

- Friday:
 - 12:00 p.m. to 2:00 p.m. – 35 inbound/23 outbound = 58 trips per hour
 - 8:00 p.m. to 10:00 p.m. – 117 inbound/39 outbound = 156 trips per hour
 - Daily - 205 inbound/205 outbound = 405 trips
- Saturday:
 - 7:00 p.m. to 8:00 p.m. – 117 inbound/39 outbound = 156 trips per hour
 - Daily - 205 inbound/205 outbound = 405 trips

Based upon a review of ABS Census data, established mode split and travel patterns associated with the previously approved developments, the following mode split for the proposal has been assumed:

- Private motor vehicle:
 - 15% designated driver

- 25% passenger (passenger/carpool with estimated vehicle occupancy of 1.5 passengers per vehicle)
- Public transport:
 - 10% (high quality/high frequency bus routes and within walkable catchment to Perth Railway Station)
- Walk/cycle:
 - 20% (inclusive of local residents, local employees and patrons visiting other venues in the area)
- Taxi/Ride Share:
 - 30% (average occupancy of 1.5 passengers per vehicle)

Based upon the above noted mode split, it is estimated that the proposed development will generate the following vehicle trips:

- Friday:
 - 12:00 p.m. to 2:00 p.m. – 4 inbound/3 outbound = 7 vehicle trips per hour
 - 8:00 p.m. to 10:00 p.m. – 14 inbound/4 outbound = 18 vehicle trips per hour
 - Daily -59 inbound/59 outbound = 118 vehicle trips
- Saturday:
 - 7:00 p.m. to 8:00 p.m. – 4 inbound/3 outbound = 7 vehicle trips per hour
 - Daily -59 inbound/59 outbound = 118 vehicle trips

4.2 TRIP DISTRIBUTION

Based upon the existing traffic patterns in the area and the spatial distribution of adjacent land uses, the following distribution for the proposed 'new' development generated traffic has been assumed:

- 50% to and from the north via William Street; and
- 50% to and from the south via William Street.

The resultant increases to the boundary road network are anticipated as follows:

- William Street (North):
 - Daily: +62 vpd
 - Friday Lunch. Peak Hour: +9 vph
 - Friday P.M. Peak Hour: +24 vph
 - Saturday P.M. Peak Hour: +24 vph
- William Street (South):
 - Daily: +62 vpd
 - Friday Lunch. Peak Hour: +9 vph
 - Friday P.M. Peak Hour: +24 vph
 - Saturday P.M. Peak Hour: +24 vph

It should be noted that these vehicle trips would not access and egress the site directly, as there is no on-site car parking proposed, but rather to and from the general area in and around the site, within a 400m walkable catchment area. The impacts to the roads, including the local access road network, within the catchment area can be comfortably accommodated within the practical capacity of the road network resulting in acceptable traffic operations on the adjacent road network.

Additional detailed traffic analysis is not warranted at the site crossover. *Austrroads' Guide to Traffic Management* provides advice on the capacity of unsignalised intersections. For minor roads where there are relatively low volumes of turning traffic, capacity considerations are usually not significant and capacity analysis is unnecessary. Intersection volumes below which capacity analysis is unnecessary are indicated in **Table 2**.

Table 2; Threshold Analysis Parameters (Austrroads, 2009)

Type of road	Light cross and turning volumes maximum design hour volumes (vehicles per hour (two way))		
	Two -lane major road	400	500
Cross road	250	200	100

In conclusion, it should be noted that based both on a review of the modelled total traffic assessment and observed traffic operations of the boundary road system, the anticipated site-generated traffic associated with the redevelopment proposal is negligible and that no external boundary road improvements will be required.

5. VEHICULAR ACCESS AND PARKING

5.1 ON-SITE QUEUING, CIRCULATION AND ACCESS

As no on-site car parking is proposed, a circulation and access assessment is not required.

Rubbish collection and service, delivery and loading activities can be accommodated along the existing William Street frontage during off-peak periods within the existing on-street parallel parking supply. A separate Loading and Servicing Management Plan can be prepared and submitted under separate cover.

Details relating to the Waste Management aspects of the project will be negotiated directly with the City of Vincent during the detailed design stages of the project.

5.2 CRASH HISTORY

A review of the crash history for the 5-year reporting period of 2018-2022 indicates a total of four (4) driveway-related crashes and three (3) parking-related crashes between Monger Street and Little Parry Street along William Street which represents a distance of approximately 100m. This very low number of crashes along this extended section of road particularly in the context of the traffic volumes on the boundary road network plus the high turnover at the commercial developments along William Street to the north and south of the site indicates that the additional traffic associated with the development will have a negligible impact on the risk profile on the local boundary road network and can accommodate pedestrians, cyclists and public transport users safely to and from the site.

5.3 PARKING DEMAND AND SUPPLY

There is no proposed on-site carparking supply for the development, as noted in Section 3.2 of this report. No vehicle access is proposed for the site with parking demands associated with the site proposed to be accommodated off-site within the public supply. This development is not expected to operate as a typical hospitality establishment due to the constraints associated with the inner-city location which is afforded high quality public transport and pedestrian and cycling infrastructure as well as size of the proposal as well as due to the changing public attitudes associated with drinking and driving and the increased popularity of and accessibility of ride share services such as Uber, Lyft and Didi.

A detailed series of surveys of the public car parking supply in the local area has been undertaken during the weekend of Friday 28th July 2023 to Saturday 29th July 2023 for the following time periods:

- Friday:
 - 10 a.m. to 2 p.m. (30-minute increments)
 - 5 p.m. to 10 p.m. (30-minute increments)
- Saturday:
 - 5 p.m. to 10 p.m. (30-minute increments)

Both on- and off-street public car parking supply was surveyed at the following locations:

- William Street (Brisbane Street-Aberdeen Street) – On-Street
- Newcastle Street (Forbes Lane-Money Street) – On-Street
- Robinson Avenue (Wellman Street-Lindsay Street) – On-Street
- Forbes Road (Forbes Lane-William Street) – On-Street
- Monger Street (William Street-Lindsay Street) – On-Street
- Lindsay Street (Brisbane Street-Little Parry Street) – On-Street
- Money Street (Newcastle Street-Monger Street) – On-Street
- 379 William Street Car Park – Off-Street

Figure 7 shows the location of the car parking surveys.

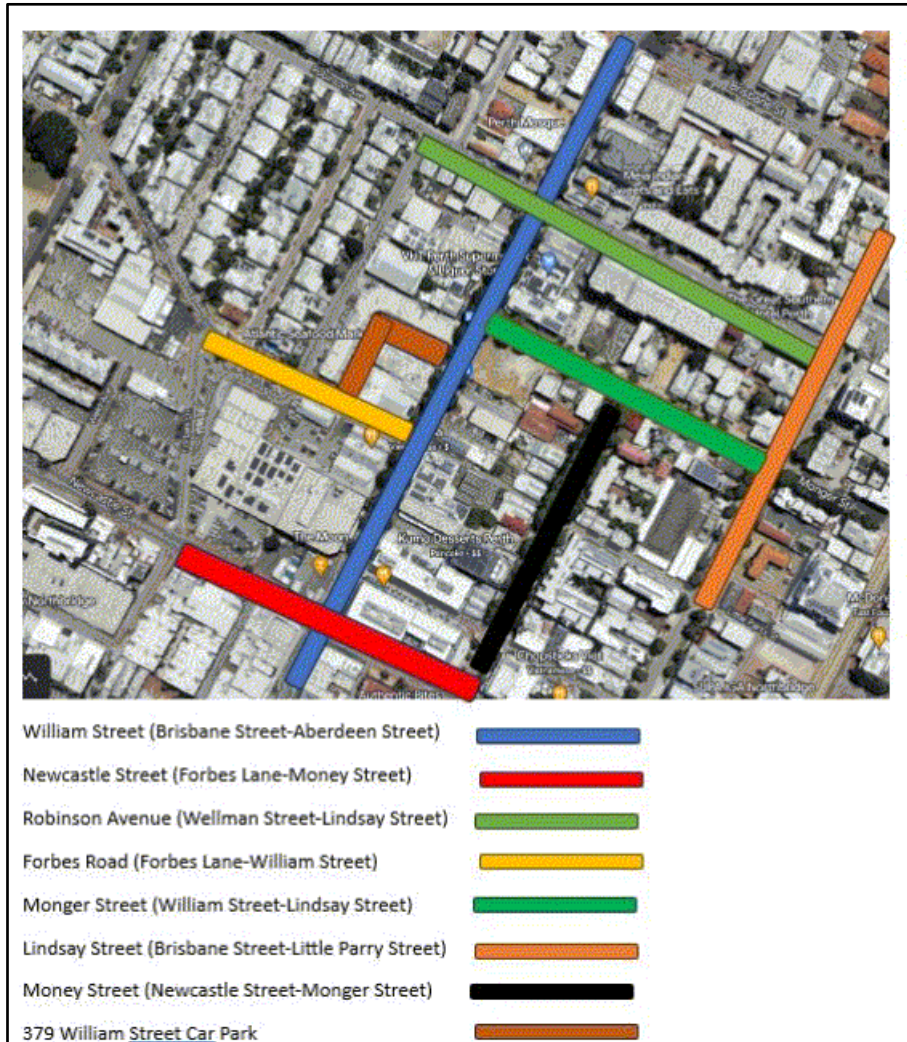


Figure 7: Car Parking Survey Locations

The public SecurePark facility on Lindsay Street was not yet open to the public during the period of the surveys but is expected to add 40 additional bays to the public parking supply in the near future in the vicinity of the site.

Table 3, Table 4 and Table 5 show the cumulative results of the survey and in particular, the available surplus of public car parking within 300m to 400m walking distance to the site including the additional 40-bay supply shortly to be commissioned within the Lindsay Street SecurePark facility.

Table 3: Results of Car Parking Surveys (Public Parking Including Lindsay Street SecurePark) – Friday 10 a.m. to 2 p.m.

TIME	OCCUPIED	AVAILABLE	TOTAL # BAYS	LINDSAY ST SECUREPARK	POTENTIAL PUBLIC SUPPLY AVAILABLE
10:00 A.M.	272	30	302	40	70
10:30 A.M.	275	27	302	40	67
11:00 A.M.	283	19	302	40	59
11:30 A.M.	289	13	302	40	53
12:00 P.M.	285	17	302	40	57
12:30 P.M.	284	18	302	40	58
1:00 P.M.	276	26	302	40	66
1:30 P.M.	275	27	302	40	67
2:00 P.M.	268	34	302	40	74

Table 4: Results of Car Parking Surveys (Public Parking Not Including Lindsay Street SecurePark) – Friday 5 p.m. to 10 p.m.

TIME	OCCUPIED	AVAILABLE	TOTAL # BAYS	LINDSAY ST SECUREPARK	POTENTIAL PUBLIC SUPPLY AVAILABLE
5:00 P.M.	222	80	302	40	120
5:30 P.M.	239	63	302	40	103
6:00 P.M.	248	54	302	40	94
6:30 P.M.	270	32	302	40	72
7:00 P.M.	281	21	302	40	61
7:30 P.M.	284	18	302	40	58
8:00 P.M.	267	35	302	40	75
8:30 P.M.	237	65	302	40	105
9:00 P.M.	203	99	302	40	139
9:30 P.M.	174	128	302	40	168
10:00 P.M.	153	149	302	40	189

Table 5: Results of Car Parking Surveys (Public Parking Not Including Lindsay Street SecurePark) – Saturday 5 p.m. to 10 p.m.

TIME	OCCUPIED	AVAILABLE	TOTAL # BAYS	LINDSAY ST SECUREPARK	POTENTIAL PUBLIC SUPPLY AVAILABLE
5:00 P.M.	246	56	302	40	96
5:30 P.M.	265	37	302	40	77
6:00 P.M.	280	22	302	40	62
6:30 P.M.	296	6	302	40	46
7:00 P.M.	290	12	302	40	52
7:30 P.M.	294	8	302	40	48
8:00 P.M.	288	14	302	40	54
8:30 P.M.	282	20	302	40	60
9:00 P.M.	260	42	302	40	82
9:30 P.M.	223	79	302	40	119
10:00 P.M.	203	99	302	40	139

The theoretical required supply in accordance with the City's *Local Planning Policy 7.7.1: Non-Residential Parking* is in the order of 0.15 bays/person or 55 parking bays. However, the anticipated traffic generation associated with the proposal indicates that it will generate a maximum demand of 1 to 4 bays (range difference between arrivals and arrivals/departures) during a typical Friday daytime peak hour between 10 a.m. and 2 p.m. and 10 to 14 bays (range difference between arrivals and arrivals/departures) during a typical Friday or Saturday evening peak hour between 5 p.m. and 10 p.m., respectively. Based upon the results of the detailed car parking surveys noted above, this quantum of car parking demand can be more than comfortably accommodated within the existing and near future public parking supply within 300m to 400m walking distance of the site.

This approach is also consistent with the City's *Local Planning Policy 7.7.1: Non-Residential Parking* which states the following:

Where a proposal does not meet the standards set out in Clauses 2, 3 or 4 of this policy, the decision-maker is required to exercise judgment to determine the proposal. The decision-maker is to exercise its judgment to consider the merits of the proposal having regard to the objectives of this policy and one or more of the following requirements:

1.2.1 The development provides adequate parking and/or transport infrastructure to service the needs of its users.

1.2.2 Alternative public car parking, bicycle parking and/or end-of-trip facilities are already provided and are available and accessible to meet the demands of the development during the hours of operations, for the life of the development.

1.2.3 Reciprocal car parking, bicycle parking and end-of-trip facilities are provided in accordance with Clause 5.

1.2.4 Alternative transport modes are convenient and adequate enough to meet the demand of the development during the hours of operation.

1.2.5 Cash-in-lieu of parking is provided in accordance with Clause 6.

1.2.6 An alternative arrangement is agreed in writing between the Applicant and the City, through an approved Parking Management Plan.

In addition, the WAPC's *Development Control Policy (DC) 1.6: Planning to Support Transit Use and Transit Oriented Development* provides the following guidance with respect to car parking concessions due to proximity to public transport options.

Excerpts from Clause 4.6: state "...in carrying out the necessary analysis as part of the local planning strategy process, and in developing related planning provisions, local governments should have particular regard to matters such as...":

- the encouragement of public transport use over car use.
- the encouragement of mixed-use development, both generally and within individual developments.
- the development and application of scheme parking standards that reflect the availability within the precinct of transit facilities and that provide discretion to vary standards, and to progressively replace surface level car parking close to stations with other more transit supportive uses over time.
- the potential to use planning provisions to provide incentives for appropriate development in transit-oriented precincts, including reduced parking standards and floor-space 'bonuses.
- For the immediate environs of transit facilities, local government is encouraged to consider the preparation of precinct plans that provide greater detail with respect to both land use and the physical form and relationship of development in the precinct to the transit facility, including design guidelines."

A recent detailed review of car parking requirements for non-residential land uses was undertaken in the form of an Interim Guidance document commissioned as part of Planning Reform initiated by the Government of Western Australia. The purpose of the interim guidance was to provide background, context and guidance as to the implementation of consistent car parking requirements for non-residential land uses within the Perth and Peel Region. The guidance which is currently in the public realm for public comment and consultation recommends that for areas and/or precincts such as the William Street Activity Corridor and the Northbridge/Highgate Precincts that

a minimum of zero (0) be established and in absence of this minimum, a maximum of one (1) car parking bays per 100m² of floor area (GFA) be provided. The proposal to not provide on-site car parking is consistent with this interim guidance.

The availability of more than sufficient public parking surplus bays within close walking distance to the site will more than adequately cater to the site's maximum demand and therefore the proposal to provide no car parking on-site can be justified.

A detailed Parking Management Plan for the proposal will be prepared in accordance with the City of Vincent's requirements.

It can therefore be concluded that a proposed reduction in statutory parking allocation maintains this aim through a corresponding restriction in on-site parking supply. This is known to be a primary factor in promotion of mode shift away from car-as-driver modes and towards single or no car households utilising the available alternative modes including carpooling, public bus transport and cycling. The location of the subject site proximal to a major public transport and other non-motorised transport infrastructure for these alternative modes will assist in the transition towards more sustainable transport in the area.

This approach is also consistent with the stated objectives of Western Australian Planning Commission in documentation including and *Directions 2031 and Beyond* and *Liveable Neighbourhoods* as well as the City's parking policy and the State Government's interim guidance on car parking.

6. CONCLUSIONS

The aim of this Transport Impact and Car Parking Assessment was to discuss the traffic likely to be generated by the proposed café/retail and tavern development proposed at 378 William Street, Perth in the City of Vincent and to assess the impacts associated with anticipated site-generated upon the adjacent transport infrastructure. In particular, the assessment considered the impacts on the local boundary road network.

A review of the anticipated traffic generation associated with the proposal indicates that the expected traffic which will be generated by the development on a daily basis and during peak weekday a.m. and p.m. periods can be comfortably accommodated within the practical capacity of the boundary road network with no impacts expected to existing traffic operations.

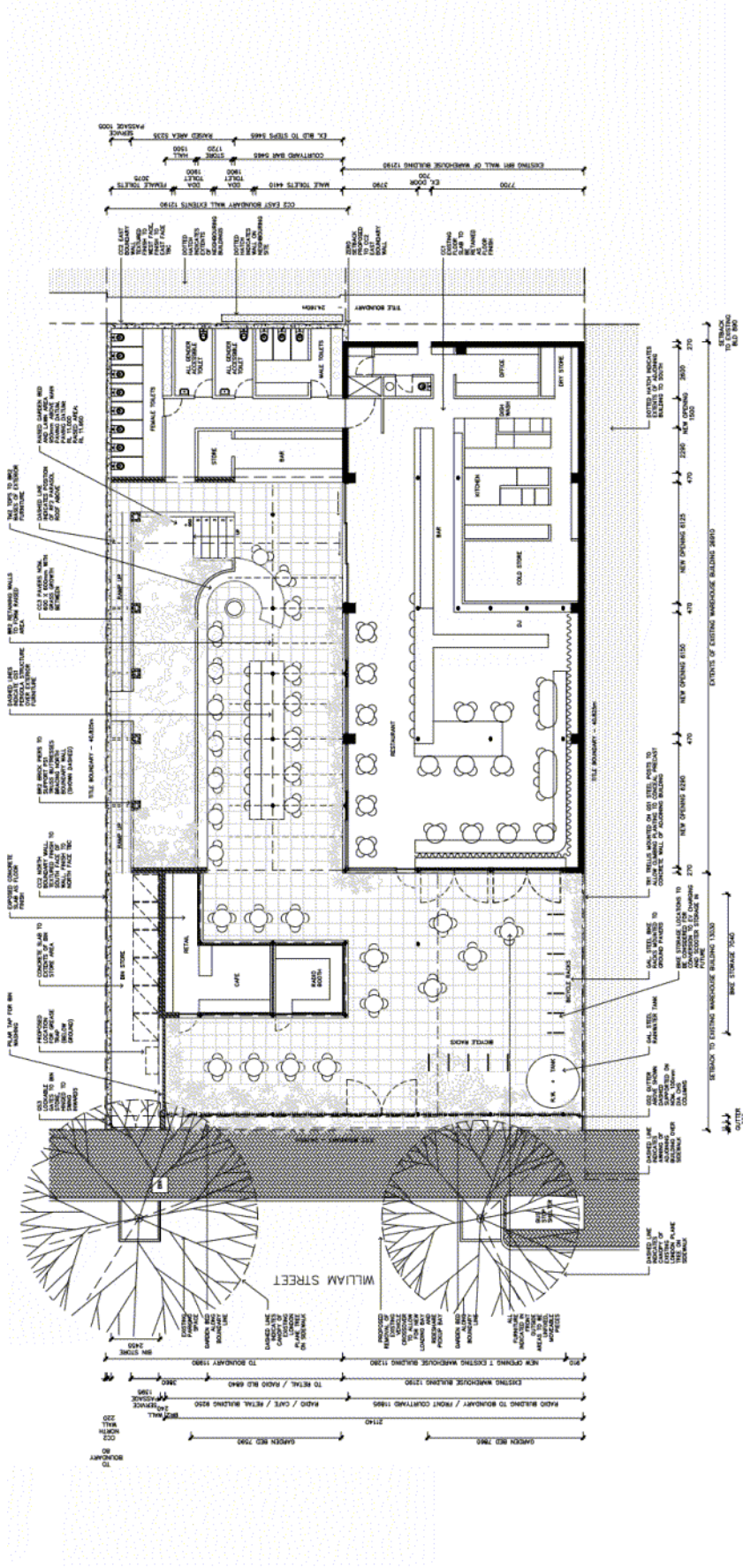
Commercial rubbish collection and service, delivery and load activities will be undertaken via kerbside collection along the William Street frontage. Details relating to the Waste Management and Loadings, Service and Delivery management aspects of the project will be negotiated directly with the City of Vincent during the detailed design stages of the project.

The availability of more than sufficient public parking surplus bays within close walking distance to the site will more than adequately cater to the site's maximum demand and therefore the proposal to provide no car parking on-site can be justified.

In conclusion, it should be noted that based both on a review of the modelled total traffic and observed traffic operations of the boundary road system, the anticipated site-generated traffic associated with the proposed development can be accommodated within the existing practical capacity and functional road classification of the local road system. The proposal to accommodate the anticipated car parking demands associated with the development within the existing off-site public parking supply is justifiable and consistent with 'best practice' urban infrastructure planning.

APPENDIX A: SITE PLAN







Appendix 1: Parking Management Plan

The purpose of the parking management plan is to assess and consider how the proposal will satisfy the parking demand of the development. Consideration of the existing site and surrounding context is required, as well as management strategies to ensure management of parking areas.

Minimum parking requirements may be waived or varied based on the parking management plan submitted. Justification for waiving or varying the minimum parking requirements is not limited to the elements contained within this template.

Owner / Applicant Details:	
Name:	Iraia Anderson, Sound Hospitality
Address:	378 William Street, Perth
Phone:	[REDACTED]
Email:	[REDACTED]
Property Details:	
Lot No.:	
Address:	378 William Street, Perth WA 6000
Parking Allocation:	
<i>Please specify for each criteria the number bays allocated to each user group (i.e., staff, customers, accessible, service, etc.).</i>	
No. car parking spaces:	0
No. short term bicycle bays:	20
No. long term bicycle bays:	4
End-of-trip facilities:	1
No. other parking: <i>e.g., scooters</i>	2 bays cash-in-lieu
Parking Demand:	
<i>Please specify the anticipated parking demand for the development.</i>	
Anticipated no. of staff at any one time:	15

Anticipated no. of customers at any one time:	350
Likelihood of	25%
Alternative transport options:	
<i>Please consider alternative ways people may be able to access your site. The Journey Planner website may provide some assistance.</i>	
Train: <i>Where is the nearest station, how far is this from your site, and is the route provided with footpath access and shade?</i>	Perth Railway Station: 350 south of site Footpaths on both sides of the road
Bus: <i>Where is the nearest bus stop, how far is this from your site and how frequent is the bus during peak periods?</i>	Bus stops on William Street: Southbound services – stop located along south-western site frontage. Northbound services – stop located 30m to north-east on western side of William Street Combined bus service frequency 5-10 minutes during weekdays and 15 minutes during evenings and weekends (Routes 980, 66, 67, 68, 950)
Cycling: <i>Is there a cycle path that accesses your site, are there existing facilities cyclists can use?</i>	City of Vincent has identified William Street as one of its endorsed cycle routes as a “primary route” and a priority cycleway for scheduled improvements in their “Bike Plan” strategy document. Bicycle parking will be provided on-site in a location which will be under CCTV surveillance.
Public parking: <i>Are there public parking facilities (on-street or car parks) in the vicinity of the site? How many bays are available and are there any parking restrictions?</i> <i>Note: use of on-street parking within residential streets will not be supported.</i>	There is a minimum of 342 public on- and off-street car parking bays within 300 to 400m of the site plus approximately 10 to 12 on-street motorcycle bays. Detailed car parking surveys were undertaken by Move Consultants on behalf of the applicant (as detailed in the TIA – V2 submitted under separate cover) and indicate that during the site peak demand periods (Friday lunchtime and Friday and Saturday evenings), there will be more than adequate surplus on- and off-street public parking bays available to accommodate the anticipated peak parking demands associated with these time periods.
Shared parking arrangement:	
<i>Clause 77Q of the Deemed Provisions of the Planning and Development (Local Planning Schemes) Regulations 2015 outlines the matters that the local government may have regard for when determining whether to approve a shared parking arrangement, including:</i>	
<i>Whether the peak operation hours of the development will overlap with those of the shared site.</i>	Not applicable

<p><i>Whether the use of the car parking spaces to be made available on the shared site will impede the use of delivery or service areas on the shared site.</i></p>	<p>Not applicable</p>
<p><i>Any relevant local planning policy.</i></p>	<p>Not applicable</p>
<p><i>Adequate car parking is likely to be available at all times for both the proposed development and the shared site.</i></p>	<p>Not applicable</p>
<p><i>The relationship between the proposed development and the shared site will be such that the shared car parking spaces are likely to be used by persons using the proposed development.</i></p>	<p>Not applicable</p>
<p>Parking Management Strategies <i>Please detail any parking management strategies that will be implemented such as management of tandem bays, way finding.</i></p>	
<p>General Strategies <i>Bay allocation and marking, time limited parking, fees payable.</i></p>	<p>Not applicable</p>
<p>Management and maintenance of parking (including car stackers)</p>	<p>Not applicable</p>
<p>Management of tandem parking for staff / tenants</p>	<p>Not applicable</p>
<p>Wayfinding measures</p>	<p>Not applicable</p>
<p>Promotion of alternative transport modes: <i>I.e., The provision of well-maintained bicycle and end of trip facilities, use of active transport initiatives or public transport promotion.</i></p>	<p>Staff will be encouraged to travel to and from the site via alternative/active transport modes. Formalised bicycle parking will be provided along the south-western boundary of the site in the form of 24 bicycle parking bays to cater to both visitors and staff. This supply is well in excess of that required under the City's LPP 7.1.1 End-of-journey facilities will also be provided on the site for staff.</p>
<p>Service Bays <i>Please detail how service vehicles will be accommodated.</i></p>	

<p>No. service bays provided: <i>Private and/or shared</i></p>	<p>Service, delivery and loading activities and rubbish collection will be accommodated on-street adjacent to the site.</p>
<p>Expected no. of deliveries: <i>Include schedule of deliveries (i.e., days, times)</i></p>	<p>Expected service, delivery and loading movements are anticipated to be in the order of five (5) per week on average entailing a maximum vehicle size of an SR 12.5m vehicle.</p> <p>Rubbish collection will occur a maximum of two (2) movements per week entailing a maximum vehicle size of 8.8m in length.</p> <p>These movements will be undertaken in a line manner in forward along the western boundary of the site within the existing William Street parking supply.</p>
<p>Management of shared service bays: <i>Include requirements of other tenancies, demonstrating there will no conflict.</i></p>	<p>The anticipated maximum number of deliveries and rubbish collection vehicle movements is anticipated to be in the order of up to seven (7) movements per week which will be approximately one (1) vehicle movement per day on average with an average dwell time on-street of less than 15 minutes for each movement. This will not result in any unacceptable conflict with other demands for the road space adjacent to the site. Existing commercial properties in close proximity to the site operate in a similar manner and accommodate their respective service, delivery, loading and rubbish tasks with a minimal impact to the existing risk profile of the road network.</p>
<p>Other <i>Sign marking, etc.</i></p>	<p>If required, two (2) pick-up/drop-off bays will be provided on-street within the existing parallel parking supply near the northern boundary of the site on the east side of William Street with relevant signage and line marking to be approved by the City of Vincent.</p>

Key findings from Traffic Impact Statement / Assessment

There is no proposed on-site car parking supply for the development, as noted in Section 3.2 of the TIA. No vehicle access is proposed for the site with parking demands associated with the site proposed to be accommodated off-site within the public supply. This development is not expected to operate as a typical hospitality establishment due to the constraints associated with the inner-city location which is afforded high quality public transport and pedestrian and cycling infrastructure as well as size of the proposal as well as due to the changing public attitudes associated with drinking and driving and the increased popularity of and accessibility of ride share services such as Uber, Lyft and Didi.

The theoretical required supply in accordance with the City's *Local Planning Policy 7.7.1: Non-Residential Parking* is in the order of 0.15 bays/person or 55 parking bays. However, the anticipated traffic generation associated with the proposal indicates that it will generate a maximum demand of 1 to 4 bays (range difference between arrivals and arrivals/departures) during a typical Friday daytime peak hour between 10 a.m. and 2 p.m. and 10 to 14 bays (range difference between arrivals and arrivals/departures) during a typical Friday or Saturday evening peak hour between 5 p.m. and 10 p.m., respectively. Based upon the results of the detailed car parking surveys noted above, this quantum of car parking demand can be more than comfortably accommodated within the existing and near future public parking supply within 300m to 400m walking distance of the site.

Justification for vehicle parking bays not satisfying the ratios stipulated in Table 1.

The proposal for no on-site carparking can be justified by the following:

- Location of site within an urban/inner-city environment attracting significant walk-in/cycle-in/local/multi-purpose trip making catchment and public transport use.
- Significant availability of public on- and off-street car parking supply within 300 to 400m walking distance of the site.
- Results of detailed parking surveys indicate a significant surplus in the public parking supply during the peak demand periods associated with the proposal which can more than adequately cater to the maximum anticipated car parking demands associated with these periods (as detailed in the TIA).
- This proposed approach is consistent with the City's LPP 7.7.1, the WAPC's D.C. 1.6 and the State Government's Interim Guidance on Car Parking for Non-Residential Uses as well as 'best practice' in the planning for and provision of car parking infrastructure.
- The applicant proposes to pay for two (2) car parking bays as cash-in-lieu remittance as well as minor works associated signage and line marking along the William Street frontage near its north-western boundary to provide for two (2) pick-up/drop-off bays on-street

PASSENGER



Venue Management Plan

378-380 William street, Northbridge, Boorloo

1. Objectives

This Management Plan outlines the Licensee's intended strategies and procedures for implementing its House Policy and Code of Conduct at the licensed premises, with a focus on responsible service of liquor and harm minimization in accordance with the Director of Liquor Licensing's Policy.

The Licensee aims to achieve successful trading and be viewed as a responsible community member. The Plan's goal is to ensure that both the Licensee and Management possess a comprehensive understanding of their obligations under relevant legislation, including the Liquor Control Act 1988. The Licensee endeavours to reduce incidents of harm or illness resulting from the consumption of liquor at the Premises by adopting the principles outlined in this Plan.

Management recognizes that providing a secure and welcoming environment for patrons, staff, and the community is crucial in minimising harm.

2. Staff Training

Passenger Management places great emphasis on the significance of having a highly skilled and experienced workforce, recognizing that well-trained staff are crucial to the success of its business. The company firmly believes that providing comprehensive, relevant, and ongoing training to all employees in responsible liquor service is imperative.

To ensure the highest standard of service, all staff members will participate in engaging and practical in-house training sessions. These sessions will not only be informative but also enjoyable, fostering a positive learning environment. Additionally, appropriate external training opportunities will be provided, covering responsible service practices, product knowledge, and the ability to identify signs of intoxicated behaviour. It is of utmost importance that staff members develop a deep understanding of their legal obligations in relation to the provision and service of food and beverages.

Furthermore, the Licensee and all Management staff will undergo specific training as prescribed by the Licensing Authority and Proprietors and will keep all staff qualifications available for inspection in the Staff Training Register, these include:

All staff members serving alcohol

- Responsible service of alcohol certification

Managers & Leadership positions:

- Management of Licensed Premises (Unrestricted)
- Provide First Aid Course
- De-escalation & OVA Training

This ensures that they possess the necessary skills and knowledge to effectively manage the establishment and comply with regulatory requirements.

Passenger Management remains committed to the continuous development of its staff, recognizing that ongoing training is essential to maintain the highest standards of service and customer satisfaction. By investing in the education and professional growth of its employees, the company aims to create an environment that promotes responsible alcohol service and enhances the overall experience for its passengers.

3. Customer Service

At Passenger, we prioritise delivering exceptional customer service that is professional yet casual. Our management is dedicated to creating a welcoming environment where guests can enjoy themselves while adhering to the highest standards of professionalism and compliance.

We take pride in our knowledgeable and attentive staff who are well-versed in their roles and responsibilities. Whether it's serving liquor or providing other hospitality services, our team upholds a "people-centred" culture and philosophy that puts our patrons at the forefront.

Our team is trained to understand the legal obligations associated with the sale of liquor and are committed to upholding those regulations. We trust that every interaction with our staff will be handled with professionalism and a comprehensive understanding of their responsibilities.

As part of our commitment to responsible service, management undertakes to display responsible service posters in prominent positions throughout the Premises.

4. CONDUCT OF STAFF

Staff will be expected to adhere to the following guidelines:

1. Maintain a friendly, competent, and professional demeanour while carrying out their duties at all times.
2. Discourage any behaviour or activities by patrons that may contribute to irresponsible consumption of alcohol on the premises.
3. Familiarise themselves with the legal obligations regarding responsible service of alcohol.
4. Seek support from supervisors in cases of customer refusal
5. Log all incidents into the Incident Report Register

Refer to Code of Conduct as Attached.

6. SALE OR SUPPLY OF FOOD AND BEVERAGES

The primary focus of our business is the sale of food on the premises, which takes place every trading day.

We are committed to maintaining the highest standards of hygiene in food preparation and meal production on the premises.

All our staff members are trained in safe food handling practices, and we continually reinforce these practices to ensure customer safety.

The licensee, management, and staff are dedicated to the responsible supply of alcoholic beverages on the premises. We strictly adhere to the Liquor Control Act 1988 and the policies set by the Licensing Authority, promoting compliance with acceptable practices.

In addition to alcoholic beverages, we proudly offer and promote a diverse range of non-alcoholic beverages on the premises, which are actively promoted by our staff.

Our management and staff are vigilant in monitoring the consumption of liquor by patrons. We are committed to recognizing early signs of intoxication and taking appropriate action to ensure the health and well-being of all our patrons and staff.

7. JUVENILES

The presence of juveniles on licensed premises is restricted under the Liquor Control Act 1988, unless certain conditions are met. These conditions include:

1. The juvenile must be accompanied at all times by a responsible adult who is supervising them.
2. The juvenile may be present on the premises for the sole purpose of obtaining a meal.
3. The juvenile may be present in an area of the premises that has been approved by the Licensing Authority for providing entertainment primarily for juveniles, where no liquor is being sold, supplied, or consumed.

To ensure compliance with these regulations, the management commits to supporting staff in refusing service and denying access to any patron who they suspect to be a juvenile. If there are reasonable grounds to believe a patron is a juvenile and they cannot produce acceptable identification verifying their age, or if it is confirmed that they are a juvenile and do not fall within the aforementioned exceptions, the licensee, approved manager, and staff have the authority to remove the patron from the premises.

Acceptable forms of identification for establishing age include a current Australian driver's licence with a photograph, a current passport with a photograph, or a Proof of Age Card as specified in the Liquor Control Regulations 1988.

The management will prominently display signage on the premises that explains the laws pertaining to the presence of juveniles on licensed premises.

In situations where a juvenile fails to satisfy the licensee, approved manager, or staff member regarding their age, or if the identification provided is deemed unacceptable, the staff member is legally permitted to request the suspected juvenile to leave the premises. They may use necessary force to remove the juvenile or enlist the assistance of others to do so.

Staff members are prohibited from:

1. Selling or supplying liquor to juveniles, both on and off the premises.
2. Allowing a juvenile to consume liquor on the premises.
3. Permitting the sale or supply of liquor to a juvenile on the premises.

8. REFUSAL OF SERVICE

Refusal of service to a customer under appropriate circumstances is a crucial component of responsible liquor service.

There are certain situations where refusal of service is necessary and obligatory. These include:

- Refusing to sell or provide liquor to an intoxicated individual.
- Prohibiting an intoxicated person from consuming liquor on the licensed premises.
- Preventing violent, argumentative, disorderly, or indecent behaviour on the licensed premises.
- Not allowing drunkenness to occur on the licensed premises.
- Declining to sell or provide liquor to a minor, or preventing a minor from remaining on the licensed premises.

To ensure the minimum possibility of hostility or disruptive behaviour from a customer who has been refused service, it is essential to handle such refusals in a professional, polite, discreet, yet firm manner. When appropriate, the reasons for the refusal should be explained to the customer.

In instances of refusal, the person or persons denied service should be politely requested to leave the premises. If they refuse to comply with the request from the Licensee, Approved Manager, or other staff members, other individuals may be asked to assist in their removal using reasonable force. In extreme cases, involving the police should be considered.

Clear signage emphasising the legal duty of the licensee and staff to refuse service will be prominently displayed within the premises.

It is of utmost importance to handle customers who are refused service or asked to leave in a professional yet firm manner. These patrons should be directed to the House Policy and Code of Conduct in effect at the premises, and alternatives such as non-alcoholic beverages, coffee, food, and assistance in arranging transportation should be offered.

Staff members must promptly notify management of any instances where service refusal occurs and report and log any incidents into the Incident Report Register.

9. ENTERTAINMENT

Passenger's entertainment plan revolves around creating a unique entertainment program that integrates a community-driven radio outlet and an indoor listening lounge. The goal is to provide an enjoyable music listening experience while enhancing the overall hospitality of the venue. However, it is essential to ensure that this entertainment does not cause any offence, excessive noise, or disturbance to individuals residing in close proximity to the premises.

10. NOISE MANAGEMENT

To minimise any potential offence, noise, or disturbance, the following measures will be implemented:

1. Compliance with laws and regulations: All entertainment provided at the premises will adhere to any conditions imposed by the Licensing Authority and all recommendations set out in the Acoustic Report. Additionally, any specific requirements or directions from relevant authorities will be strictly followed.
2. Communication with entertainment providers: Passenger management will effectively communicate to the providers of entertainment the general and special requirements that are in place regarding the provision of entertainment at the premises. This ensures that the performers are aware of the guidelines they need to adhere to while providing their services.
3. Handling complaints: In the event that a member of the public raises a complaint about experiencing undue offence, noise, or disturbance due to the entertainment provided at the premises, the Duty Manager will be promptly notified. The Duty Manager will then take reasonable steps to address the matter and resolve it, if possible. All complaints received will be documented and recorded in the appropriate register for reference and follow-up.

By implementing these measures, Passenger aims to strike a balance between providing an enjoyable entertainment experience and being considerate to the local community. This approach demonstrates a commitment to responsible entertainment management and ensures that any concerns raised by nearby residents are addressed in a timely and appropriate manner.

11. CROWD CONTROL

To maintain order, safety, and a positive atmosphere Passenger management will be implementing effective crowd control measures inside the licensed premises using the following plan:

1. Adhere to Maximum Occupancy: The maximum number of patrons will be set out by local authorities. Management will display the maximum occupancy certificate prominently and ensure that it is not exceeded.
2. Entry and Exit Management:
 - a. Implement controlled entry points to regulate the flow of patrons.
 - b. Clearly mark designated exit routes and ensure they remain unobstructed at all times.
 - c. Train staff to manage entry queues efficiently, maintaining order and preventing overcrowding.
3. Staff Training:
 - a. Provide comprehensive training to all staff members on crowd control procedures, including recognizing signs of potential disruptions and de-escalation techniques.
 - b. Train staff to monitor the crowd, identify any unusual or concerning behaviour, and respond appropriately. Supervisors will undertake De-escalation and OVA certified training courses.
4. Visible Signage and Communication:
 - a. Display visible signage indicating occupancy limits, exit locations, and emergency procedures.
 - b. Use clear and concise communication systems (e.g., public address systems, digital displays) to convey important information to patrons.
5. Security and Staff Presence:
 - a. During peak periods, security will provide assistance with crowd controlling.
 - b. Passenger will ensure all security guards undertaking work on premises are trained in crowd control, conflict resolution, and emergency response.
 - c. Regularly assess the adequacy of security staffing levels and adjust as needed.
6. Regular Monitoring:
 - a. Continuously monitor the crowd density and behaviour within the premises and its immediate vicinity.
 - b. Conduct regular checks to ensure compliance with occupancy limits and address any potential overcrowding.
7. Continuous Improvement:
 - a. Regularly review and update the crowd control management plan based on feedback, incident reports, and changing regulations.
 - b. Encourage staff feedback and suggestions for improving crowd control measures.

By implementing this comprehensive crowd control management plan, management can effectively maintain order, ensure safety, prevent disruptive behaviour, and enhance the overall customer experience in licensed premises.

12. CREATING A SAFE COMMUNITY ENVIRONMENT

Management is fully dedicated to establishing a secure environment both within the Premises and the surrounding community.

One of the primary goals of Passenger Management is to implement responsible service practices at the Premises, with the aim of fostering an environment that prioritises the safety and satisfaction of patrons to the greatest extent possible.

Furthermore, it is crucial that the peace and tranquillity of the Premises' neighbours are not unjustly disrupted by excessive noise or disruptive behaviour from patrons when they exit the Premises. To achieve these objectives, the following measures will be implemented:

1. Staff should ensure that reasonable measures are in place to minimise the escape of undue noise from the Premises.
2. Staff should promote and encourage amongst patrons a respect for the amenity of the neighbourhood in which the Premises is situated, and rights of neighbouring premises not to be unduly disturbed by patrons' behaviour on or off the Premises.
3. Staff will take steps to prevent patrons from leaving the Premises with open cans, bottles or with glasses.
4. Staff will collect on a regular basis empty and discarded glasses, cans and bottles throughout the Premises.
5. Any complaint by a member of the public that they are experiencing undue offence, noise or disturbance as a result of the operation of the Premises will be brought to the attention of Management who will take reasonable steps to resolve the matter (if possible). All complaints are to be recorded in the appropriate register.

HOUSE POLICY

WE PROMOTE THE RESPONSIBLE SERVICE OF ALCOHOL.

Passenger strives to deliver exceptional hospitality experiences through a well-trained passionate team, ensuring the highest standards of service and quality. This includes creating a welcoming and relaxed atmosphere for enjoyment of food and beverages; providing the space and facilities within which meaningful social interactions and expressions of local culture can take place.

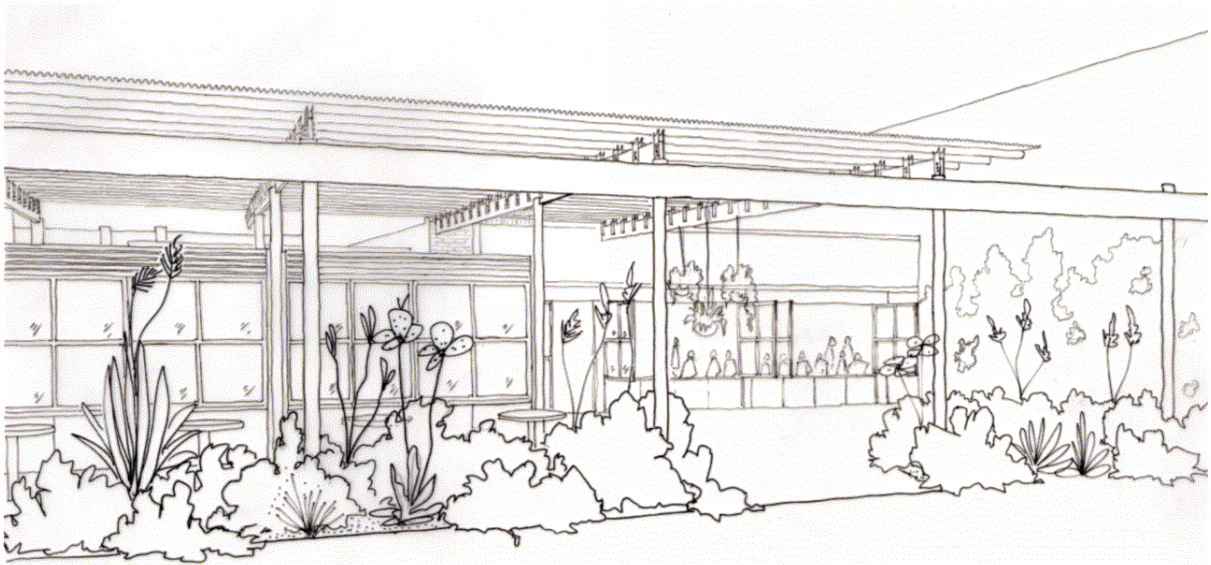
CODE OF CONDUCT

1. **Respect** - At Passenger we celebrate the wide and diverse people and cultures of Whadjuk Noongar nation. We gratefully acknowledge that we're coming together on Aboriginal land, land that was never ceded, and we pay our respect to elders, past and present. Everyone has the equal right to enjoy and participate in our venue. Discrimination or anti-social behaviour towards patrons and/or staff is absolutely unacceptable.
2. **Intoxication** - In accord with our liquor licence obligations under The Liquor Control Reform Act 1998 – WA, you can be refused service if you are intoxicated. Section 3AB of this Act states that a person is in a state of intoxication if “his or her speech, balance, co-ordination or behaviour is noticeably affected and there are reasonable grounds for believing that this is the result of consumption of liquor.” In this instance you will be provided with water and ask to stop consuming alcohol and may be asked to vacate the premises.
3. **Don't Creep** - It is perfectly natural to be attracted to another person. Please feel free to express yourself and engage with others in a gentle and respectful way. Always respect other people's personal space, and if you believe you have been invited into someone's space, then use your manners, be careful and considerate. If it does not work out, then please accept no for an answer, keep your head held high and come and see us at the bar.
4. **Love Thy Neighbours** - Passenger strives to be part of the solution and not the problem. We love our neighbours and we ask that all of our patrons please help us to keep a healthy relationship by leaving the venue swiftly and quietly. Please refrain from walking down residential streets and make your way South towards the city if you need to kick on!
We are available at all times to respond to the concerns or complaints of our patrons and our neighbours. If you have any concerns over the way Passenger operates please contact us by email: info@passengerwa.net

5. **No Juveniles** - Persons under the age of 18 years shall not be permitted on these premises unless accompanied by a Parent/Guardian and shall not be permitted to purchase or consume alcohol. Persons entering these premises MUST produce on demand, valid photographic identification with proof of age (WA Drivers License, Current Australian passport or Proof of Age Card).
6. **We are here for you** - If you are not feeling safe, comfortable or free to be your authentic self, for whatever reason, then please talk to our service or bar staff, or our security team so we can help make Passenger a genuinely inclusive and safe space for all.
7. **Respect our Staff** - Our team's health and safety are paramount. They are a professional and energetic group of individuals who strive to ensure YOU have the most wonderful experience. Any patron showing disrespectful behaviour to any of our staff will be asked to leave the premises immediately.
8. **Bring the community together** - Have fun, step out of your comfort zone, meet new people, loosen the tie (or leave it at home!), share in the good times, be the community you wish to see. Passenger is here for you. If you need an extra pal, please pull up to the bar and if you have any dramas please reach out to management for assistance.

PUBLIC INTEREST ASSESSMENT

PASSENGER



Prepared by:

“The Applicants”

Iraia Andersen & Benjamin Taaffe

Sound Hospitality Pty Ltd

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1. INTRODUCTION

1.1 Terms Used

In this submission, reference to:

01. **Act** means the Liquor Control Act 1988;
02. **Applicant** means Sound Hospitality Pty Ltd;
03. **Licence Application** means the application by the Applicant for the Licence in respect of the Premises;
04. **Development Application** means the application by the Applicant to the LGA for change of use in respect of the Premises;
05. **DLGSC** means the Department of Local Government, Sport and Cultural Industries;
06. **COV** means the City of Vincent which is the local government authority which may intervene or make representations in these proceedings pursuant to section 69(7), Act;
07. **Licence** means tavern restricted licence pursuant to s.41(1)(c) of the Act;
08. **Locality** means the area relevant to the Application as defined in these submissions;
09. **PIA** means this public interest assessment in support of the Application;
10. **Premises** means the land and buildings proposed to be the subject of the Licence at 378-380 William street, Perth, WA;
11. **Regulations** means the Liquor Control Regulations 1989;
12. **RSA** means the responsible service of alcohol; and
13. **Venue** means the business proposed to operate under the Licence and trade under the name 'Passenger' at the premises.

1.2 Overview.

The Applicant is applying for licensing and approvals from both the COV and the DLGSC to transform a disused warehouse into an innovative, community-centred hospitality, retail, and media facility called "Passenger" - servicing local music and creative communities that offers a record store and cafe, a bar and kitchen, a garden bar and an online media/radio hub in the heart of Northbridge. The proposed Venue will be operated by the Applicant's directors: seasoned hospitality entrepreneur Iraia Andersen and events industry veteran Ben Taaffe. In support of the applications for development and liquor licensing approval, this Public Interest Assessment (PIA) has been prepared by the Applicant.

The Applicant is seeking approval from the LGA for the grant of a “Change of Use” Development Application in respect to the Premises, which allows the proposed site to be changed from “District Centre” zone to a “Zone A” (a.k.a. “Discretionary Zone”). An acoustic report has been submitted with the Development Application as required. The Applicant will subsequently apply to the licensing authority for the conditional grant of a tavern restricted licence pursuant to s.41(1)(c) of the Act which states:

41. Hotel licence, kinds, conditions and effect of

... (1) For the purposes of this Act —

(a) where a hotel licence is not subject to any condition referred to in subsection (4) and is not a small bar licence it shall be referred to as a tavern licence; and

(b) where a hotel licence is subject to a condition —

(i) prohibiting the sale of packaged liquor to persons other than lodgers; and

(ii) restricting the sale of liquor to be consumed on the licensed premises, it shall be referred to as a hotel restricted licence; and

(c) where a tavern licence is subject to a condition —

(i) prohibiting the sale of packaged liquor; and

(ii) restricting the sale of liquor to be consumed on

the licensed premises, it shall be referred to as a tavern restricted licence,

and an application may be made for a tavern licence or a tavern restricted licence if the applicant does not seek a licence for a hotel offering accommodation, or for a hotel restricted licence only.

The Venue promises to be a local hub where patrons can gather with friends, listen to music, have a meal, or just relax with a drink. At its core, the venue is committed to providing an environment where quality, music, connection, and community shape the experiences of every visitor. Through this project, the Applicant is seeking to service a growing need for a facility and social space that combines modern practices of digital cultural consumption and production with authentic social and community experiences. In line with its mission, the Venue will feature an extensive program of online radio, workshops, exhibitions, and live musicians and DJs.

The premises is currently an unused derelict warehouse and an adjacent empty bitumen block that provides very little public amenity, and occupies highly valuable civic space along a section of William Street in the Perth / Northbridge area. The Applicant proposes to transform this site from its current state into an iconic hospitality and retail facility that contributes to the cultural enrichment of the area by fostering local culture and connection through a unique combination of digital, cultural and social interactions.

Figure 1 - the premises - existing conditions:



To achieve this transformation, the Venue will consist of four connected components that will be spread over the 980m² site. These include:

- Passenger Bar: A lounge bar and kitchen offering high-quality drinks and simple, fresh and affordable food, with options for casual drinking and dining or a more intimate listening experience in the lounge space that has been crafted for an immersive listening experience.
- Passenger Backyard: A spacious garden bar boasting large shady trees, set behind the online media hub and record store, to create a leafy retreat from the grimey urban landscape beyond.
- Passenger Radio: An online media streaming facility and internet radio program, broadcasting diverse, progressive, and positive signals from Perth to the world while connecting virtual communities with local spaces and real life.
- A world-class street-facing record shop & cafe; Shari Vari Records, stocks a curated selection of vinyl records, music magazines, and clothing, and serves coffee and food.

Full plans, materials schedule and elevations of the Venue design have been provided in **attachment 1 - Plans**.

Figure 2 - Architect sketch of front of venue (inc. landscaping):

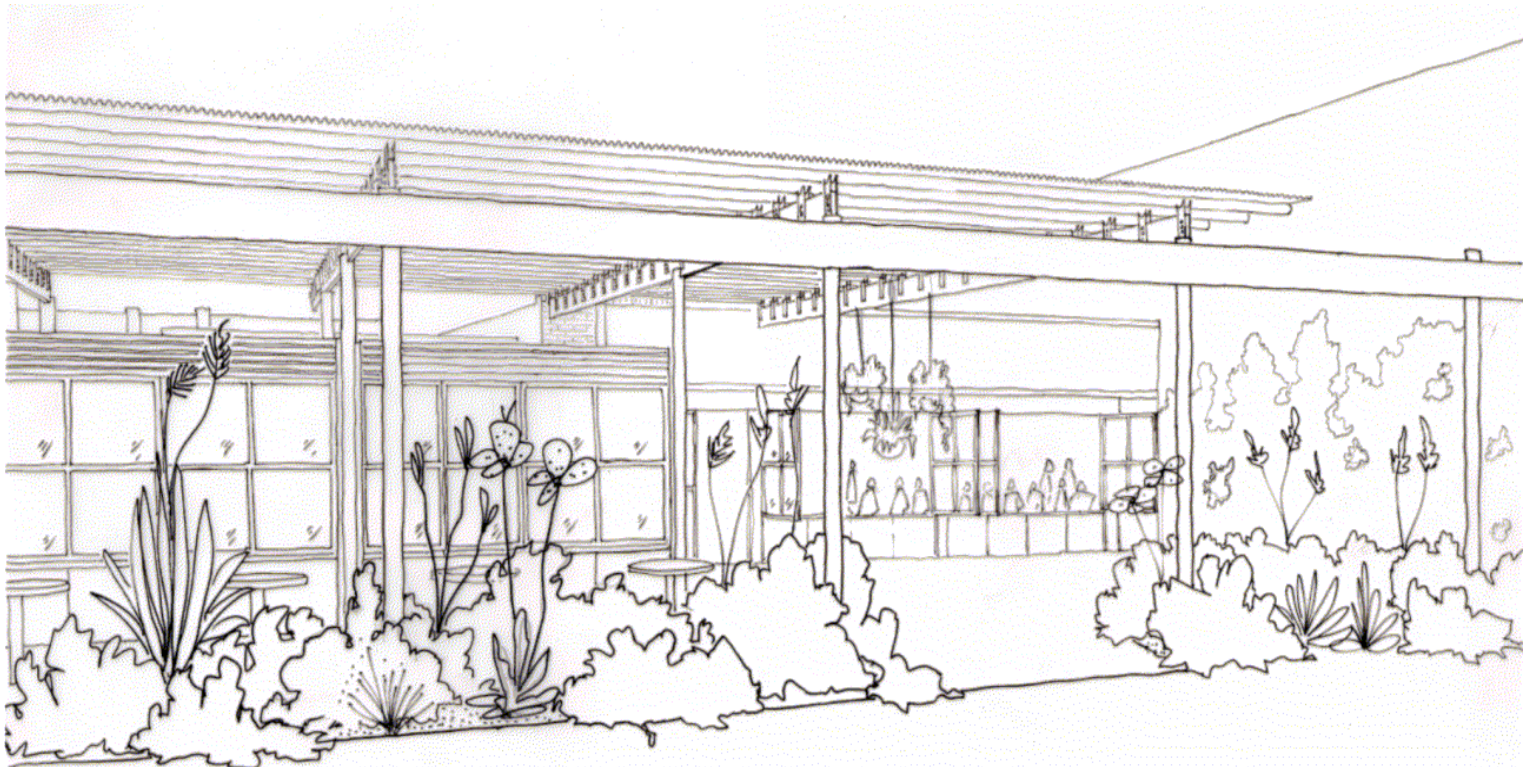
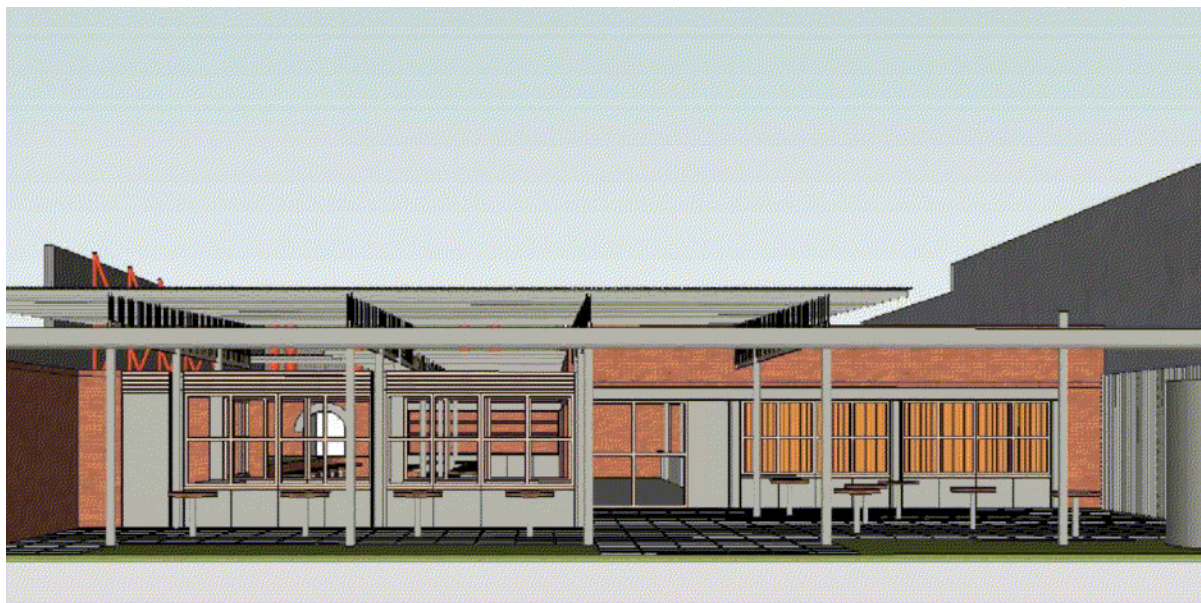
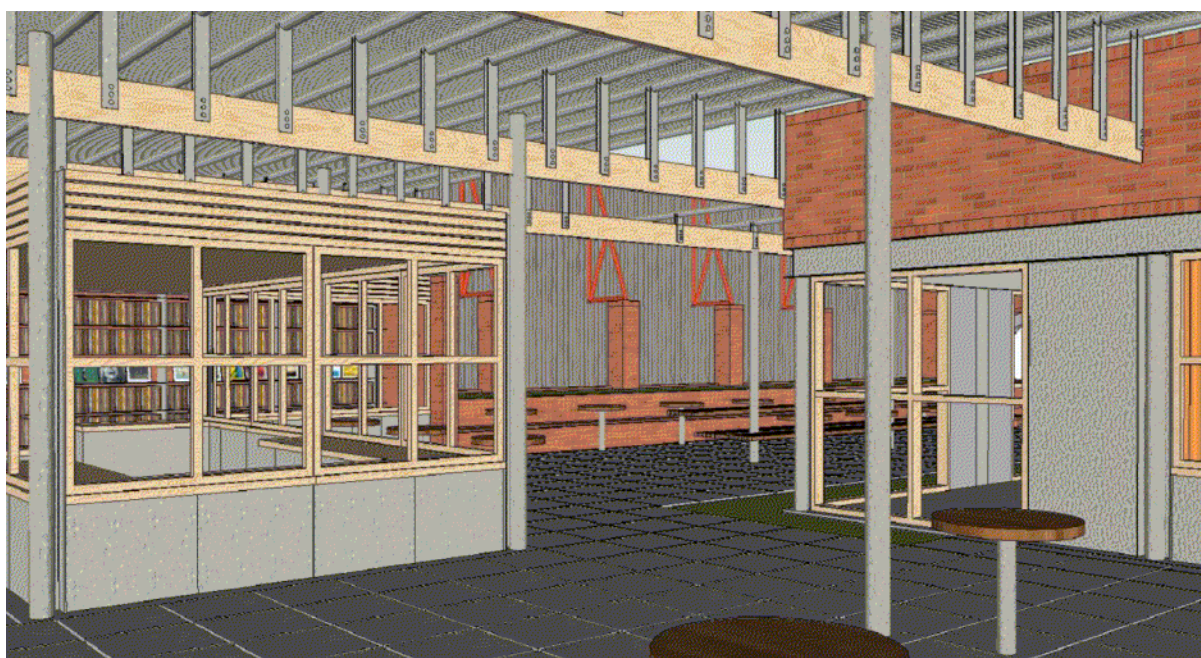


Figure 3.1 - Front perspective of venue (without landscaping):



The Venue will be operated as per industry best practice, with focus on providing a diverse, unique and contemporary offering that is currently unavailable to consumers in the locality. The Venue will adhere to the Department of Local Government, Sport and Cultural Industries' policies relating to the sale and supply of liquor for consumption on the premises pursuant to [the Liquor Control Act 1988](#).

Figure 3.2 - Online Media Booth & view to back garden bar (without landscaping)



In the design and planning phase for the Venue, the Applicant has meticulously considered and addressed issues related to any potential harm and ill-health associated with alcohol consumption. This is a matter the licensing authority may take into consideration in determining the granting of this application pursuant to s38(4)(a) of

the Act. A robust harm minimisation strategy has been developed by the Applicant and included in the Venue Management Plan (Attachment 3 – VMP), with a strong commitment to its consistent implementation to mitigate any potential adverse effects within the vicinity. Training is a core aspect of the Venue's operation, including in-depth customer service, product knowledge, first aid and harm minimisation training. The Applicant will ensure staff qualifications are maintained and logged in the Training Register in line with the Director's Policy on Mandatory Training.

The Venue will be staffed by hospitality professionals trained in the Responsible Service of Alcohol (RSA) and supervised at all times by highly qualified, Approved Managers. Staff will not only enforce legal requirements around the service of alcohol, but also promote social responsibility and healthy, balanced hospitality experiences. An Approved Manager will oversee the premises at all times, ensuring compliance with all licensing and regulatory requirements, and a professional full-time management team will supervise operations, staff, and training.

Consideration has also been given to the demographics of local residents ([Population and Demographics](#), [At Risk Groups](#)) and those travelling to the locality as outlined in [section 4 - Locality](#). Along with the current demographic and population data for the locality, the Applicant has also considered the future growth in the area, with the local population set to increase by more than 30% over the next 20 years. 2021 Census data has been accessed via the [ABS website](#). Information relating to harm reduction and research on drug and alcohol use in Western Australia obtained from [WA Mental Health Commission](#) website, [The Australian Institute of Health and Welfare](#), and the [Department of Health website](#). Information relating to visitor statistics, economic contribution, tourism regions and food and wine tourism obtained from the [Tourism WA website](#).

The Applicant is resolute in their commitment to ensure that the operation of the Venue does not cause any undue offence, annoyance, disturbance, or inconvenience to people who reside or work in the vicinity of the Venue. This is reflected in their carefully designed Venue Management Plan, which includes effective Harm Minimisation, Noise Management and Risk Management Strategies.

In compiling this Public Interest Assessment, the Applicant has also reviewed the DLGSC's [Policy on Public Interest Assessments](#). Accordingly, the Applicant has developed their business model, and venue design with the aim of ensuring that the Venue operates at all times:

- as a safe and comfortable environment for patrons of all ages, abilities and different walks of life.
- without causing any increase in alcohol related harm and ill-health in the community;

- without causing undue disturbance to persons living and working in the Locality; and
- providing a positive contribution to the amenity of the Locality and benefit to the local community.

The Applicant is committed to creating a venue that positively contributes to the amenity, lifestyle and cultural vibrancy of the Locality while minimising the risk of any negative impacts in the community.

This submission also demonstrates how the proposed development aligns with the [COV Development And Design Policy No. 7.5.7 - Licensed Premises](#), [COV Strategic Community Plan 2018–2028](#), the more recent [Strategic Community Plan 2022-2032](#) and the COV [Thriving Places Strategy 2023-2028](#). The City of Vincent (COV) Strategic Community Plan 2018–2028 includes the vision that:

“In 2028, the City of Vincent is a leafy and vibrant 24hr city, which is synonymous with quality design and sustainability. Its diverse population is supported in their innovative endeavours by a Council that says yes!”

The sections of the PIA to follow outline a clear plan and firm commitment to creating a Venue that contributes to a leafier, safer, more sustainable, more enjoyable locality, and supports and services the local community, while ensuring operations minimise any potential harm or disturbance to the area.

As well as the above mentioned documents, several other key COV vision, strategy, planning and policy documents have also been used to inform the planning of the project and creation of this PIA submission:

- [District Planning Scheme No. 2 and associated planning policies](#)
- [COV Development And Design Policy No. 7.5.7 - Licensed Premises](#)
- [COV's Built Form Policy](#)
- [Thriving Places Strategy 2023-2028](#)
- [Strategic Community Plan 2022 - 2032](#)
- [Public Open Space Strategy](#)
- [Public Health Plan 2020 - 2025](#)
- [Sustainable Environment Strategy 2019-2024](#)
- [Waste Strategy 2018 -2023](#)
- [Car Parking Strategy](#)
- [Precinct Parking Management Plan](#)
- [Community and Stakeholder Engagement Strategy](#)
- [Safer Vincent Plan 2019-2022](#)
- [2021 Volume 01 Vincent Town Centre Place Plan](#)

The Applicant notes that at the time of writing this PIA, a William Street or Northbridge Town Centre Place Plan has not been published. The Applicant eagerly awaits the

publication of this planning document and will seek to align all further planning and development processes with the goals of the plan when it is available.

It is also relevant that the City of Vincent is considered to be a supportive council when it comes to growing businesses. The COV website states:

“Activation is a growing trend whereby businesses use their adjacent public space in creative ways to engage customers. The City is focused on encouraging businesses to think outside the box and utilise the public space around their business to expand their business and activate the public realm ... Active places are good for business and they help to create safe, healthy and inspired communities.”

In line with the COV's supportive intentions and related policies the Applicant submits that consideration should be given to the following positive aspects of the application.

The various features of the Venue as described in this submission will combine to create a popular destination that is distinctive and unique in context of the Locality. With its unique combination of hospitality and retail offerings, green space, local cultural programming and digital media facilities, the Venue will attract renewed interest in the area and contribute significantly to the development of local culture and community, as well as boosting the local retail and hospitality industries. As such, the Venue will contribute to the revitalisation of the Locality by creating positive impacts in the areas of livability, vibrancy, safety, economic activity and green spaces for local residents and visitors, while ensuring a safe environment and minimising harm or negative impact on the community.

The Venue has been designed to be a place where digital and online cultural practices intersect harmoniously with local culture and community - a space where people can create social connections and build community while engaging in activities of cultural consumption and production that are increasingly experienced through online platforms only - Eg: buying records and books at the music retail outlet rather than from an online record store; sitting and listening to local radio DJs and musicians host a radio program or performing live, rather than watching a live stream or on-demand through a digital device. In this way the project seeks to fulfil a growing need for a type of public or civic space where social connection and community building can take place around modern forms of digital cultural consumption and production that occur predominantly online. The goal is to counteract the rising trend of isolated, individualised cultural consumption and instead, foster a vibrant social atmosphere within the Venue.

The Applicant submits that the intersection of community engagement, creative participation and local cultural production and consumption with modern hospitality facilities and services, which include the service and consumption of liquor, embodies the proper development of the hospitality industry and will be in the public interest. Bringing these elements together not only enhances the locality and the overall

experience of the clientele, but also promotes an atmosphere of cultural exchange and social connection that can lead to a significantly improved societal well-being and social cohesion.

The result will be an inclusive and dynamic venue that caters to online and “in-real-life” creative communities alike, engaging local City of Perth and COV residents as well as visitors seeking out the unique experiences the venue has to offer. The Venue’s cultural programming will involve a rich and unique schedule of online streaming, live performance, activations, workshops, special events and exhibitions designed to celebrate local culture, engage with diverse communities and foster a thriving music and arts landscape in the area. This unique blend of new media-technologies, local culture and modern hospitality encourages the expression and progression of local cultural identity, providing a platform for diverse voices and experiences. The Venue, therefore, represents more than just a hospitality business - it seeks to contribute to the fabric of a vibrant, inclusive, and culturally rich community.

In conclusion, the Applicant submits that granting the Tavern restricted licence for the Venue will be strongly in the public interest. As a responsible and professional operator, Sound Hospitality Pty Ltd seeks to contribute positively to the local liquor, tourism, and hospitality industries, while serving the interests of the local community and satisfying the evolving needs of consumers. By placing social connections, community engagement and local culture at the centre of its cultural programming and business model, offering experiences and facilities that currently do not exist in the locality and striving to support local culture and community, the Venue will (pending approval) enhance and revitalise the local area.

2. THE ACT

2.1 Legislative Framework – Liquor Control Act 1988

This application is for a tavern restricted licence in accordance with Section 41(1)(c) of the Liquor Control Act 1988, and The Applicant has considered all relevant matters relating to the Objects of the Act, as outlined below.

2.1.1 Objects of the Act

The primary objects of the Act as set out in Section 5 (1) are:

- a) To regulate the sale, supply, and consumption of liquor;*
- b) To minimise the harm or ill-health caused to people, or any group of people, due to the use of liquor;*

c) To cater to the requirements of consumers of liquor-related services, with regard to the proper development of the liquor industry, the tourism industry, and hospitality industries in the state;

As show in the submission throughout this PIA, the applicant is aware of these requirements and will address the primary objects of the Act in the following ways:

As a venue that prioritises responsible practices, the planned beverage offerings and pricing are designed to deter 'at risk' groups in the locality (See Indicative Menu - Attachment 6).

A comprehensive Venue Management Plan (Attachment 3), including the venue's harm minimisation strategy, is formulated and attached to this submission, designed to guide the Applicant and management teams towards a rigorous approach to minimising harm, and responsibly regulating the sale, supply and consumption of liquor.

The Applicant will provide thorough in-house training for staff, encompassing customer service, product awareness, and harm mitigation, with a focus on preventing excessive intoxication and discouraging anti-social behaviour.

Committed to bolstering the local liquor, hospitality, and tourism sectors, the Applicant supports local suppliers of boutique wine, craft beer, and bespoke beverage producers. As an independent entity, the Applicant is well-positioned to foster mutually beneficial relationships with a variety of Western Australian growers, producers and suppliers for their food and beverages menus.

The Applicant contends that bringing together practices of community engagement, creative participation, and digital modes of cultural consumption and production, with traditional hospitality services, that include the service and consumption of liquor, aligns with the proper development of the hospitality industry and is in the public interest.

2.1.2 Secondary Objects of the Act

The secondary objects of the Act as set out in Section 5(2) are:

a) To facilitate the use and development of licensed facilities, including their use and development of live original music, reflecting the diversity of requirements of consumers in the state;

c)? NA?

d) To provide adequate controls over the persons directly or indirectly involved in the sale, disposal, and consumption of liquor;

e) To provide a flexible system, with as little formality or technicality as may be practicable for the administration of this Act;

f) To encourage responsible attitudes and practices towards the promotion, sale, supply, service, and consumption of liquor that are consistent with the interests of the community.

The Applicant plans for the Venue to focus genuinely on meeting the requirements of local residents and those travelling to the Venue for their unique and diverse creative and cultural programming, high-quality food and beverage offerings, and engagement in community-centred experiences not available elsewhere in the locality.

The responsible Applicant is committed to working with the Department of Local Government, Sport and Cultural Industries, the WA Police, WA Department of Health, the City of Vincent, and other relevant statutory authorities throughout the application process and operation of the premises (subject to approval).

All employees involved in the sale and supply of alcohol will be required to undergo the appropriate RSA training from a Nationally Accredited Training Provider, and their qualifications will be kept in a Training Register on the premises in accordance with the [Director's Policy on Mandatory Training](#), last amended on 23 February 2021.

The Applicant will ensure the presence of an Approved Manager at all times to oversee the premises' operation in compliance with all licensing and regulatory requirements relating to 'Managers at Licensed Premises.' The Approved Manager will be available at all times to respond to the concerns of patrons, local residents, and local law enforcement officers.

The Applicant will implement risk management and harm minimisation strategies, incorporating a Code of Conduct and, House Policy into the Venue Management Plan (Attachment 3). This plan will be in place at all times and will apply to patrons, staff, and management in accordance with the [Department's Policy on Harm Minimisation](#).

The Applicant, as an experienced and responsible operator, will ensure that any in-house promotion is suitable and does not support excessive or reckless consumption of liquor in the future. The Applicant will adhere to the Director's Policy on the Responsible Promotion of Liquor at all times.

2.1.3 Public Interest Matters

The public interest matters to be considered under Section 38(4) of the Act include:

"Without limiting subsection (2), the matters the licensing authority may have regard to in determining whether granting an application is in the public interest include —

(a) the harm or ill-health that might be caused to people, or any group of people, due to the use of liquor; and

(b) whether the amenity, quiet or good order of the locality in which the licensed premises or proposed licensed premises are, or are to be, situated might in some manner be lessened; and

(c) whether offence, annoyance, disturbance or inconvenience might be caused to people who reside or work in the vicinity of the licensed premises or proposed licensed premises;

and

(ca) any effect the granting of the application might have in relation to tourism, or community or cultural matters; and

(d) any other prescribed matter.”

The Applicant notes that there are currently no other prescribed matters.

It is also relevant to note that Section 33 (1) of the Act provides that...

“the licensing authority has absolute discretion to grant or refuse an application on any ground or for any reason that it considers in the public interest.

The Applicant acknowledges that all necessary requirements to satisfy Section 5 and address Section 38 (2) and (4) of the Act have been provided within submissions following. The Applicant has diligently addressed the primary and secondary objects of Section 5 of the Act, and all of the matters set out in Section 38(2)(4) of the Act in this PIA. Should there be any minor perceived deficiency in the Applicant's submission, the Applicant requests with respect that the Director applies an appropriate level of discretion and flexibility in granting the tavern restricted licence in the public interest as afforded by this section of the Act.

3. THE APPLICANT

3.1 Applicant Details

The Applicant, Sound Hospitality Pty Ltd, is a proprietary limited company founded by directors and exclusive shareholders Iraia Andersen and Benjamin Taaffe. Sound Hospitality Pty Ltd is a new company whose focus is to create unique experiences that foster community and connection through a rich engagement with and knowledge of music and hospitality. Both founding directors will play a hands-on role in developing, delivering and managing the Venue. Their breadth of knowledge, networks and experience throughout Perth's hospitality and events industries is a key driver to the businesses success. Mr Andersen has previously co-founded, managed and held the position of Licensee in successful hospitality establishments Ezra Pound Bar and Old

Faithful BBQ, while Mr Taaffe has worked extensively in venue programming, marketing and event management and production roles through his own events company Move for over 15 years, and has also volunteered at community radio station, RTRFM for the last 20 years. They are committed to ensuring that the Venue operates in the public interest and minimises any potential negative impact on the local amenities (subject to approval).

Hospitality operations of the Venue will primarily be overseen by Director Iraia Andersen and the Manager of the venue manager. Mr Andersen is an awarded and experienced venue operator and hospitality professional, who strives for industry leading standards of service, quality, safety and accessibility at all times. Together with the staff, he will promote a healthy and sustainable hospitality culture at the Venue with a focus on proactive approaches to harm-minimisation, safety and security.

Cultural and community programming, marketing and engagement operations will be managed by director Ben Taaffe. Mr Taaffe is a successful events producer and program coordinator. His passion for music and connection to community drives a successful twenty-one year career within the Boorloo events, arts and community sectors. Through his events company Move, Ben has built a reputation as a leading figure in the Australian music industry, producing high-quality and dynamic events and collaborating with leading cultural organisations – including Boola Bardip (WA Museum), The Bakery, RTRFM, Fringe World and Red Bull Music Academy.

The directors are well-known for their past quality premises and operations and have very strong support from local residents and stakeholders in the region. It is clear that under the leadership of the directors, the Applicant will maintain a consistent focus on the quality of food and beverages, and aims to improve upon leading industry standards for customer services.

With the Applicants many years of experience producing events, operating hospitality venues and Director Iraia Andersen holding several positions as Licensee across his many years in the hospitality industry, while engaging successfully with local government and licensing authorities, the Applicant is fully aware of the rights and responsibilities linked to holding a liquor licence.

3.2 A Fit and Proper Person to Hold a Licence

Section 33(6) of the Act refers to creditworthiness, character, convictions, conduct of other businesses and reports or interventions made against an Applicant as matters relevant to determining whether the applicant is a fit and proper person to hold a licence. It states:

“...(a) the creditworthiness of that person; and

(aa) the character and reputation of that person; and

(b) the number and nature of any convictions of that person for offences in any jurisdiction; and

(c) the conduct of that person in respect to other businesses or to matters to which this Act relates; and

(d) any report submitted, or intervention made, under section 69 are relevant and amongst the matters to which consideration may be given."

The Applicant's director Iraia Andersen has been previously approved to operate in Western Australia and the Applicant is confident that they will be found fit and proper to manage the Venue under a Tavern Restricted Licence (subject to approval). The directors have demonstrated their creditworthiness, character, and good conduct through their operations in other businesses and their willingness to engage with relevant stakeholders and authorities in the interest of the proper development of the liquor, hospitality, and tourism industry and are qualified to hold a Liquor License in Western Australia.

The Applicant is committed to actively engaging with local government and any other relevant authority to reduce undue, alcohol-related harm in the locality. They have engaged and will continue to engage with the WA Mental Health Commission (MHC), the WA Police Liquor Enforcement Unit (LEU), and various community and nearest 'at risk' groups and representative organisations during the application process. To date, none of the 'at risk' groups contacted have expressed concerns with the grant of the application. All of the 'at risk' groups in the locality will be contacted during the advertising period, through written notifications as outlined by the licensing authority.

4. LOCALITY

4.1 Defining the Locality

The proposed establishment will be located at 378-380 William Street, approximately halfway along the area defined in key COV planning and strategy documents as either the Northbridge Town Centre or William Street Town Centre (more recently adopted).

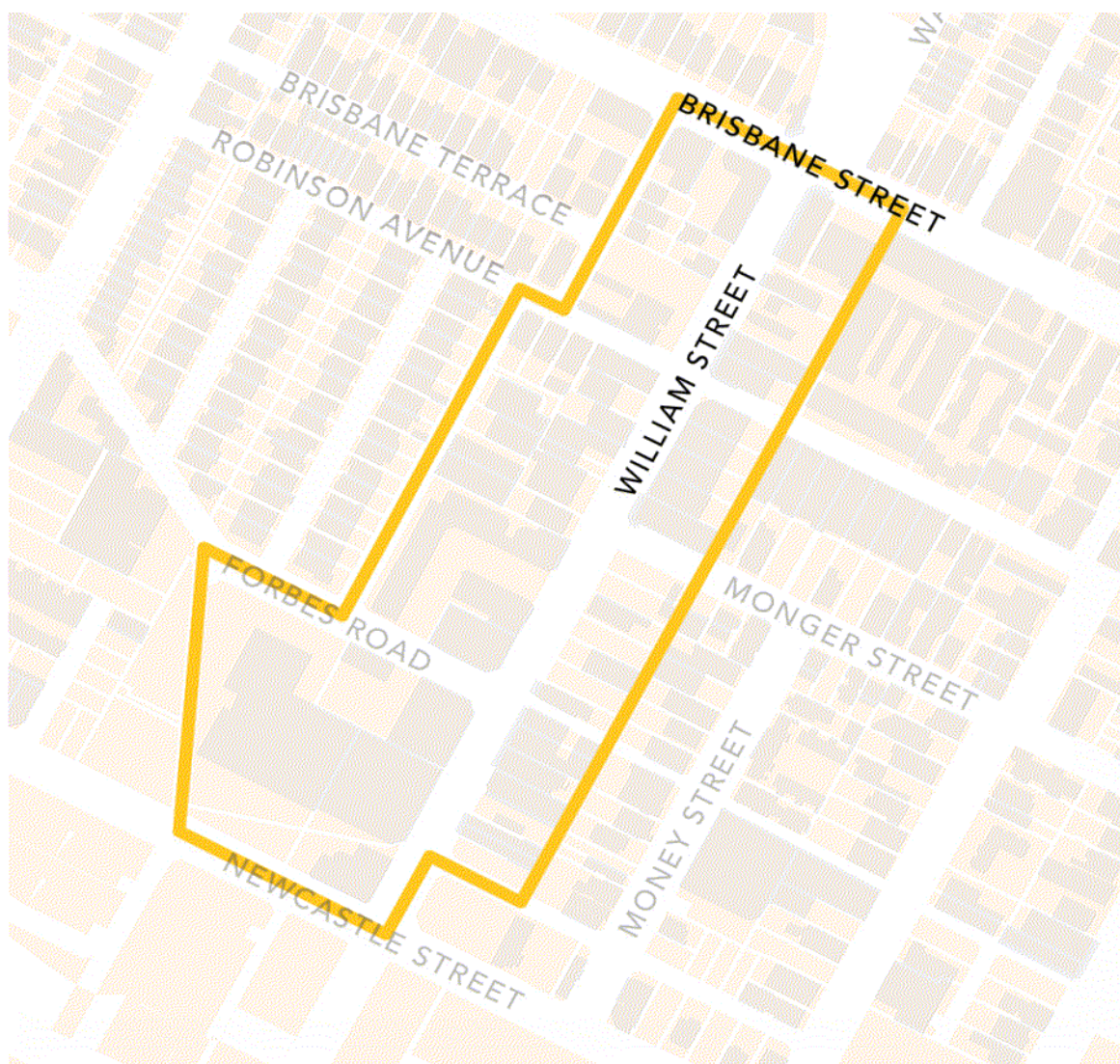
"William Street connects the City of Vincent to the City of Perth. This area is our most demographically diverse Town Centre – a cultural hot pot brimming with restaurants and culinary delights from around the world"

COV has identified Northbridge/William Street as one of six priority "Town Centres" that represent the focal point for economic, cultural and community activities in a particular local area - each with its own unique character and identity, and each providing value and supporting community for the surrounding residents and to the greater COV area.

The Applicant submits that in the context of this public interest assessment, the area deemed the Locality within which the Venue will be situated, is an area within a radius of 2km surrounding the premises, prescribed by the Director's Public Interest Assessment Policy as generally applying to [any suburb located within 15 kms of the Perth CBD](#). The proposed Locality, therefore, includes the suburbs of **Northbridge, Perth, East Perth, North Perth, West Perth, Highgate, Leederville, West Leederville, and Mount Lawley**. Data drawn from these locations is combined to provide an overview of the Locality population and demographics throughout this PIA.

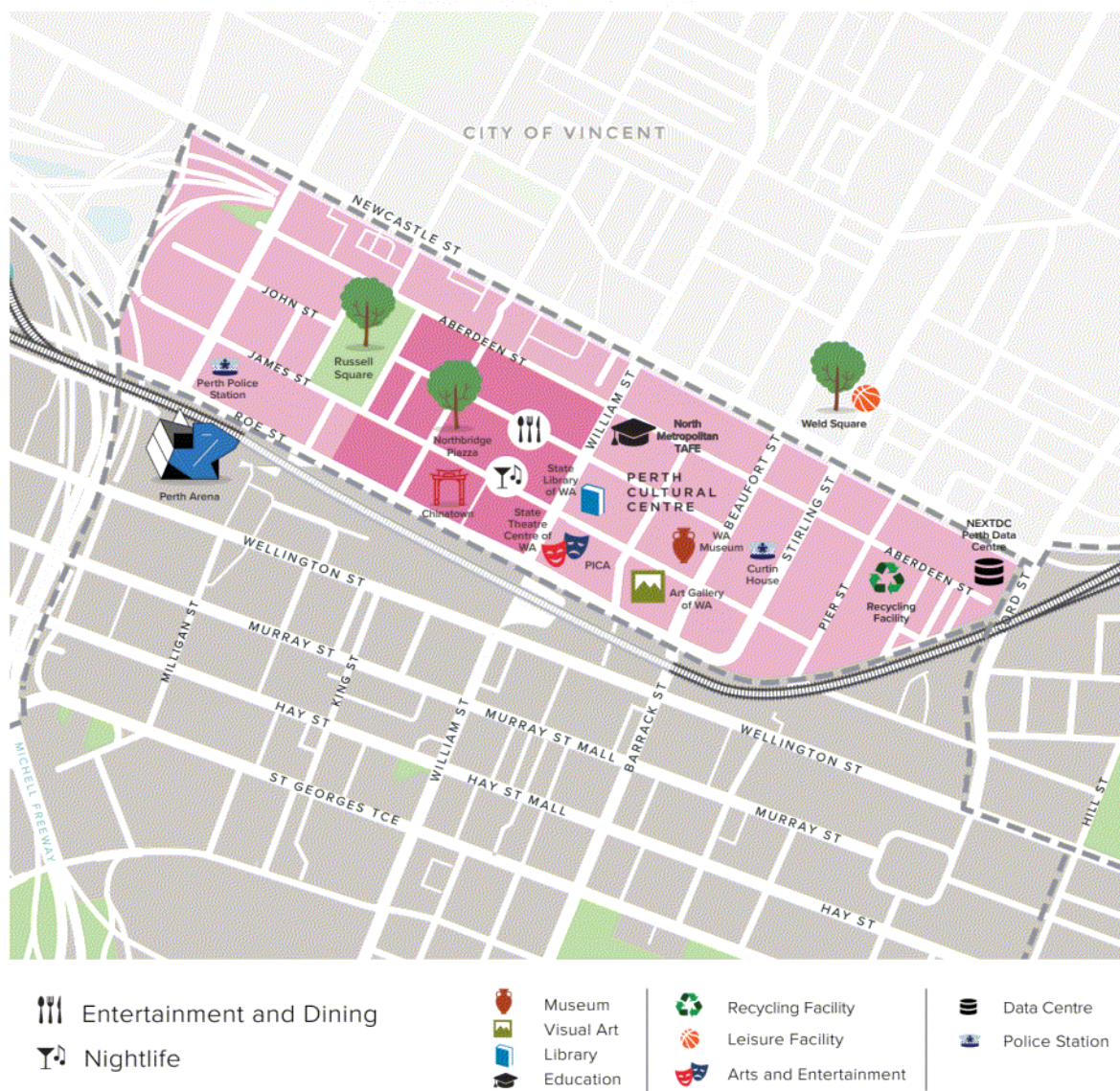
A significant portion of the information and assertions submitted in this assessment report, however, relate more specifically to the area commonly known as either "Northbridge", within the City of Perth, or The William Street / Northbridge Town Centre within the City of Vincent, which sits within the Locality as defined above. This area represents the most relevant and immediately impacted neighbourhood in relation to the Venue and will therefore be given special attention. This area spans across both COV and City of Perth boundaries to encompass the William Street hospitality and retail strip running from Roe Street (COP) to Bulwer Street (COV) as the area widely known in the community as "Northbridge", with Newcastle Street as a borderline between the two LGA districts.

Figure 4: WILLIAM STREET TOWN CENTRE (City of Vincent)



As a matter of clarification, "Northbridge" is defined within City of Perth documents, as an area within the City of Perth boundaries between Roe Street and Newcastle Street, bound by Fitzgerald Street to the west and the eastern suburbs train lines to the east. In the COV documents, the area bounded by Vincent Street, William Street and Lincoln Street in the north, Lord Street in the east, Newcastle Street in the south, and Fitzgerald Street in the west is labelled as the suburb of "Perth"; and within this suburb, a smaller area surrounding William Street from Newcastle Street to Brisbane Street, is identified as either the "William Street Town Centre" or "Northbridge Town Centre". For the purposes of this public interest assessment, however, both of these areas are included in the Applicant's assessment of the relevant "neighbourhood", within which the Venue is situated and the combined areas are referred to simply as "Northbridge".

Figure 5: NORTHBRIDGE (City of Perth)



4.2 Population and Demographics

In the following paragraphs and tables, demographic information, Census data and forecasts have been obtained from the Australian Bureau of Statistics (ABS) directly, and also from the [Informed Decisions online resource](#), which uses ABS data. Data related to all of the suburbs that connect to and either wholly or partially constitute the locality, as listed above, have been used in line with requirements outlined in the Director's Guideline on ['Specification of Locality'](#) as part of the Public Interest Assessment Policy. The applicant notes that for practical reasons, the statistical data listed below, and used to represent the Locality, includes the total data for each of the suburbs that intersect with a 2km radius of the Premises, but is not the exact demographic information of only those residents living within the 2km radius of the venue.

Locality - 2km Radius from Passenger (2km Radius by Suburbs - combined ABS data)

Total Locality Population:	56,031
Median Age:	33

Forecast population	2023	2028	2041	Growth
Locality	56,031	61,681	79,227	1.94% pa
COV	38,923	41,540	49,081	1.31% pa
COP	30,879	33,779	46,226	1.80% pa

"At Risk"	Number	% Locality	% Greater Perth	% WA
Aboriginal or Torres Strait Islander origin	501	0.89%	2.00%	3.30%
families (with children) under 18	4563	17.89%	32.00%	30.20%
	5547	9.90%	22.50%	22.60%

Age Groups (2021)	Number	% Locality	% Greater Perth	% WA
18-24	5315	9.49%	8.60%	8.20%
25-34	14953	26.69%	14.40%	14.00%
35-49	12160	21.70%	21.00%	20.80%
50-59	5654	10.09%	12.40%	12.60%

Attending Higher Education	Number	% Locality	% Greater Perth	% WA
university	4459	7.96%	4.90%	4.20%
Tafe / vocational education	1809	3.23%	2.30%	2.30%
higher education (Total)	6268	11.19%	7.20%	6.50%

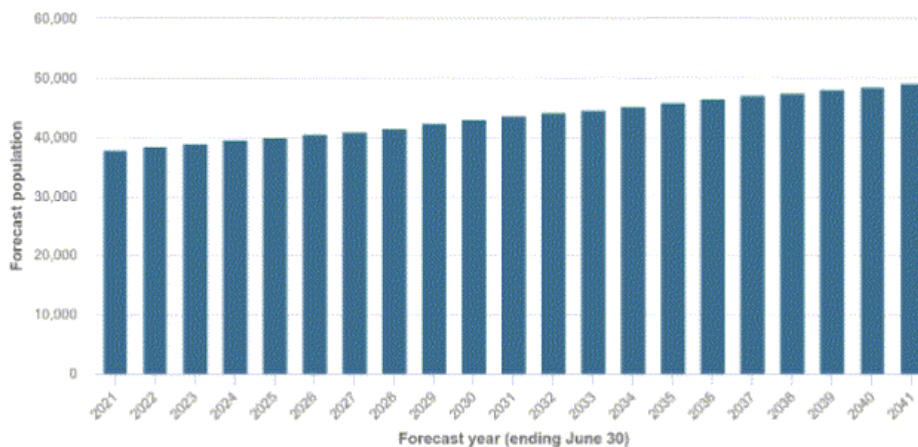
4.2.1 Population

The Locality is projected to have a population of 56,031 in 2023 with a population density measure of approximately 3600 persons per square km, which is expected to continue increasing rapidly with the ongoing processes of in-filling and development of numerous multi-story apartment complexes in the area. The Locality population is forecast to grow to 61,681 by 2028, and further to 79,227 by 2041. This represents an average annual growth rate of 1.94%.

The City of Vincent's (COV) population in 2023 is forecasted to be 38,923, which is predicted to increase to 41,540 in 2028, and then to 49,081 by 2041. The average annual growth rate for COV is 1.31%.

Forecast population

City of Vincent



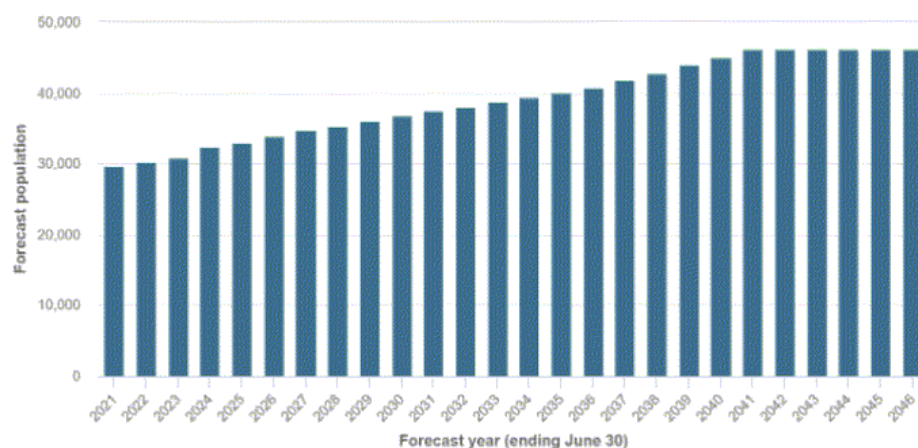
Source: Population and household forecasts, 2021 to 2041, prepared by [jd](#) (informed decisions), March 2023.

.id informed decisions

The City of Perth (COP) is estimated to have a population of 30,879 in 2023, growing to 33,779 in 2028, and dramatically increasing to 46,226 by 2041. The COP is projected to grow at an average annual rate of 1.80%.

Forecast population

City of Perth



Source: Population and household forecasts, 2021 to 2046, prepared by [jd](#) (informed decisions), November 2022.

.id informed decisions

These data show steady population growth in all three regions, with the Locality population growing at the fastest pace.

The Applicant notes that these forecasts differ to the City of Perth's aspirational population target, which sets goals of 55,000 residents in the city by 2036 and 90,000 residents by 2050, relying on government partnerships, policy and program interventions to accelerate development in the area. Accelerating population growth and demand for accommodation in the area is already seen to be driving increased investment in development multi-story apartment complexes for residential and student accommodation markets in the Locality. Plans for development of a new university campus (Edith Cowan University, ECU, and Western Australian Academy of Performing Arts, WAAPA) provides another strong indication for growth into the future.

4.2.2 Age

The Locality represents the area with the most young people (above the age of 18) within the central Perth Suburbs. The Locality has a lower proportion of children (under 18) and a lower proportion of persons aged 60 or older than the City of Vincent, and Greater Perth. The median age in the locality is 33, compared to 36 in the City of Vincent, and 37 in Greater Perth. The predominant age group in the Locality is 20-29 years, which is expected to increase slowly over time with very high proportions of student population, young professionals and couples without dependents and single person households. The largest population growth by age segment in this area between 2021 and 2026 is forecast to be those in the age bracket of 25-34 years. There is a lower proportion of people in the younger age groups (0 to 17 years) in the locality, as well as a lower proportion of people in the older age groups (60+ years).

4.2.3 Travel

The Applicant submits that analysis of the Australia Bureau of Statistics 2021 Census Data reveals distinct travel patterns, whereby residents in the Locality are...

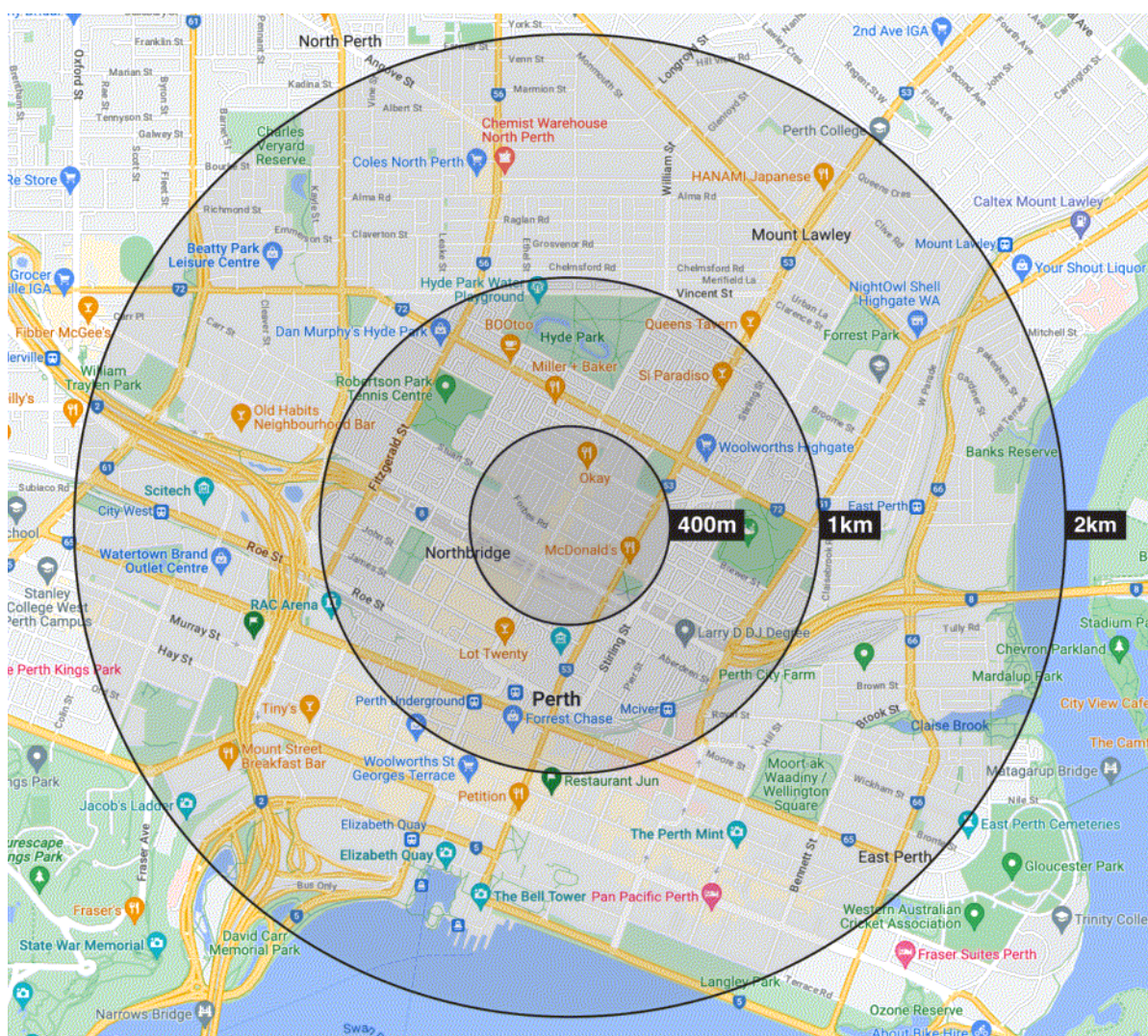
- 1.7 times less likely to Live in a household with a car compared to City of Vincent
- 3.3 times less likely to Live in a household with a car compared to Greater Perth and Western Australia.
- 3.1 times more likely to cycle to work than Greater Perth
- 2.6 times more likely to cycle to work than Western Australia
- 1.4 times more likely to travel to work on public transport than City of Vincent
- 2.3 times more likely to travel to work on public transport than Greater Perth
- 2.6 time more likely to travel to work on public transport than Western Australia
- 1.9 times more likely to walk to work than City of Vincent
- 7.4 time more likely to walk to work than Greater Perth

These findings suggest that many residents within the Locality are more inclined to use active and public modes of transport to travel to work, and/or live within walking distance of their workplace. In addition, the data reflects fewer motor vehicles per household in the locality compared to the broader WA and Greater Perth. Significantly more households in the locality have no vehicle (12.48% vs. 4.9%) or only one vehicle (48.02% vs. 31.6% and 32.2%). There are also fewer households with two vehicles (27.7% vs. 38.9% and 39.4%) and three or more vehicles (8.752% vs. 21.5% and 20.8%).

In summary, the data confirms the Applicant's assertion that residents in the locality are significantly more likely to use sustainable transportation modes such as walking, cycling, or public transport for short trips within their neighbourhood, and that a much higher proportion of households in this locality either have no motor vehicle or possess only a single vehicle.

4.3 Amenities

Please find below a map showing 1km and 2km areas around the Premises (Figure 6):



Here is a list of all active Tavern and Tavern Restricted Licences within a 1km radius of the proposed Venue that have a similar proposed manner of trade:

1. Wines of While - Licence No. 602210157217
2. The Brisbane Hotel - Licence No. 6020001180
3. Aberdeen Hotel - Licence No. 6020002220
4. Brass Monkey - Licence No. 6020035063
5. Beaufort Tavern - Licence No. 6020125477
6. Northbridge Brewing Co - Licence No. 6020128422
7. Queens Tavern (BWS) - Licence No. 6020002170
8. Hyde Park Hotel - Licence No. 6020001701
9. Mars BBQ Bar & Grill - Licence No. 6020135856
10. Court Hotel - Licence No. 6020001412

(Source: <https://portal.rgl.wa.gov.au/forms/fr/search/findallicence/new>)

Despite the presence of these establishments, the Applicant argues that the Venue will offer a unique service within a 2km radius as outlined throughout this document and in [section 10](#), in particular.

As mentioned above, the location also sits in an area densely populated by long-standing Chinese, Vietnamese, Malay, Japanese, Thai and other Southeast Asian restaurants, grocers and businesses, which continue to provide a rich and important source of culture, life and character to this area and form the foundation for a truly vibrant and diverse mix of hospitality and retail business to build upon. As such, the location provides an opportunity to connect and interact with these businesses in a mutually positive way, drawing new patrons to the area whilst engaging with existing ones, and creating a broader range of shopping, dining and entertainment options for all.

5. PREMISES

5.1 Location

The Premises, located at 378-380 William st, Perth WA 6000, is a large, street facing property comprising an unused warehouse with adjacent car park and is ideally located in the vicinity of Perth's cultural, entertainment and hospitality hotspot, Northbridge. The building has been sitting dormant for over 10 years before the applicant signed the lease in June 2023. This location is situated within a well-known and popular destination and experiences a large number of intrastate, interstate, and international tourists year-round, as well as a high number of 'day-trip' visitors, and local regulars. In this context, the Venue is intended to become a popular tourism destination that attracts a

variety of people for numerous reasons. The specific tourism benefits of granting this application will be outlined and discussed in further detail in [Section 10.3 \(Tourism Benefits\)](#) of these PIA submissions.

The location of the premises is highly suitable for the Venue in light of the following features:

- A. Located in the heart of the Northbridge entertainment precinct in proximity to numerous office, retail and residential properties;
- B. Located in a key WA tourist precinct which attracts significant number of visitors every year;
- C. Located in the culturally rich "chinatown" area with diverse south east asian businesses.
- D. Conveniently accessible to persons travelling to and from the premises by active modes of transport such as walking, cycling and e-scooters and various other electronic forms of active transport.
- E. Conveniently accessible to persons travelling to and from the premises by public transport, taxi and rideshare services as well as private motor vehicles.

The Venue's strategic location bridges the gap between the north and south ends of the William Street Town Centre, creating a pathway between other leading hospitality businesses in the lower City of Perth section of the Northbridge area and the upper COV area. This unique positioning allows the Venue to contribute to the development of retail and hospitality in the area significantly, helping to establish a continuous strip of hospitality and retail businesses along William Street.

5.2 Venue Design

Award-winning Architect Peter Fredrick Cole will lead the project's design team, working closely with the Applicant and consultants to create an iconic and cohesive landmark venue. The venue's design will accommodate various amenities, including interior and exterior bars, a commercial kitchen, a small media studio, a cafe and retail space as well as large outdoor garden and alfresco areas. A high standard of fit out, design and décor of the external and internal areas of the Venue will be utilised at a total project budget of approximately \$400-600k.

The vision for the space combines inspiration from iconic listening and lounge bars from around the world with outstanding outdoor and alfresco dining venues - perfectly suited to Perth's world class climate. Design principles and elements will be deployed that promote local identity, acoustic quality and noise reduction, sustainability and durability, interior and exterior gardens, natural light, open spaces and intimate nooks to create the multi-functional hospitality, media and retail hub. Balancing open inviting social spaces with private and focused areas for engagement with our retail, radio or listening experiences is a critical challenge that the design process will resolve.

Figure 7.1: Interior Design References



Figure 7.2: Alfresco & Outdoor Design References



The upgrades include new amenities and finishes, including the following:

- A. Full refurbishment of warehouse including new roof, kitchen, lounge bar and sound treatment throughout;
- B. Upgrade to bitumen area and installation of new toilet block, outdoor bar, media studio and cafe / retail space;
- C. Substantial landscaping upgrades to the bitumen area including 2 mature trees, a raised grass area and many native plants;
- D. New bicycle parking and bin store areas

The result will be a striking and functional design that situates comfortably and respectfully within the existing environment - a humble and unpretentious place for the celebration of local culture, community, high-quality music and sound, bespoke beverages and simply delicious food and community. Inviting long visits, intimate interactions, creative and cultural exchanges towards a healthier hospitality culture.

6. OPERATIONS (Manner of Trade)

The Applicant will ensure that all operations are conducted in accordance with the conditions of its liquor licence and in accordance with the management plan for the Venue. The Applicant will also comply with all relevant laws and regulations. The Applicant has meticulously planned each facet of its proposal, encompassing the following key areas of operation, harm minimisation and risk management:

- Capacity
- Trading Hours
- Target Audience
- Dining Options
- Beverage Choices
- Functions
- Entertainment
- Management, Supervision & Training
- Responsible Service of Alcohol (RSA) Policies & Procedures
- Juvenile Policy
- Safety and Security
- Harm Minimisation
- Neighborhood & Amenity Concerns

These aspects of venue management and operations have been carefully considered and designed to balance the needs of the target market and local community, whilst also mitigating against risk of any potential negative impacts of the operation of the Venue on the local community.

Through carefully designed and professionally executed operational plans, the Venue will develop:

- A reputation for industry leading practices in hospitality, service, safety and accessibility.
- A loyal customer base of local residents from the surrounding locality.
- A strong demand from visiting customers travelling to the locality to engage with the multiple services offered at the Venue.
- Seasonal menus and beverage lists that will be unique and contain many products not available elsewhere in the locality.
- A diverse and locally focused entertainment and activation schedule, designed to promote and enhance social connection and experiences, which remain a priority for the Venue at all times.
- A robust recruitment and training program that will ensure all staff understand the culture and values of the business and the community-centred approach being taken.
- A safety focused and professional culture that respects all patrons, staff and suppliers resulting in a safe, cohesive and respectful work environment.

The operational planning and policies that the Applicant has developed ensure a very high level of confidence that the Venue will operate without negative impacts in the local community, either in terms of reducing amenity or causing undue noise or disturbance in the locality.

6.1 Capacity

The LGA has not yet issued an accommodation certificate for the Premises; however, a maximum capacity of approximately 350 persons and 15 staff is expected.

6.2 Trading hours

The Applicant is seeking trading hours in accordance with the Act;

- Mondays to Saturdays: 7.00am – 12am; and
- Sundays: 7.00am – 10pm

Actual opening hours may differ and vary depending on operational requirements and the various areas within the Premises. For example, it is unlikely that the Premises will trade until midnight across all nights of the week or open every morning at 7am to serve coffee, and it is likely that opening hours will fluctuate seasonally.

6.3 Clientele

Due to the Applicant's reputation and experience in the hospitality and events sectors, there will be a broad range of loyal followers of previous venues and events who will support this project and enjoy the amenity, experience, and services provided at the Venue. The Clientele will include a diverse range of consumers living locally, and travelling from nearby areas within the locality, who can be characterised as:

- 25-45 years of age (the 25-34 yo segment have been identified as [fastest growing age group in Locality](#))
- Local residents living in the locality, and surrounding central or inner-city suburbs.
- Workers coming into the area for employment purposes, and
- Tourists and other visitors travelling to the premises for its unique cultural programming and experiences, community focus and facility, great food, and industry leading service.

As consumers these groups are typically health-focused, culturally diverse, and tech-savvy, with strong connections to their local communities, and a preference for alternative and unique experiences. Artists, musicians, higher education students and young creative industry professionals form priority segments of this target market. They can be characterised as politically and culturally engaged and early adopters of new media platforms and technologies - including social media platforms, streaming content devices and platforms, as well as online booking and digital ordering/payment options. Driven by a growing awareness of the positive impact of local economies on our communities and environment, there is an increasing support for local businesses, products, and services found among this cohort. They seek authentic experiences, favouring socially and physically tangible products and services, local stories and connection, over globalised trends and mass produced cultural forms.

To name a few; the venue is located in close proximity to several educational institutions, including North Metropolitan TAFE, SAE Creative Media Institute, and the new Perth City campus of Edith Cowan University and the Western Australian Academy of Performing Arts scheduled for opening in 2026.

City of Perth Chair Commissioner Andrew Hammond recently commented in relation to the new Perth City University campus;

["The scale of a tertiary education development of this size would be greater than anything that's ever been seen before, catering for around 6,000 students and 1,000 staff attending the facility each day. Economic data suggest students would spend almost \\$70 million a year within the local economy, and visiting relatives and friends, would spend around \\$4 million. This endeavour would add enormous value to the city's day and night time economies, and would be the catalyst that we are all looking](#)

for to stimulate our population growth so we can better underpin our core retail area."

The Venue is, therefore, well-positioned to attract a growing local customer base seeking creative engagement, connection, and progressive hospitality services. This market segment includes young musicians, DJs, record collectors, artists, and creatives, students, young professionals, and friends and associates of these groups who seek local cultural experiences and sustainable services. Among these groups there is an unmet demand for spaces to engage in local media, music and culture as they seek authenticity, community, and sustainability in modern life.

The Applicant is aware that adherence to a carefully devised dress code can be an effective way to manage licensed premises and patron profile. The dress code for the Venue will be "smart casual" and employed with discretion by security and staff. The dress code will be used to ensure that the Venue is attended by the desired demographic and to minimise persons in from 'at risk' groups from seeking to attend the Venue.

6.4 Dining

Food service is the central aspect of the venue's hospitality offerings, with the design and menus created to invite all patrons to engage in a dining experience. A well-sized, fully-equipped kitchen (refer to venue plans - attachment 1) will handle a high volume of diners and cater to a diverse range of consumer needs. Dining services, aligning with Responsible Service of Alcohol (RSA) principles, will be available until half an hour before closing.

The venue offers ample seating for patrons in the spacious alfresco dining areas, both front and rear, and in the indoor dining sections during all trading hours. Designed to provide both semi-formal and casual dining options, the venue facilitates a laid-back, comfortable environment for food and liquor consumption at any time. Attentive yet casual service will be offered across all areas of the venue allowing the servers to have a comfortable overview of its patrons regarding RSA. Table service will be provided in both the dining room and lounge area, while Patrons enjoying our outside areas will be able to place orders for food through a digital online ordering system available at each table, or at any of the venue's bars.

This kitchen will be fully furnished with the necessary equipment to cater to the anticipated substantial demand for dining services. The Venue's food menu will be built upon principles of simplicity, freshness, value, approachability, and sustainability, with a focus on modern charcoal-fired and roasted dishes. To support local businesses and promote sustainability, menus will incorporate locally sourced, fresh, seasonal produce whenever possible. This aligns with the venue's dedication to sustainable practice and positively impacting the local community and environment. Menus will be updated

regularly to maintain a dynamic dining experience, featuring a modern spin on Australian charcoal BBQ and roasts. It is important to note that this style of cuisine is currently not available anywhere in the area and, therefore, will provide a unique offering in the locality. See indicative food menu - attachment 7.

6.5 Beverages

The Applicant will offer beverage services with an emphasis on quality products and excellent service, including the Venue's passionate wine program focusing on small-scale, family-owned and operated wineries with an emphasis on eco-friendly, organic and minimal intervention styles of winemaking; and a small range of tap beer pouring a list of beers from leading independent Australian brewers. See indicative drinks menu (attachment 6).

Through their food and beverage program, the Applicant is committed to supporting the development of the local liquor, hospitality, and tourism industries by supporting smaller producers of fresh produce, boutique wines, craft beers and other bespoke beverages. Being independently owned and operated, the Applicant submits that they can develop close, supportive and mutually beneficial relationships with a range of Western Australian producers and suppliers.

Importantly, the Venue's hospitality program adds a significant dimension to the venue's overall experience - promoting and supporting a fast growing culture of health-conscious, environmentally aware, locally connected and globally conscious producers and consumers. The products available will not be the type of products that would typically appeal to customers who are seeking liquor products simply for the intoxicating effect. Furthermore, the nature of the beverage offerings on sale at the Venue and the price point of products to be sold are not expected to appeal to 'at risk' groups in the locality.

The Venue will be proactive in its approach to ensure moderate and health-conscious consumption experiences are the norm. The Applicant will ensure that a large range of mid-strength, low strength, and non-alcoholic products are promoted as a key component of the beverages menu and will be available to patrons at all times. Knowledgeable, attentive but casual service approach will be anchored in the Venue's "people centred" culture and philosophy and commitment to RSA policies and procedures. Staff will be provided training on how to match food and beverages, and will encourage consumers to always purchase food with their beverages. The range, promotion and service of liquor products will be subject to strict RSA policies and procedures. For further details of the Applicants approach in this regard, a robust harm minimisation strategy has been included as part of the Venue Management Plan and included as an attachment to this submission (attachment 3).

6.6 Functions

Given the venue's size and central location, the Applicant expects that functions and larger bookings will be sourced through the diverse community groups that engage with the venue throughout the year, and from business and corporate groups during holiday and other peak season periods. The Venue design does not include any dedicated function spaces, but will host private events using temporary reserved areas as required. All functions must comply with the venue's house policy, code of conduct and management plan to ensure their conduct aligns with set guidelines. Strict monitoring and control will be exercised over these functions to prevent any negative impact on the local community.

6.7 Entertainment

The Applicant will develop and deliver an evolving schedule of entertainment and community programming to activate the various areas at the Venue and engage their clientele. The programming will include DJ performances in the listening lounge area, live internet radio streaming events hosted from the internet radio booth, low-volume acoustic performances, retail activations and other low-impact social events. It is important to note that, while music communities and related practices such as DJ performances, online streaming, music retail and radio and are central to the identity and purpose of the Venue, the primary function of all hospitality areas, and the experience that the Applicant will deliver therein, is of a social space - the venue is not intended to operate as a concert, live performance or entertainment programme driven business. See indicative entertainment program - attachment 8.

The cultural programming and experiences that will be developed at the Venue will be undertaken in conversation with the local community, in collaboration with leading local creatives, and specifically selected for their unique characteristics. As a priority, the Applicant will ensure that entertainment is always conducted in accordance with the conditions of its liquor licence and in accordance with the management plan for the Venue. Any and all entertainment programming will be conducted in a way that promotes diversity, accessibility and inclusivity, respects the rights and safety of patrons and staff, and does not disrupt the local community.

Staffing & Staff Culture

Staffing levels will be strictly controlled to ensure a high level of customer safety, service and satisfaction at all times, with additional staff rostered on during peak times. Staff safety and wellness is a priority for the Applicant and a culture of healthy, balanced and sustainable hospitality experiences will be promoted at the venue starting with our service team and extending to our clientele. With a strong community and local cultural

programming focus in place at the Venue, the engagement between staff and consumers will remain strong throughout all aspects of Venue operations.

The Applicant understands that staff can fulfil an important role as role-models for safe, respectful and peaceful behaviour at the Venue. Staff roles in relation to safety and harm minimisation will extend beyond enforcing the legal requirements and LGA policies around service of alcohol, and other safety and security related concerns. Staff at the Venue will also take the leading role in pro-actively engaging with clientele to promote social responsibility, and healthy, balanced and sustainable hospitality experiences at all times. There will be a culture of risk and harm prevention, as part of the harm minimisation strategy of the Venue.

Training will be a mainstay of the premises, where all staff will be provided with comprehensive in-house training focused on customer service, health and well-being, product knowledge, and harm minimisation, with a focus on the prevention of undue intoxication and anti-social behaviour.

Section 5(2)(d) of the Act states that one of the secondary objectives of the Act is:

“...(d) to provide adequate controls over the persons directly or indirectly involved in the sale, disposal, and consumption of liquor;...”

In order to achieve this directive, the Applicant will ensure that:

- all employees involved in the sale and supply of alcohol will be required to undergo the appropriate RSA training from a Nationally Accredited Training Provider, and their qualifications will be kept in a Training Register on the premises in accordance with the Director's Policy on Mandatory Training, last amended on 23 February 2021 (More details provided in section 8, below); an Approved Manager will be present at all times to oversee the premises' operation in compliance with all licensing and regulatory requirements relating to 'Managers at Licensed Premises'; and
- an Approved Manager will be available at all times to respond to any concerns of patrons, local residents, and local law enforcement officers.

7. HARM MINIMISATION & RESPONSIBLE SERVICE OF ALCOHOL (s38(4)(a))

In determining this application the licensing authority will consider if any harm or ill-health might be caused to people, or any group of people, due to the use of liquor proposed at the venue. In particular, Section 38(4)(a) of the Act states that;

"Without limiting subsection (2), the matters the licensing authority may have regard to in determining whether granting an application is in the public interest will include

(a) the harm or ill-health that might be caused to people, or any group of people, due to the use of liquor;..."

The Applicant recognizes that alcohol can be misused and cause harm if not responsibly consumed. The Applicant is committed to adhering to the Department of Racing, Gaming and Liquor Industry Guideline on the Responsible Promotion of Liquor. The industry guideline;

"provides the industry with a framework of practices which are considered acceptable in order to prevent the intoxication and antisocial behaviour of patrons and, in all other respects, to ensure that the premises are being properly managed. The framework highlights those practices which are discouraged and are not in the public interest."

The following section of the PIA outlines the Applicant's planning and strategies designed to minimise any risk of harm or ill-health that could result from the sale and use of liquor at the venue. This includes:

- Management, Supervision & Training
- RSA Policies and Procedures
- Juveniles and other "At Risk" groups
- Safety and Security

In these areas of operations, harm minimization will be a very important concern for the Applicant, and the Applicant notes that its track record of operating hospitality businesses and events in the locality is distinguished by a complete lack of breaches, cautions, or infringements from the DLGSC, LEU, or other statutory office; no complaints from local residents; no complaints from local government authorities; and no complaints or negative feedback from 'at-risk' people or groups of people in the locality.

To continue in this manner, the Applicant will employ formal controls such as:

- Harm minimization measures,
- Juvenile management and procedural policies,

- Responsible service of alcohol practices, etc.,

and informal controls including:

- Active community engagement and participation in the Venue, and
- Ongoing engagement with 'at-risk' groups and/or organisations.

The following sources have been reviewed and considered by the Applicant to gain a better understanding of the harmful effects of alcohol abuse and various strategies recommended for minimising such harmful effects:

- WA Police (www.police.wa.gov.au)
- Australian Institute of Criminology (www.aic.gov.au)
- University of Western Australia (www.crc.law.uwa.edu.au)
- Office of Crime Prevention (www.crimeprevention.wa.gov.au)
- Population Health (WA) (www.population.health.wa.gov.au)
- Drug & Alcohol Office (www.MHC.health.wa.gov.au)
- Department of Racing Gaming & Liquor (www.rgl.wa.gov.au)
- Australian Bureau of Statistics (www.abs.gov.au)
- National Drug Strategy (www.nationaldrugstrategy.gov.au)
- Office of Road Safety (www.officeofroadsafety.wa.gov.au)
- WA Planning Commission (www.wapc.wa.gov.au)
- National Drug Research Institute (www.ndri.curtin.edu)
- Department of Indigenous Affairs (www.dia.gov.au)
- National Health & Medical Research (www.nhmrc.gov.au)
- National Centre for Education & Training on Addiction Adelaide (www.nceta.flinders.edu.au)
- Australian Government Department of Health & Ageing ([Http://health.wa.gov.au](http://health.wa.gov.au))
- West Pilbara District Education Office (www.det.wa.edu.au)
- Health Services (WA) (www.health.wa.gov.au)
- Australian Transport Safety Bureau (www.atsb.gov.au)

Information from these sources has been used by the Applicant to inform its harm minimisation and responsible service of alcohol practices. Should this application be successful, the Applicant will be committed to regularly review and comply with any new strategies or policies related to minimising harm or ill-health due to the use of liquor, as proposed by the above-mentioned sources, as well as any other regulatory authorities.

7.1 Social Health Indicators

Over the last half-century, Australian drinking habits have notably shifted towards moderation, driving down alcohol consumption to levels not seen since the 1960s. Several reputable publications have published research outlining these changes,

including [Wine Australia](#) and the [Australian Bureau of Statistics](#), In the period 2017-2018, the total alcohol consumed was 186 million litres of pure alcohol, averaging 9.4 litres per person aged 15 and over, the lowest figure since 1961-62. This downward trend initiated around 2008-09 continues to date.

A significant finding from the [2019 National Drug Strategy Household Survey \(NDSHS\)](#) is that fewer Australians exceed the NHMRC guidelines for lifetime risk by consuming over two standard drinks per day - down from 21% in 2001 to 16.8% in 2019. Alcohol-induced deaths have also seen a decrease, standing at 5.1 per 100,000 Australians in 2017, compared to 6.6 deaths 20 years prior.

In addition, the Australian Institute of Health and Welfare reports the significant change in the consumption of different alcoholic beverages from 1967-68 to 2017-18, notably the decrease in beer consumption (from 73.5% to 39.0%) and increase in wine consumption (from 14.4% to 38.6%). An increased preference for premium liquor has also been identified alongside increasing health consciousness that has led Australians towards adopting "[less but better](#)" and "[mindful drinking](#)" practices. There's also a notable rise in the adoption of low or no-alcohol alternatives.

Various surveys and government resources reflect a reduction in Australians exceeding lifetime risk guidelines. For example, the [National Drug Strategy Household Survey in 2016](#) and the [National Health Survey 2020-21](#) reported declines in the proportion of people aged 14 and older, and adults aged 18 and over, respectively, exceeding these guidelines;

[One in six \(16.1%\) persons aged 18 years and over consumed more than two standard drinks per day on average, exceeding the lifetime risk guideline in 2017-18. This continued to decline from 17.4% in 2014-15 and 19.5% in 2011-12.](#)

[Whilst men were more likely than women to exceed the guideline, the proportion of men exceeding declined since 2014-15 \(25.8%\) whilst for women the rate remains largely unchanged \(9.3%\)](#)

[The National Alcohol Strategy 2019-2028](#) further echoes this decline in alcohol consumption over the past 40 years and reports a steady or rising trend in abstinence, particularly among younger Australians.

Mirroring national patterns, Western Australia has seen a decline in annual alcohol consumption levels and associated risks over the recent decades. [Key insights from local surveys](#) reveal a decrease in the number of people drinking weekly and an increase in those abstaining from alcohol. Finally, the [Health and Wellbeing of Adults in Western Australia 2019](#) report indicates that most Western Australians are at low or less than low risk of long-term and short-term alcohol-related harm, while the percentage of high-risk consumers is diminishing.

Overall, these data present a promising trend in reducing the risk of alcohol-related harm due to the decrease in overall alcohol consumption and the rise in moderate drinking practices in Australia. They offer compelling evidence for the success of government and industry aligned strategies targeting “at-risk” drinkers and harm-minimisation and highlight a gradual improvement towards a culture of moderate and safe consumption of alcohol and the resulting decline in the risks associated with alcohol use in Western Australia.

Despite these positive trends, the Applicant acknowledges the ongoing responsibility to operate safely, responsibly, and in the public interest. Considering the available research and statistics to inform harm minimisation strategies, combined with the Applicant’s previous experience operating licensed premises, an in-depth knowledge of the local area, and the Venue’s stringent [RSA procedures](#) and [harm minimisation strategy](#) (outlined below), the Applicant believes the proposed venue will not unduly contribute to alcohol-related hospitalisations and deaths in the area.

7.2 Crime & Violence

To be effective in reducing alcohol-related violence, the research suggests that policy and practice need to give importance to the;

[“...need for a range of partners within the community to work together to set appropriate standards for the consumption of alcohol and set formal and informal controls on the misuse of alcohol and the problem behaviours that result.”](#)

The Applicant is committed to being actively and regularly involved on a community level, as demonstrated in the passages throughout this document that outline the community engagement principles central to the Venue’s operations and identity.

The Applicant is also committed to working with the local Police, LEU, and CHO as required to mitigate and minimise any undue, potential negative risk that may result within the locality. As described in the [community and stakeholder consultation section](#) in chapter 8, the Applicant has engaged relevant authorities and stakeholders from the early stages of business planning and development, and will continue to do so.

The Applicant understands the potential for undue, negative social impacts related to the sale and supply of liquor, which will be evident in their Harm Minimization Plan and staff training and induction procedures provided to all staff upon employment (subject to approval).

The proposed premises align with the principles outlined in the "Designing Out Crime Planning Guidelines" issued by the WA Planning Commission. Key features include:

1. **Appropriate Location:** The premises will be situated in a designated mixed-use area zoned "District Centre", thus ensuring compatibility with neighbouring entities.
2. **Constant Monitoring:** The site will be under constant scrutiny from both staff members and surveillance equipment to ensure a secure environment.
3. **Specific Entry/Exit Points:** The design includes dedicated locations for entry and exit, helping to regulate movement in and out of the premises.
4. **Passive Surveillance:** Both staff and patrons in and around the premises will assist in monitoring the surrounding streetscape through passive surveillance, thereby contributing to overall neighbourhood watchfulness.

The Applicant is committed to understanding any existing levels of harm or ill-health related to alcohol use in the locality so that limited resources can be put towards the most effective means of minimising any potential harm related to alcohol use in the Locality.

To gain a detailed view and understanding of the issues, the Applicant has thoroughly engaged with the latest crime statistics provided by WA Police for the 2km area designated as the 'specified locality' under the Director's PIA Policy. Given the venue's specific location, the professional and comprehensive safety and security planning and the local and community focused nature of the amenities provided, the Applicant submits that it is highly unlikely that an increase in criminal activity will occur as a result of the application approval.

7.3 Strategies to Minimise Harm or Ill-Health

As outlined previously in this submission, the Venue's harm minimisation strategies and RSA policies and procedure, found in the Venue Management Plan (Attachment 3), will minimise any potential, undue harm or ill-health that could be caused to the community. Along with stringent harm minimization practices, the manner of trade, dress standards, attentive staff, and high standard fit out will also assist to continue to attract the mature and responsible patrons that the premises intends to cater to.

Further to this, [COV Safer Vincent Plan 2019-2022](#) emphasises the importance of ["activities that connect residents, businesses and visitors, as well as projects that activate public spaces"](#) to create actively engaged and connected communities where people are less likely to engage in anti-social or self-destructive behaviour, and more likely to look after each other and stand against anti-social behaviour;

["Building and maintaining these strong community connections creates a sense of safety within local neighbourhoods, provides support networks where people look out for each other and strengthens passive surveillance. Activities that promote community involvement are particularly important for people in our community who are socially isolated and vulnerable as they are already at a higher risk of being a](#)

victim of crime.”

The Applicant submits that by activating an underutilised area and unoccupied site along the William Street “high-street”; by engaging with local community to create a sense of local cultural identity, community connectedness and community ownership at the premise; and by increasing local community events and cultural activities, the Venue will contribute to the prevention of anti-social sentiment and behaviour in the locality.

7.4 Promotion

The Liquor Control Act 1988 places an obligation on the Applicants to sell and supply liquor in a responsible manner. The uses of ‘gimmick’ promotions of cheap or discounted liquor that are likely to encourage irresponsible consumption of alcohol are not acceptable. Any activities to promote their business will not be an incentive for patrons to consume liquor more rapidly or in greater amounts than they normally otherwise would.

7.5 Management, Supervision & Training

The Applicant will form a robust, full-time management team, comprising experienced professionals such as an operations manager, venue manager, assistant managers, and a dedicated functions manager. Their role will encompass overseeing operations, supervising staff, and administering effective training programs.

7.5.1 Training and Certification

Induction: All new ‘front of house’ staff including bar, waiting, and glass collection staff, will undergo comprehensive induction training. This will stress the importance of responsible alcohol consumption, identify signs of intoxication, and formulate effective measures to manage patrons exhibiting such signs.

Ongoing Training: The training will continue, with a focus on encouraging patrons to balance alcohol consumption with food and water or non-alcoholic beverages.

Refresher Training: Regular sessions will be held for all approved managers and general staff, emphasising RSA principles.

RSA Certification: As part of the commitment to safety and legality, all front of house staff must hold a valid Responsible Service of Alcohol (RSA) Certificate.

Product Trials: New products will be routinely trialled by staff to understand their appeal, appropriate serving methods, and suitable food pairings.

Additional Training: Besides RSA, staff will receive regular training that includes detailed product knowledge, exceptional customer service, and first aid.

This comprehensive approach to training ensures all staff at the Venue are equipped with an in-depth understanding of harm minimisation principles, RSA policies, the products they sell, their origins, and their pairing with various food offerings.

7.5.2 Awareness and Compliance

The Applicant pledges to ensure that the management and staff remain updated about their responsibilities under the Liquor Act and the Liquor Regulation. This entails making sure that all policies and procedures are communicated to all staff regularly and updated as necessary. The records of these trainings will be maintained on-site as per the [Director's Policy on Mandatory Training](#).

7.6 RSA Policies & Procedures

The Applicant is devoted to implementing a thorough RSA policy, encompassing measures designed to promote responsible alcohol consumption, prevent intoxication, and handle any instances of intoxication effectively:

1. **RSA Training and Certification:** All bar and service staff will complete RSA training, with certification records maintained on-site. The management team will hold the advanced MLP1 qualification. Only RSA-certified staff will manage the sale and supply of liquor.
2. **Policy Communication and Update:** The RSA policies and procedures will be effectively conveyed to staff and patrons, with updates occurring regularly.
3. **Refusal of Service:** Staff will be trained to deny service to intoxicated or disruptive individuals. If a patron appears intoxicated, they will be denied service and asked to leave the premises.
4. **Protecting Minors and Others:** Strict measures will be implemented to prevent promoting or serving alcohol to minors and to ensure all patrons respect others' rights and safety.
5. **Responsible Promotion:** The Applicant will adhere to the [Director's Policy on the Responsible Promotion of Liquor](#) and will ensure any in-house promotion is suitable and does not support excessive or reckless consumption of liquor in the future.
6. **Responsible Marketing:** Advertising will be responsibly targeted at adults, with support for programs addressing alcohol misuse and promoting responsible consumption offered regularly through the venues community engagement practices and social media activities.
7. **Availability of Non-alcoholic Options:** Free drinking water and a diverse range of high-quality non-alcoholic beverages will be promoted and accessible during operational hours.
8. **Non-alcoholic and Mid-strength Drinks:** A wide variety of these beverages will be readily available and promoted during all trading hours at the Venue.

9. **Informed Choices:** All necessary information will be provided to patrons for informed choices regarding alcohol consumption.
10. **Patron Sobriety Assessment:** Staff will continuously monitor the state of patrons' sobriety, reporting any signs of intoxication to the on-duty senior manager.
11. **Intox Sweep:** Regular 'intox sweeps' will be conducted by managers to identify and manage intoxicated patrons following the Venue's documented RSA practices.

7.7 Self-imposed Trading Conditions

Demonstrating its commitment to providing a safe environment and based on harm minimisation strategies identified in previous liquor licence applications by the WA Police Liquor Enforcement Unit and the Department of Health, the Applicant is willing to adopt the following self-imposed trading conditions, if deemed necessary by the Director of Liquor Licensing:

1. A maximum purchase limit of four alcoholic drinks or one bottle of wine per patron.
2. The Venue will not serve "shots" or "shooters".
3. The Venue's outdoor bar will close 30 minutes prior to the Venue's closing time.
4. Entry to the Venue will be restricted 30 minutes prior to closing time, establishing a 'lock-out' period.

The Applicant stands ready to accept any necessary conditions attached to the licence in the public interest.

7.8 “At Risk” Groups and Individuals

The Applicant has given due consideration to those groups deemed the most “at risk” of alcohol related harm and ill-health, as identified by the [Drug and Alcohol Interagency Framework for Western Australia 2011 – 2015](#). The determined “at risk” groups relevant to the Locality may include:

- Children and Young People,
- Aboriginal people and communities,
- Families,
- Migrant groups from non-English speaking countries,

Below is a table that presents the numbers and percentages of “at risk” groups in the locality, which are relevant to this application:

ABS 2021 Census Category	Number	Locality	Greater Perth	WA
Average age	na	32	37	38
Juveniles (% of people under 18)	5547	9.9%	22.5%	22.6%
Aboriginal and Torres Strait Islander people (% of population)	501	0.9%	2.0%	3.3%
Top Ancestry Response (English)	15034	21.5%	37.0%	37.6%
Top Country of Birth (Australia)	24975	35.7%	59.5%	62.0%
Second Top Country of Birth (UK)	3812	5.2%	9.7%	8.9%
Total Overseas Born	23021	32.9%	36.0%	32.2%
Families (combined Single Parent & Couple Families with Children)	4563	17.9%	32.0%	44.6%
Couple family without children (%)	6979	23.2%	25.4%	25.4%
Household composition: single (or lone) (%)	9382	31.1%	24.0%	23.8%
Median weekly income - household	na	\$2,202	\$1,866	\$1,834

Children and Young People: The Applicant notes that according to the above-referenced ABS statistics, the number of Families (combined Single Parent Families and Couple Families with Children), at 17.9% of total households, is much lower in the defined Locality than in Greater Perth (32%) and Western Australia (30.2%). The Population under the Age of 18 is significantly lower at 9.9% compared to Greater Perth (22.5%) and Western Australia (22.6%). This indicates an area that does not have a high percentage of families with children (particularly young children), living in close proximity to the Venue.

Indigenous Population: The population data also indicate a much smaller number of Aboriginal residents in the Locality compared with Greater Perth and Western Australia. The Applicant notes that the number of Indigenous persons typically residing in the locality is only 0.89% of the total population. This is significantly lower than the Greater Perth average of 2% and the Western Australia state average of 3.30%. Nevertheless, the applicant recognises the presence of Aboriginal people in the area and submits that the proposed services and facilities will not include any specific culturally sensitive elements, and the Venue's harm reduction strategy and RSA procedures will be applied indiscriminately to all clientele in the effort to promote a safe, inclusive and healthy hospitality environment.

Regional, Rural, and Remote Community Members: The Venue is located within the metropolitan area and thus does not directly serve any regional, rural, or remote community. However, the applicant acknowledges that individuals from such areas may visit the locality, often as tourists.

Families: As mentioned above, the locality's demographic data reveal that families constitute a significantly smaller percentage of the resident community compared to Greater Perth and WA averages.

Migrant Groups from Non-English Speaking Countries: Australian Bureau of Statistics (ABS) data shows a higher-than-average concentration of migrants residing in the Locality. The applicant emphasises that the presence of migrants from non-English speaking countries is not seen as a negative factor. The venue's offerings, design, and planned programming do not contain any elements deemed culturally inappropriate or sensitive.

People in Low Socio-Economic Areas: Residents of the proposed locality generally enjoy higher-than-average incomes as indicated by the data from the Australian Bureau of Statistics (ABS). The Socio-Economic Indexes for Areas (SEIFA), which measure relative levels of socio-economic disadvantage and/or advantage based on various ABS Census characteristics, provide further insights. The City of Perth ranks 11th and the COV ranks 9th out of 138 local government areas in Western Australia on the Index of Relative Socio-economic Advantage and Disadvantage. These high rankings suggest that residents experience relatively advantageous socio-economic conditions in the Locality.

Mining Communities or Communities with High Number of Itinerant Workers: No known mining operations are occurring within the locality. Therefore, concerns related to mining communities are irrelevant for the proposed venue.

Communities Experiencing High Tourist Numbers: The locality, particularly Northbridge and the City of Perth, is noted for its attraction of a high volume of tourists. As the venue has a strong local community engagement strategy, however, the Applicant does not anticipate that at any point the majority of Clientele would be from this group. Further information regarding tourism in the relevant locality is provided in other sections of this application.

Further to this statistical analysis of 'at-risk' groups in the Locality, the applicant submits that the venue's operational strategies and planning will be such that it is unlikely to attract 'at-risk' groups to the premises due to the following features:

1. Community-centred approach and family-friendly location.
2. High-quality fit-out comprising majority seating areas, table service model for inside the venue, along with dining and function services throughout.
3. Provision of function space.
4. Unique beverage offerings intended and promoted to be consumed with food.

5. Pricing strategy that reflects the quality of products and services offered.
6. Planned entertainment (attachment 8)).
7. Outdoor areas for patrons' enjoyment.
8. Implementation of a dress code, enforcement of Venue code of conduct.

7.9 Juvenile Policy

The Applicant will consistently execute a robust juvenile policy. This incorporates preventative measures against serving alcohol to minors and ensuring that all patrons respect others' rights and safety. The Venue has been designed to cater to a diverse community, with a focus on the local community, including family groups with children. It is anticipated, however, that family groups will frequent the Venue mostly during lunch and dinner times, especially on weekends and public holidays. Unaccompanied juveniles will not be allowed in the venue, except under circumstances permitted by the Act. The policy mandates that juveniles accompanied by an adult are not permitted to stay on the premises after 9.00 pm. Proactive measures will be implemented to effectively handle any instances of juveniles attempting to access the Venue or consume alcohol.

7.10 Safety and Security

Recognising the importance of safety and security in their operations, the Applicant is committed to providing a secure environment for patrons, staff, and the local community. Ensuring the safety and security of staff, clientele, and of the locality more generally, is the Applicants highest priority in operating the venue. To maintain a low-risk profile for the venue the Applicant will integrate several safety and security features into their venue design and operations including:

1. Promoting the venue to and catering to "our target audience who would be considered to be low risk groups".
2. Offering a high-quality design and fit-out, that takes into account the latest design theory related to building safe environments and "[creating safer spaces by design](#)".
3. Emphasis on dining services, with food availability until 30 minutes prior to closing.
4. Implementing strict Responsible Service of Alcohol (RSA) policies and procedures.
5. Providing ample seating for patrons.
6. Observing the Act's regulations by not trading beyond the permitted hours.
7. Enforcing a patron dress code.
8. Leveraging the licensee's professional experience in responsible liquor industry management.
9. Committing to high-standard staff training, quality management, and comprehensive controls, policies, and procedures.

10. Maintaining security guard presence during busy periods until 30 minutes after closing on Friday and Saturday evenings.
11. Ensuring full coverage through a comprehensive CCTV system.

The features outlined above combine to ensure that the venue falls into a low risk category in terms of the risk of alcohol related problems. Building upon these features, the Applicant has developed comprehensive harm minimisation and safety and security strategies, outlined in the VMP (attachment 3) to address all potential safety and security issues and emphasise harm prevention and minimisation. This plan will be regularly reviewed and updated to stay effective. As part of their safety and security strategy, the Applicant will engage with a professional and experienced security company to provide security services as needed to support staff and maintain a safe and secure venue.

Further, the Applicant's experience as an operator within the locality complements the outlined policies and procedures, assisting in minimising and mitigating potential harm or ill-health to the locality. Cooperation with local police and emergency services is another key strategy for effectively responding to any incidents as the Applicant has done successfully in past business operations. Building and maintaining strong community connections creates a sense of safety within local neighbourhoods, strengthens passive surveillance, and provides support networks where people look after each other and stand against anti-social behaviour.

8. Neighbourhood, Community & Amenity issues (ss38(4)(b)-(c))

The Applicant recognises the importance of being a good neighbour and is committed to operating the Venue in a way that respects the rights of nearby residents and businesses. Important neighbourhood, community and amenity issues to be considered as part of this PIA have been identified under Section 38(4)(b)-(c) of the Act, which state:

"...(b) whether the amenity, quiet or good order of the locality in which the licensed premises or proposed licensed premises are, or are to be, situated might in some manner be lessened; and

(c) whether offence, annoyance, disturbance or inconvenience might be caused to people who reside or work in the vicinity of the licensed premises or proposed licensed premises;..."

The Applicant is committed to ensuring the Venue's operations do not negatively impact the tranquillity, order, and amenity of the surrounding locality. This commitment involves implementing measures to mitigate disturbances, such as managing noise levels and ensuring patrons depart the Venue without disrupting the local community.

The Venue Management Plan and Noise Management Plan attached to this submission outline the applicant's comprehensive approach to mitigating any potential impacts on the local amenity. These plans will be regularly reviewed and updated. This will include promoting and maintaining a complaints procedure for neighbours to report any issues, so that the Applicant can respond promptly and effectively to any complaints.

The Applicant plans for the Venue to focus genuinely on meeting the requirements of local residents as well as those travelling to the locality for their unique and diverse cultural programming, their high-quality food and beverage offerings, and unique opportunities to engage in community-centred experiences that are not available elsewhere in the locality. In fact, the Applicant has placed the concept of supporting and engaging the local community, and enhancing the amenity of the locality at the centre of its business model and organisational values.

8.1 Risk Assessment & Profile

The Venue is categorised as low risk concerning potential disturbances and amenity. This assessment is based on the following considerations:

- **Location:** The Venue is in a mixed-use, inner city location, in proximity to several licensed and unlicensed hospitality venues, which contributes to a significant level of ambient noise, especially at night and on weekends.
- **Proximity to Sensitive Areas:** It is located where a limited number of noise-sensitive premises are present.
- **Trading Hours:** It operates within normal permitted hours for a tavern.
- **Operation Style:** The Venue maintains a low-intensity operation focusing on dining services and local cultural programming with ample seating.
- **Target Audience:** The Venue caters to a broad range of patrons, including mature individuals and families with children, minimising the likelihood of attracting 'at risk' public in terms of irresponsible alcohol consumption or antisocial behaviour.
- **Experienced Management:** The Venue is run by seasoned hospitality professionals with a record of responsible and lawful operations in WA.
- **Management Controls:** The Venue implements a comprehensive range of management controls and risk mitigation measures with a strong emphasis on Responsible Service of Alcohol (RSA) principles.

8.2 Positive Impact on the Locality

Introducing the Venue to the Locality aligns with the objectives of the local government authority and enhances the Locality's amenity in terms of:

1. Enhancing the quality and diversity of leisure and hospitality amenities and facilities.
2. Physical hub for a growing culture of online music engagement and participation.

3. Improving the liveability and quality of life for people living, working, and visiting the Locality.
4. Support active streets and the presence of outdoor dining and seating.
5. Supporting the William Street Town Centre to be safe, attractive and pedestrian focused while increasing foot traffic between southern and northern end of William Street.
6. Increase green space, tree canopy cover and sustainable business practices in the Locality.
7. Through it's cultural programming focus, local community engagement and experience based offerings.

8.2.1 Support active streets and the presence of outdoor dining and seating.

The Applicant submits that the Venue will provide over 150 outdoor seats for clientele throughout the venue's outdoor areas, as well as a large 54m² grassed area designed for sitting under the venue's shady trees.

Access from the street will be open and free flowing during venue operations, except where entrance check points are needed to be implemented to maintain safety and security requirements on peak trade evenings.

At the front of the Venue, 34 secure bike parking bays will be provided within the lockable area of the venue, under staff supervision during operating hours, providing bicycle parking for both clientele and staff bicycles, e-bikes / e-scooters. An electric bike and scooter charging point will also be installed at this location to encourage active transport choices for all who attend or work at the Venue.

The Venue's large front alfresco area and outdoor garden bar is a surprisingly rare feature in the locality, and provides a highly sought-after facility for hosting outdoor social occasions, larger gatherings and functions.

8.2.2 Supporting The William Street Town Centre to be Safe, Attractive and Pedestrian Focused.

The venue is conveniently located within a 5-minute walk from North Metropolitan TAFE, a 10-minute walk from the new Perth City campus of Edith Cowan University and the Western Australian Academy of Performing Arts (currently under construction), including their school of music, and a 15-minute walk from SAE Creative Media Institute. At the outer edge of the expanding central urban precinct, the Venue sits perfectly positioned to draw together a local customer base seeking creative engagement and connection in their area.

As outlined in [8.2.1](#) the venue will have 34 secure bike, ebike and escooter parking bays

with an electric charging point installed to encourage active transport choices for all who attend or work at the Venue. Further information about ease of access to the Premises and availability of public and active transport routes to the Premises is provided in section [8.3 Ease of Access](#).

8.2.3 Increase Green Space, Tree Canopy Cover And Sustainable Practices:

As stated in the COV [Greening Plan 2018-2023](#) Increasing green space and tree canopy in the locality will provide the following benefits:

- more liveable neighbourhoods;
- enhanced community well-being;
- removal of atmospheric carbon to counteract human-induced climate change;
- mitigation of the urban heat island effect;
- increased biodiversity;
- improved air quality and overall environmental health;

In line with the COV [Greening Plan 2018-2023](#) and the [Sustainable Environment Strategy 2019-2024](#), the Venue has been intentionally designed to provide increased green space, tree canopy cover, and champion sustainable design and practices.

With two mature trees providing a significant level of tree canopy cover, and an expansive grass area designed for patrons to enjoy, the Venue's outdoor garden bar and dining area offers a shady natural retreat from the harsh urban surrounds. This initiative aligns with the City of Vincent's target to achieve tree canopy cover of 12% on privately owned land by 2050.

The benefits of this alignment include:

1. **More Livable Neighbourhoods:** By contributing to the "Greening the town centres" initiative, the Venue enhances the overall amenity of William Street town centres.
2. **Enhanced Community Well-being:** The garden provides a place of relief and escape from the urban environment, resonating with the community's desire for more environmentally friendly spaces and safe local environments.
3. **Climate Change Mitigation:** As noted in COV documents, "Plant species such as trees and large shrubs act as carbon sinks." The Venue's tree canopy aids in removing atmospheric carbon and mitigating human-induced climate change.
4. **Urban Heat Island Effect Mitigation:** The Venue plays a role in "reducing the urban heat island effect through trees and other vegetation, thereby improving air quality and overall environmental health".
5. **Increased Biodiversity:** The garden promotes biodiversity, aligning with COV's goal to "enhance habitat and promote biodiversity" through diversified vegetation.

6. **Community Involvement and Awareness:** By "inviting and supporting community involvement in greening activities," the Venue also becomes a focal point for educating the community about the social and environmental benefits of trees and green spaces.

The Venue's garden is not just a leisure spot; it's an example of eco-conscious community building in the Locality. The incorporation of a water tank and rainwater system for the garden demonstrates sustainable water practices. The Venue's commitment to integrating green spaces and sustainable practices makes it a progressive model for urban development in the locality. By aligning with the City's strategic goals and values, it fosters a vibrant social environment that nurtures not only the human inhabitants but the ecology of the locality. As the COV [Greening Plan 2018–2023](#) and the [Sustainable Environment Strategy 2019-2024](#) suggest, such venues that prioritise green spaces, recycling initiatives, and sustainable materials not only enhance the local environment and support human health and wellbeing but set a precedent for future urban planning and development.

8.2.4 Risk Mitigation Strategies

In addition to the low risk of the Venue causing disturbance, the Applicant has been proactive in minimising potential negative impacts through thoughtful design of the premises and development of appropriate operating policies and procedures, such as:

- Noise Management: Implementing a stringent noise management plan.
- Adherence to RSA: Strict compliance with RSA guidelines.
- Patron Egress: Ensuring an orderly departure of patrons.
- Waste Management: Effective collection, internal handling, and removal of glass and general rubbish.
- Communication & Complaints: Establishing resident communication, liaison, and complaint procedures.
- Closing Procedures: Implementing meticulous closing procedures.

8.2.5 Venue Noise Management Plan

Please refer to the Venue's Noise Management Plan (attachment 4).

8.2.6 Architectural & Design Measures for Noise Mitigation

The Applicant has consulted with Acoustic Consultants Australia (ACA) and project architect Peter Cole to develop the Venue's architectural plans with specific measures to mitigate noise, such as:

- **Sound Barrier:** Construction of a 5 - 6m tall sound barrier wall along the eastern and northern perimeters of the premises.

- **Acoustic Roofs:** Installation of acoustically rated roof structures over busy or potentially noisy areas to prevent noise from escaping the Venue.
- **Strategic Positioning:** Placement of toilet blocks, outdoor bar structures, Kitchen, and indoor bar at strategic locations to act as additional noise barriers.

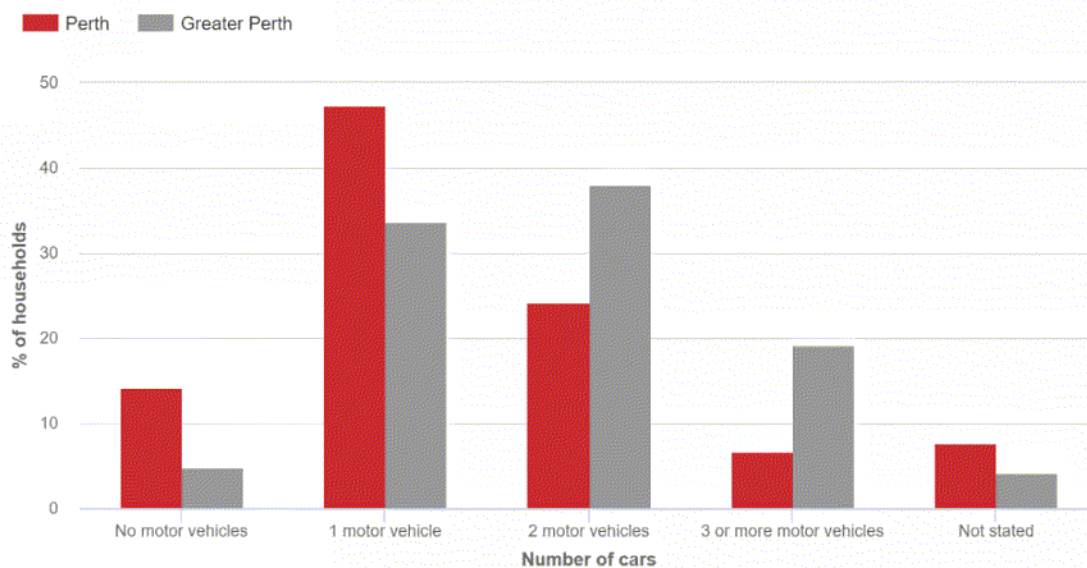
The Applicant is dedicated to operating the Venue as a positive addition to the local community, implementing recommendations from the Acoustic Impact Report, and adhering to all operational conditions imposed by the LGA to minimise potential disturbances and ensure a pleasant environment for all.

8.3 Ease of access

The premises will be easily accessed on foot and on bicycle, scooter or other forms of active and public transport by local residents, students and business employees in the neighbourhood. The Applicant submits that the Venue will be a locally focused and frequented premises that will be considered to be a neighbourhood venue by those living in and close to the Locality, catering to residents within the Locality who are more likely to walk, ride or use public transport to travel to the venue than to drive a car. 2021 census data indicates that people residing in the Northbridge area are over 5 times more likely to ride to work and 10 times more likely to walk to work. 2021 ABS Census data on car ownership and preferred methods for travelling to work for the suburbs in the 2km radius surrounding the venue show a strong trend towards residents living close by to the venue preferring active and public modes of transport over motor vehicles in their choice of mode of transport to work. Residents in this area are:

- over 3 times more likely to live in a household without a car (16% of all households) compared to the average respondent in the Greater Perth area (4.8%).
- over 3 times more likely to cycle to work (1.8% of all employed residents) compared to the average respondent in the Greater Perth area (0.6%).
- over 7 times more likely to walk to work (11.7% of all employed residents) compared to the average respondent in the Greater Perth area (2%).
- more than twice as likely to take public transport to work (19.4% of all employed residents) compared to the average respondent in the Greater Perth area (8.4%).

Car ownership, 2021



Source: Australian Bureau of Statistics, Census of Population and Housing, 2021 (Enumerated data). Compiled and presented in profile.id by .id (informed decisions).

.id informed decisions

Looking only at the areas closest to the venue shows these trends become significantly stronger as the distance to the venue decreases. For example; residents in the neighbouring suburb of Northbridge are over 14 times more likely to choose to walk to their place of employment as their primary mode of transport (23.7% of residents), compared to the average respondent in the Greater Perth area (1.6%); or residents in the vicinity of COV's "Northbridge Town Centre" are over 4 times more likely to choose to cycle to their place of employment as their primary mode of transport (2.6% of residents), compared to the average respondent in the Greater Perth area (0.6%).

Considering the statistics outlined above, the Applicant highlights the following features of the locality in regard to the ease of access to the venue for local residents;

- 1. Transportation Preferences:** The residents of the locality are less reliant on cars than those in the broader region of Western Australia and Greater Perth. A higher percentage of residents walk, cycle, or take public transit, indicating a preference for more sustainable modes of transportation.
- 2. Car Ownership:** The households in the locality are less likely to own multiple vehicles, with a large majority having just one or no vehicle. This could be due to a combination of factors such as smaller household sizes (as indicated by the large number of couple households), closer proximity to workplace and other places visited frequently, high costs associated with car ownership, and convenient access to public transportation.
- 3. Sustainable Lifestyle:** The residents' transportation habits indicate a tendency towards a more sustainable lifestyle, with lower dependency on private vehicles. This will have positive implications for the locality in terms of reduced traffic

congestion, lesser demand on parking facilities, lower carbon emissions, and improved air quality.

4. **Urban Development:** The high percentage of walking and public transportation usage, as well as lower car ownership, reflects the character of the locality as high-density residential with mixed-use zoning that places workplaces, amenities, and services within walking or biking distance of homes.

Furthermore, several features of the venue can be shown to further support alternative, active and public transportation choices for the Venue's clientele and align with COV strategic goals around enhanced neighbourhood connectivity, sustainable transport and healthy and active residents:

1. A Transperth bus stop for route 680 is situated directly at the front of the premises, providing a highly convenient access point for those arriving to or leaving the premises by public transport.
2. COV has identified William Street as one of its endorsed cycle routes as a "primary route" and a priority cycleway for scheduled improvements in their "Bike Plan" strategy document. As stated in the document;

"Bike riding, along with walking, is the most sustainable form of transport, contributing to creating great places, cleaner local environments, healthier lifestyles and providing economic benefits to local neighbourhoods. The City wants to increase the number of people riding and walking to work from 15% of residents in 2020 to 17% of residents by 2025 and 20% of residents by 2030. The City wants bike riding and walking to be the preferred mode of transport for short trips to, from and within the city and a viable, safe and efficient option for longer trips. By encouraging more people to choose bike riding and walking for everyday trips, we will improve neighbourhoods and the environment, reduce car use and traffic congestion and improve the general health of our community"

3. The venue will ensure secure bicycle parking, in excess of LGA requirements, will be available on the premises during operational hours in the secure front area of the Venue; and end-of-trip facilities will be installed for employees of the venue to encourage active transport choices.
4. The venue is also conveniently located within a 5-minute walk from North Metropolitan TAFE, a 10-minute walk from the new Perth City campus of Edith Cowan University and the Western Australian Academy of Performing Arts (currently under construction), including their school of music, and a 15-minute walk from SAE Creative Media Institute. Staff and students from these important creative and educational institutions within the locality are considered to be part of the Venue's primary target audience group alongside local residents and employees.

5. Marketing and promotional activities will be used to specifically target and promote the venue to local residents such as “locals night” food specials, local letter drops with special offers and of course the venue’s locally focused cultural programming.

For the visiting clientele who travel from further afield to enjoy the range of experiences and cultural programming at the Venue;

- Ample street parking will be available in the vicinity on William Street, Monger Street, Forbes Road, Newcastle Street and Brisbane Street, as well as designated car parks operated both privately and by the COV that are in close proximity. (for details, see attachment 10 - Traffic Impact Statement)
- There is convenient access to Perth Train Station by walking down William Street; only 850m away and less than 15 minutes walk.
- A 5min walk to the closest Beaufort Street bus stop, provides access to additional bus routes 66, 67, 68 & 950, which run consistently across the venue's planned opening hours.
- Uber services are becoming increasingly popular as a means of travelling to the northbridge area and Perth CBD, with users of the rideshare app doubling in Australia between 2016 and 2020. Uber is most popular in Western Australia, with [25.7 per cent of the state’s population using the app](#). It is also important to note that the target demographic for the Venue, and the dominant age group in the locality, is known to be the most prolific and fastest growing user group for rideshare apps such as Uber and others, where the 35 to 49 age group saw a 142 per cent user increase between 2016 and 2020, and 75 per cent more users between 18 and 24 [were reported](#).

8.4 Minimisation of Offence, annoyance, disturbance or inconvenience - s38(4)(c)

In determining this application the licensing authority may consider if any offence, annoyance, disturbance or inconvenience might be caused to people who reside or work in the vicinity of the proposed licensed premises, as set out in section 38 (4) (c) of the Act. The following passages will outline the Applicants commitment and planning towards preventing any potential offence, disturbance or inconvenience to people in the locality of the Venue. The Applicant proposes to implement the following strategies to minimise any potential, undue offence, disturbance or inconvenience that may occur, should the application be approved in the public interest:

1. The licensed premises will be supervised by an appropriate number of staff at all times.
2. Tried, tested and proven operational policies from the Applicant’s previous operational experience form the foundation for the Venue management

planning and procedures to be implemented.

3. Intoxicated patrons will not be permitted entry at any time.
4. The premise will be refurbished to a high standard to attract the demographic that the Applicant is seeking to attract.
5. The business is operated by mature, skilled and experienced staff that will offer customers a high level of customer service.
6. Non-alcohol and Low alcohol options will be available within the premises.
7. Packaged liquor products will not be promoted or sold at cheap or discounted prices so that they are not attractive to "at risk" consumers or encourage excessive consumption.
8. the Applicant will not use advertising or promotional material that is deemed attractive to young people.
9. Refresher training will be conducted at regular intervals to ensure staff are aware of their rights and responsibilities in relation to RSA and customer service, given the higher risk, Tavern Licence.

9. Community & Stakeholder Consultation

The Applicant will actively engage with the local community to understand their interests, values and concerns and to work cooperatively to address any issues that may arise. All consultation processes associated with the development application shall be carried out in accordance with the [City's Policy No. 4.1.5 - "Community Consultation"](#).

9.1 Local Authority and Stakeholder Consultation

As a first step in this process, in April 2023, the Applicant delivered an information letter to all adjoining properties with an invitation to attend a community information session at the venue. The information session was held at the venue on April 13th 2023, where Director Ben Taaffe and Method Planning Consultant Trent Durward attended to meet neighbouring business owners or representatives, discuss the proposed venue and answer questions.

The next stage of the Applicants Community Consultation process, involved contacting key organisations to discuss the Tavern Licence application. The following authority organisations were invited to comment and provide feedback with concerns (if any) in relation to the application:

WA Police Liquor Enforcement Unit (LEU)

An email outlining the details of the application was sent to the LEU, stating that the Applicant would appreciate any feedback the LEU could provide. The Applicant respects that the LEU will consider the application in its entirety when advertised and intervene if required. The Applicant notes that any communication they have with the LEU does not constitute support for this application; however, the LEU did support the Applicant's proposed conditions.

Perth Police Station

The Applicant's representative called the Perth Police Station and spoke with an officer who advised that the LEU would review the application in full if required and provide written feedback to be considered prior to submission to RGL.

9.2 Ongoing Consultation Plan

During the next stages of business development, pending and subject to approval, the Applicant is committed to and will continue to engage with stakeholders, local authorities and community members to conduct further community consultation practices and engage with the local community.

These practices will include, but are not limited to;

1. Notify and consult with local COV ranger team members.
2. Call out to industry and community members for letters of support.
3. Customer & community survey.
4. Launching online radio operations - call out for participants.
5. Launching Passenger website with online streaming.

9.3 Advertising

The Applicant also notes that a period of advertising will be undertaken once the dates for the specified period are determined by the Department of Local Government, Sport and Cultural Industries. As part of this process a Notice of Application will be mailed to the following schools, hospitals, hospices, aged care facilities, drug and alcohol treatment centres, short term accommodation or refuges for young people, childcare centres, churches, local government authority, and local police station, situated in the specified locality of the premises.

- Royal Perth Hospital on Wellington Street, East Perth.
- Chua Chanh Giac Temple on Money street, Perth Mosque on William Street and Greek Orthodox Cathedral of Saints Constantine and Helen, at the corner of Francis and Parker Streets.
- The Learning Sanctuary Kings Square, The Akidamy School of Early Learning and Buttercups Childcare & Early Learning Centre, Northbridge.

- Highgate Primary School on Lincoln Street St Georges Anglican Grammar School on William Street, Perth.
- St John of God Social Outreach on Wellington Street, Perth; Peer Based Harm Reduction WA; and WANADA, Perth.
- Bethanie On Pier, Aegis Lincoln Park, KinCare, and Chung Wah Community Care - Aged care facilities.
- Perth Police Station, Fitzgerald street.
- The Salvation Army Perth Fortress, William street, The Beacon, Aberdeen street, Northbridge.

The commencement of advertising activities will coincide with the Department's determination of dates and continue for the specified duration. This Public Interest Assessment submission for the Venue will be made available for public inspection as part of the advertising period. The advertising initiatives will include, but are not limited to:

- A formal Notice of Application to be delivered to all residents and commercial properties within a 200-metre radius of the venue;
- A Notice of Application to be forwarded to any local Aboriginal communities as well as the regional office of the Department of Indigenous Affairs; and
- A Notice of Application will also be sent to all of the addresses listed above.

The advertising initiatives as outlined above will satisfy the requirements for advertising as specified by the Department of Local Government, Sport and Cultural Industries.

10. Positive recreational, cultural, employment and tourism benefits - s38(4)(ca)

The Applicant submits that the grant of the Development Approval by COV and then, subsequently (and subject to approval), the grant of the Tavern Restricted Licence for the Venue by DLGSC will be in the future public interest and provide numerous positive amenity impacts in the Locality as it will;

1. stimulate and support increased cultural and economic activity in the area;
2. improve the quality and variety of leisure and hospitality facilities and services available in the Locality;
3. improve the liveability of the Locality and the quality of life of persons living in, working in and visiting the Locality;

4. serve to fill a noticeable gap in the local hospitality and entertainment market by providing an online media facility connected to an accessible and inclusive space that prioritises and supports the social dimensions of the local music and arts communities;
5. place practices of community engagement and development at the centre of the Venue business model, as evidenced by cultural programming and online media facility that is included in the venue design and program planning;
6. support and celebrate the development of local culture and shared identity through the proposed facilities and programming;
7. provide a large and accessible outdoor green space with generous garden areas, shady trees (providing additional tree canopy cover in the area), and numerous native plant species - designed for the local community to enjoy a connection to nature and find reprieve from the urban surroundings and digital overload often experienced in modern life; and
8. contribute to revitalising the greater Perth CBD area and William Street Town Centre, and the continuing development of the locality and the Northbridge neighbourhood in particular as “Thriving Places”

The introduction of the Venue into the Locality will be consistent with the objectives of COV and State Government WA as outlined in their strategy and development guidelines documents - [State Planning Policy 4.2 - Activity Centres \(2023\)](#), [COV Strategic Community Plan 2018–2028](#), the more recent [Strategic Community Plan 2022-2032](#) and the COV [Thriving Places Strategy 2023-2028](#). The COV Thriving Places Strategy document states:

[“The City identifies Thriving Places as a key priority in its Strategic Community Plan. This is supported by outcomes that we will work towards such as being recognised as a City that supports local and small business”](#)

In this document, “thriving places” are defined as:

- *local and small businesses that are supported;*
- *town centres and gathering spaces that are safe, easy to use, and attractive places where pedestrians have priority;*
- *public and private spaces that host innovative businesses, social enterprises*
- *and imaginative uses;*
- *assets in the public realm that are efficiently managed and maintained; and*
- *art, history, and our community’s living cultures that are evident in the public realm.*

The applicant submits that it will contribute positively to each of the key characteristics of “thriving places” as outlined above, through the combination of services, events, experiences and amenities provided at the Venue.

The Applicant submits that the intersection of community engagement, creative

participation and local cultural production and consumption with modern hospitality facilities and services, which include the service and consumption of liquor, embodies the proper development of the hospitality industry and will be in the public interest. Bringing these elements together not only enhances the locality and the overall experience of the clientele, but also promotes an atmosphere of cultural exchange and social connection that can lead to a significantly improved societal well-being and social cohesion.

By bringing together the digital spaces of content creation with a physical hub of localised cultural activity, the Venue will provide opportunities for engagement and participation that blur the lines between online and "in-real-life" modes of participation. An extensive schedule of community-focused events, such as art, craft and plant markets, exhibitions, fundraisers, workshops, and DJ performances in the listening lounge is aimed at promoting local creative talent, fostering community engagement, and creating shared cultural experiences. This unique blend of new media-technologies, local culture and modern hospitality encourages the expression and progression of local cultural identity, providing a platform for diverse voices and experiences. The Venue, therefore, represents more than just a hospitality business - it seeks to contribute to the fabric of a vibrant, inclusive, and culturally rich community.

10.1 Economic Contribution

Hospitality projects, particularly unique and well-planned establishments like the Venue, offer numerous benefits to local economies, contributing significantly to urban development. They stimulate economic growth, create diverse employment opportunities, support local suppliers, enhance cultural vibrancy, and foster environmental sustainability. Therefore, such projects should be encouraged and supported by urban development policies and planning initiatives.

By adding a unique and popular venue to the area, the Venue will contribute to an expanded array of shopping, dining, and drinking options, and, therefore, will benefit the local economy. With its strategic location at the heart of Northbridge town centre, the Venue will serve as a vital connector between other prominent hospitality businesses within the City of Perth and the City of Vincent, drawing more people into the Locality directly from neighbouring town centres and suburbs.

Moreover, the Venue's proximity to several educational institutions such as North Metropolitan TAFE, SAE Creative Media Institute, and the future Perth City campus of Edith Cowan University and the Western Australian Academy of Performing Arts slated for 2026, puts it in a strong position to attract a growing local customer base as student numbers in the area increase.

Key areas of the positive economic impact that the Venue will create can be

summarised as follows.

10.1.1 Job Creation and Employment Diversity:

Hospitality projects contribute directly to local employment, both during the construction phase and ongoing operations. The Venue is projected to create over 20 construction jobs during the period of construction and then 50-75 ongoing roles and contracts, offering opportunities for a diverse range of professionals - from management staff to musicians, DJs, chefs, bartenders, wait staff and a range of hospitality service contractors.

The Applicant also notes that [research conducted](#) by DLGSC on the Western Australian creative economy show that between 2011 and 2016 employment opportunities in the creative industries in WA experienced an average annual growth rate of 0.5% per annum, which is lower than the general employment growth rate of 1.5% in the same period. Unfortunately, in recent years, the growth rate for employment in the creative industries declined, with a decrease of -3.8% pa from 2017 to 2018. This negative growth in the creative sector contrasts with the broader employment growth in WA, which has maintained an average growth rate of about 0.9% per year. Nor does the WA trend mirror what is happening nationally, where employment in the creative industries across Australia has shown consistent growth, outpacing the overall national employment growth rate.

This data demonstrates a need for greater support for creative industry businesses and businesses that provide opportunities for those working in the creative industries. The Applicant submits that a significant percentage of the employment opportunities that will be created through the Venue's operations will be such creative industry roles in music, media, and various other areas.

10.1.2 Revenue Generation:

The Venue is projected to generate well over \$1 million in annual revenue for the local economy through its various components and services, which is a substantial contribution to the area's economic growth. The Venue's business model aligns with a renewed cultural interest in "the local" that is driving growth in creative and cultural economies around local food, wine, music, arts and design. The Venue's target market includes young musicians, creatives, students, and professionals that are seeking innovative hospitality services, local cultural experiences, and sustainable business practices. Catering to this unmet demand adds to the economic potential of the Venue, generating new revenue streams and making it an essential asset to the community and contributing to the overall economic development of the area.

Statistics from the 2021 Census show that, over a five-year period, [WA's creative industries grew by only 0.7% per year compared to a national average of 2.2%](#). This suggests that WA may be lagging behind the rest of Australia, and the world, in the growth of and investment into its creative economy, while many other places are enjoying an advancement in this area where in some cases the creative economy is outstripping other industry segments over the last decade. Within this context, a new venue that seeks to promote and engage creative industries and local culture represents an important opportunity to promote growth in WA's creative industries, and strengthen the local economy.

10.1.3 Support for Local Suppliers:

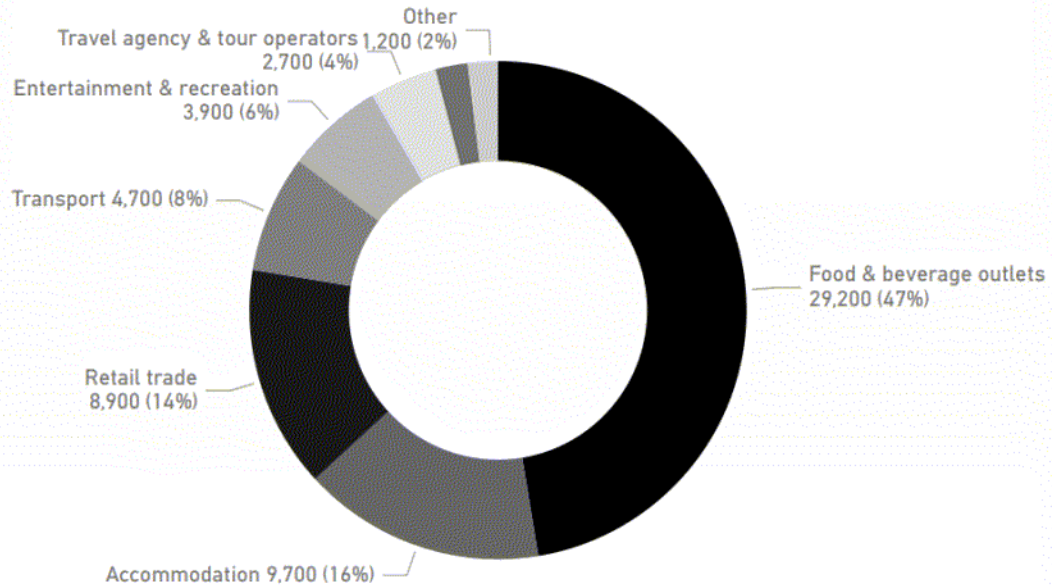
The Applicant is committed to showcasing locally sourced produce and industry, such as wine, beer, fresh produce and other food items. Through this commitment the venue will stimulate the local economy and in particular seek to support small-scale, family-owned wineries, independent brewers, and local food suppliers. This type of increased local economic activity also has a knock-on effect, strengthening the broader supply chains.

10.1.4 Boost to Tourism and Related Industries:

In addition to the local clientele attracted to the Venue, a significant number of travellers and visitors from outside the immediate area are also expected to be drawn to the locality to attend the Venue. While demonstrating a direct boost to the WA tourism industry, an increase in visitors to the area also indirectly boosts other sectors such as arts and entertainment, transportation, retail, and accommodation. In addition to direct and indirect boosts to the tourism industry, the venue will also contribute to a growing awareness of the identity of Northbridge around the world through its locally focused online radio content. The applicant submits that these benefits contribute significantly to boost the local and WA tourism industry demonstrating a positive effect for the community more broadly in relation to Section 38(4)(ca) of the Act - "Tourism, Cultural and Community Matters".

Tourism is a major contributor to the Western Australian economy and employment in WA. Tourism WA has published data that shows a 23% increase in tourism industry jobs over the 21-22 period, with the majority of tourism jobs in Western Australia in the 'Food and Beverage' (46%), 'Retail' (14%), and 'Accommodation (16%)' industries, and with 'Recreation and Entertainment' contributing to 6% of tourism jobs.

Direct Tourism Filled Jobs by Industry



A recent Tourism WA publication [Perth Entertainment Precincts Research 2022 | Final Report](#) provides contemporary data and insights into WA consumers' wants, needs and perceptions of entertainment precincts in Perth. The publication reported that public perception of the Northbridge area has declined in comparison to other Entertainment precincts, with Fremantle remaining as the most favoured entertainment precinct in Perth.

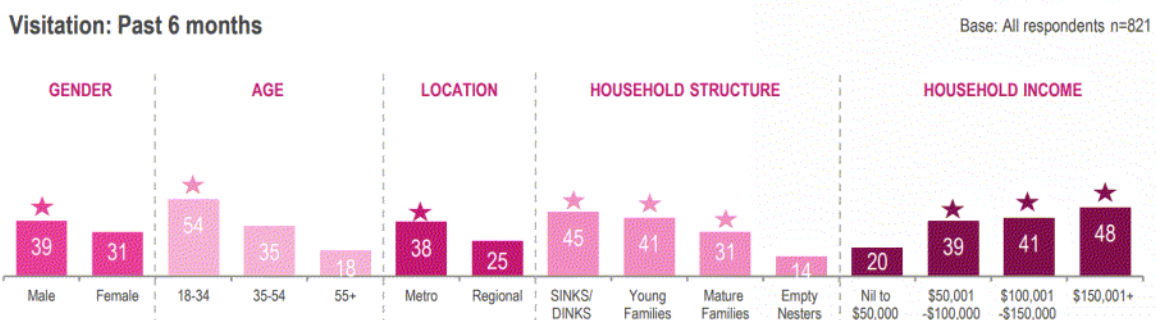
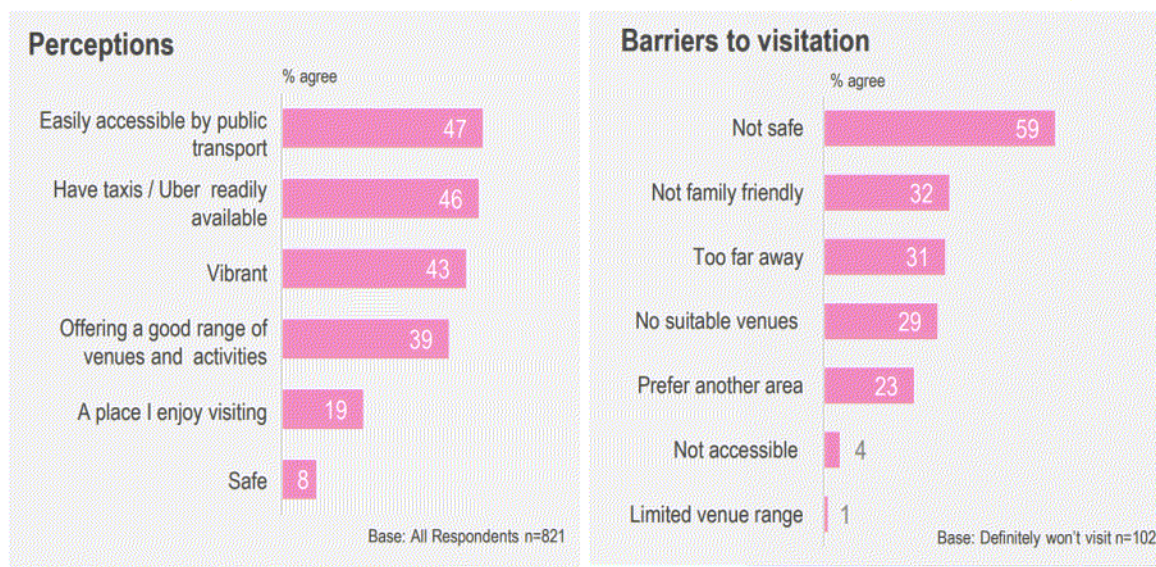
["Since 2017, the order of preference has changed - with Scarborough increasing in favour the most and Northbridge falling most considerably"](#)

According to this report, Northbridge is considered by community members as...

"the least enjoyable and least safe precinct to visit. However despite this, Northbridge is still the 4th most visited precinct in Perth"

The following snapshot of visitation statistics and community perception taken from the same report is submitted to highlight the importance of supporting new hospitality businesses in the locality that will pro-actively work to improve public safety and community perception of the locality, as is presented in [section 7](#) and [section 10](#) of this PIA in relation to the impact of The Venue on the Locality.

Figure 9 - [Visitation and Visitor Perception Snap Shot - Northbridge.](#)



The Venue will offer a welcoming and safe environment that promotes and progresses local cultural activity, providing a range of amenities required in the locality and unique to the premises, thereby aiding in the development of the liquor, tourism, creative, cultural and hospitality industries in the state. A dynamic new venue that hosts a range of modern facilities and promotes local culture, community and creative economic activity will provide a significant benefit to tourism visitors to the State. With its unique combination of offerings, the Venue is poised to fill a noticeable gap in the local hospitality and entertainment market, providing fresh and unique experiences for locals and visitors.

10.2 Cultural and Community Matters!

The Applicant submits proudly that culture and community matters! In this section, the Applicant aims to demonstrate how its unique approach to community engagement and cultural programming, social connection, and civic space can significantly benefit the local community. The Venue engagement strategy innovatively integrates digital and physical community engagement, cultural practices, and modern hospitality to create a dynamic and inclusive atmosphere that enhances the locality.

10.2.1 Unique Cultural Programming

The Venue's cultural programming is designed to celebrate local culture, engage with diverse communities and foster the thriving music and arts landscape in the area. The unique programming approach will include activations, workshops, special events, broadcasts, DJ performances in the listening lounge, live-streaming and exhibitions. A schedule of community-focused events and partnerships with local businesses will support the growth of local enterprises while enriching our offerings for clientele. These offerings are designed to celebrate local culture, engage with diverse communities, and promote the thriving music and arts landscape in the area. This strategy aims to create a civic space where modern forms of digital cultural consumption and production intersect harmoniously with physical and social connection, fostering both online and "in-real-life" creative communities. It fulfils a growing need for public spaces that foster local creative communities (both online and "in-real-life"), offering a hub that caters to the residents of the Locality and like-minded visitors seeking out the unique experiences the venue has to offer. Our programming facilitates the transition from isolated, digital cultural consumption to a vibrant social atmosphere, helping to counter the rising trend of individualised cultural consumption. See attachment 8 - Example of Cultural Programming & Entertainment Schedule.

Director Ben Taaffe has an impressive track record of producing engaged and diverse community and public programming through previous roles held at WA Museum Boola Bardip, YMCA HQ, Leederville and The ARTRAGE Bakery Complex, Northbridge; and contracts with Perth Festival. The Venue's cultural programming and engagement activities will be led by Mr Taaffe and engage his extensive network of community and creative industry professionals and practitioners. Through over 15 years presenting and volunteering at Perth's long standing community FM broadcast radio station RTRFM, Mr Taaffe also possesses a wealth of knowledge about and experience in the community radio sector, with a deep understanding of the social and cultural benefits related to this area of the Venue's programming. The cultural programming and engagement activities will focus on local stories, unique experiences and diverse content that represent the locality and the local community. As previously stated, the cultural programming and experiences that will be developed at the Venue will be undertaken in

conversation with the local community, in collaboration with leading local creatives, and specifically selected for their unique characteristics.

10.2.2 Community Engagement, Connection & Civic Space

The [City of Vincent's Strategic Community Plan 2018-2028](#) recognises the importance of providing space for social interactions, which contributes significantly to the quality of life and economic vitality of the area. The Applicant has identified the lack of such space in the locality, in terms of social space or meeting places that centre around local creative and cultural identity. While some venues in the locality support creative industry and local culture through focusing on live performance programming, the Venue prioritises the provision of social space and community based programming.

The Venue will include generous outdoor seating areas, with long shared tables, a large open green space area featuring shady trees and a grass area for sitting under the trees. The Venue design has maintained an open and accessible approach to the space, especially along the William Street facing side of the premises, inviting clientele in from the surrounding streetscape to facilitate community engagement and to serve as a welcoming social and civic space in the locality. Regular, informal and ongoing community engagement and consultation will occur in these spaces, through the Venue's cultural programming and online radio facility, and through the day to day engagement of staff and management with clientele, community members and local stakeholders.

10.2.3 Online and I.R.L. (In Real Life) Engagement

Through the media streaming facility and the associated online media platform and content that will be central to the Venue's cultural programming plan, the Venue will also engage with an online community that overlaps with and extends from the local community. Engaging with online communities allows the venue to promote, support and evolve local culture and identity through online / digital platforms, while participating in national and worldwide conversations.

This can be understood as a new layer of community engagement that connects clientele's digital and online cultural activities with their physical activity in the locality. The Venue's cultural programming aims to build upon these online activities to add an important element of social, physical and local engagement that emphasises direct, unmediated interpersonal connections over virtual interactions - offering a place to escape from the dominance of algorithmic digital engagement and the so-called *attention wars*, and supporting clientele to re-focus cultural activity towards physical and social connection.

The intention is to promote an alternative to the rising trend of isolated and individualised cultural consumption by nurturing a vibrant social environment within the Venue. In this way the Venue may serve to fulfil a growing need for the type of

public or civic space where social connection and community building can take place around modern forms of digital cultural consumption and production that occur predominantly online.

10.2.4 Internet “Radio” and live-streaming

In the digital era where online and virtual interactions pervade, the Venue's integrated online streaming and digital media facility serves as a bridge between the digital and physical worlds. It creates a pathway to reinforce local culture, stimulate the creative economy, and foster social connection. Around the world, internet radio platforms and the physical hubs from which they operate are emerging as a positive new form of community based media that create diverse and enriching cultural content, and opportunities for participants and audiences to connect and build community ties. Their flexibility, creativity, and community orientation make them a significant force in modern media, promoting cultural diversity, supporting local talent, and offering innovative listener experiences.

Drawing on the Applicants history of involvement in local community radio and engagement with global online “internet radio” stations for over two decades, the Venue will host a dynamic and evolving schedule of live-streaming online media content produced from the media facility on site. Several prominent examples of “internet radio” stations from around the world have been studied carefully by the Applicant to inform the cultural programming strategy for the Venue, including the recent launch of Freo-Cast in South Fremantle.

- [Sky Lab](#) - Melbourne
- [NTS Radio](#) - London
- [Dublab Radio](#) - LA
- [KEXP](#) - Seattle
- [The Lot Radio](#) - New York
- [Freo-Cast](#) - Fremantle

The applicant submits that the emergent trend in local internet media hubs is a positive development driven by increasing community demand for such facilities, and the opportunities they present for community members to engage and connect while celebrating their diverse local identities.

This section describes the unique features and benefits that a volunteer-based online community radio facility can provide to the local community:

Enhancing Local Culture and Diversity

- **Celebrating Local Identity:** the online media produced at the venue will be instrumental in highlighting local stories, traditions, and arts, reflecting the unique character of the community.

- **Building Community Ties:** Through programming that resonates with local interests, and engages leaders from within local creative communities, the radio programming and digital content will nurture a sense of pride and belonging, in association with the locality.
- **Expanding Cultural Reach:** Internet radio and live streaming allow the Venue to share local culture with a broader audience, promoting understanding and appreciation. Produced onsite, digital content representing local culture and community can be accessed across different localities, cities and even countries, connecting audiences worldwide.
- **Integrating Digital and Physical Worlds:** They bridge online cultural activities with physical presence, fostering a holistic community experience.
- **Eclectic Programming:** The online radio programming will feature diverse music genres, showcasing a wide variety of artists, both known and unknown.

Stimulating Creative Economy

- **Supporting Local Talent:** By providing a platform for local musicians, artists, and creators, internet radio contributes to the growth of the creative economy within the locality.
- **Encouraging Collaboration:** The media streaming facility and programming fosters partnerships between artists and businesses, driving cultural innovation.
- **Empowering Independent Artists:** These platforms offer opportunities for local and underground artists, driving economic activity and fostering artistic entrepreneurship. The Venue will provide opportunities for new and emerging artists, giving them exposure and a chance to reach a broader audience.
- **Innovating Cultural Consumption:** The Venue's integration of digital broadcasting adds a dynamic layer to cultural consumption, boosting the creative economy.

Fostering Social Connection

- **Facilitating Interpersonal Interaction:** By bringing production of online content into the physical and social space of the Venue, this type of programming counters the isolation often associated with digital engagement.
- **Promoting Civic Engagement:** This area of programming fosters dialogue and collaboration, turning the Venue into a hub for social connection and civic responsibility.
- **Creating Social Spaces Around Digital Media:** The Venue serves as a nexus where online cultural consumption transforms into vibrant social interaction, reinforcing community bonds.
- **Promoting Physical Connection:** By counteracting the trend of isolated online engagement, the Venue encourages visitors to reconnect physically, enhancing social cohesion.

The Venue's strategic approach to online media production and streaming provides an integrated cultural experience to the locality that is not currently available. It not only strengthens local culture and stimulates the creative economy but also champions the social connection by bringing together online and offline worlds into an unmediated social space. Embracing digital media fuels, rather than hampers, social engagement, creating a thriving hub for cultural enrichment and community building.

10.2.5 Supporting Arts in the Locality

Led by Director Ben Taaffe, the Venue's cultural programming focuses on local stories and diversity, aligning with the stated goals of the COV [Arts Plan 2023-2028](#). In the recently release draft edition of the plan, up-to-date community consultation findings were highlighted, describing how:

"(t)he four proposed themes 'Connected Community', 'Vibrant and Thriving', 'Place and Identity' and 'Innovation and Creative Economy' were all of high importance to respondents, with 'very important' being the most used response to all categories"

It was also noted that;

"The provision of suitable venues and spaces, alongside financial resources, were identified as the highest priority service to support the Arts industry."

In the same draft plan, key findings from community consultation undertaken by COV with arts and cultural industry leaders are presented as follows:

- *There is an opportunity for more variety and diversity in arts activities and offerings in Vincent.*
- *There is high demand for a variety of physical spaces for creative production and practice.*
- *There is a need to create better awareness and provide accessible information about arts activities and opportunities through marketing and promotions.*
- *There is a desire for more opportunities for capacity building.*
- *There is a need for partnerships and collaboration within the creative industry to strengthen and widen the arts and cultural offering by pooling resources.*
- *There is a desire for more support for grassroots and industry-led programming and activations across all districts and town centres.*

The Applicant submits that the cultural programming focus, community based engagement strategy and online media facility at the Venue demonstrate a positive contribution to the locality in relation to each of these key findings.

11. SUMMARY

The Public Interest Assessment submission presented by the Applicant in support of the Tavern Restricted Licence application for the premises known as Passenger at 378-380 William St, Perth, WA 6000, demonstrates the Applicant's commitment to being a responsible and professional operator. They have complied with all relevant requirements and are dedicated to adhering to the Director's policies and compliance measures if the licence is approved. The submission thoroughly considers the potential impact of the venue on the local community, including harm minimization, risk mitigation, and public benefits. The Applicant has considered Section 38 (4) (a, b, c and ca) of the Liquor Control Act 1988 in relation to harm or ill-health, impact on amenity, the offence, annoyance, disturbance or inconvenience and the tourism, community and cultural benefits the Tavern Restricted Licence may have on the local community in this Public Interest Assessment submission.

In relation to harm minimization and risk mitigation, the Applicant has outlined a comprehensive strategy that includes having an Approved Manager on-site at all times to ensure compliance with licensing and regulatory requirements. Additionally, the Venue will continually develop its harm minimization strategy, incorporated into its Code of Conduct, House Policy, and Venue Management Plan. These measures reflect the Applicant's commitment to maintaining a safe environment and addressing any concerns promptly.

This PIA submission highlights that the Venue is not just a typical hospitality business. It aspires to be a distinctive and unique destination, combining hospitality and retail offerings with local community and cultural programming and digital media facilities. The Applicant envisions the Venue becoming a hub for social connection, community engagement, and local culture, contributing significantly to the development of the area and boosting the local retail and hospitality industries. By focusing on social experiences and authentic community engagement, the Venue seeks to enhance the livability, vibrancy, and economic activity of the locality.

In conclusion, the Applicant has presented a compelling case for granting the Tavern Restricted Licence for the Venue. Their dedication to being a responsible operator, along with their emphasis on contributing positively to the local community and satisfying consumer needs, indicates that the Venue will be an asset to the area. The Applicant's commitment to harm minimization and risk mitigation strategies further ensures a safe and secure environment for patrons and residents. Overall, the Venue's unique blend of hospitality, cultural programming, and social experiences aligns well with the evolving needs of the community, making it a valuable addition to the fabric of a vibrant, inclusive, and culturally rich locality. Therefore, it is requested that the application for development and approval of the proposed Venue be approved in the public interest.

12. LIST OF ATTACHMENTS

Attachment 1 - Architect Plans

Attachment 2 - Acoustic Report

Attachment 3 - Venue Management Plan

Attachment 4 - Noise Management Plan

Attachment 5 - Waste Management Plan

Attachment 6 - Example Drinks Menu

Attachment 7 - Example Food Menu

Attachment 8 - Example Cultural Programming and Entertainment Schedule.

Attachment 9 - Letter of Support - Sustainability Consultant - Aimee Smith.

Attachment 10 - Traffic Impact Statement

These plans and attachments are available on request or already included in the planning report



**“PASSENGER”
378-380 WILLIAM ST,
NORTHBRIDGE
ENTERTAINMENT
NOISE ASSESSMENT**

Report 10.00482R-02
prepared on 13/07/2023





"PASSENGER" 378-380 WILLIAM ST, NORTHBRIDGE
ENTERTAINMENT NOISE ASSESSMENT

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BASIS OF REPORT

This report has been prepared by **Acoustics Consultants Australia (ACA)** with all reasonable skill, care and diligence, and taking account of the timescale and resources allocated to it by agreement with the Client. Information reported herein is based on the interpretation of data collected, which has been accepted in good faith as being accurate and valid.

This report is for the exclusive use of the Client. No warranties or guarantees are expressed or should be inferred by any third parties. This report may not be relied upon by other parties without written consent from ACA. ACA disclaims any responsibility to the Client and others in respect of any matters outside the agreed scope of the work.

DOCUMENT CONTROL

REFERENCE	DATE	PREPARED	REVIEWED	AUTHORISED
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Report 10.00482R-02

1. INTRODUCTION

This report presents the findings of the noise assessment conducted by Acoustics Consultants Australia (ACA) for the proposed "Passenger" Bar, Café, Radio and Retail venue located at 378-380 William St, Northbridge.

The aims of this assessment are:

- To identify the main sources of noise from the proposal and the nearest noise sensitive receivers;
- to conduct an objective noise assessment based on a 3D noise model calibrated with measurements conducted at similar venues during busy operations; and
- to provide recommendations that will set basis for noise management, where required.

Noise from the proposal has been identified as a potential source of annoyance to surrounding sensitive receivers. The site lays within mix-use land and it is in close proximity to other lots of commercial and residential use. Of particular concern are lots located towards the rear of the site, away from William Street, with current or potential future residential use.

This assessment has been prepared in accordance with the WA Environmental Protection (Noise) Regulations 1997 (EPNR). The methodology and Standards used to conduct the assessment, as well as the numeric assessment results are presented in the following sections of this report.

Acoustic terms used in this report are defined in the Glossary of **Appendix A**.



2. BACKGROUND INFORMATION

The proposal is for the refurbishment of an existing commercial unit to allow for a food and beverage venue with annexed café and music retail hub. The proposal includes new indoor bar and dining lounge, kitchen and food storage, beer garden, staff room, a combined café and music retail shop with alfresco area, an interactive radio and music studio, toilets and service areas. This noise impact assessment includes all the potential worst-case noise generating scenarios from the proposed operations that could generate noise impacts at the nearest sensitive receivers.

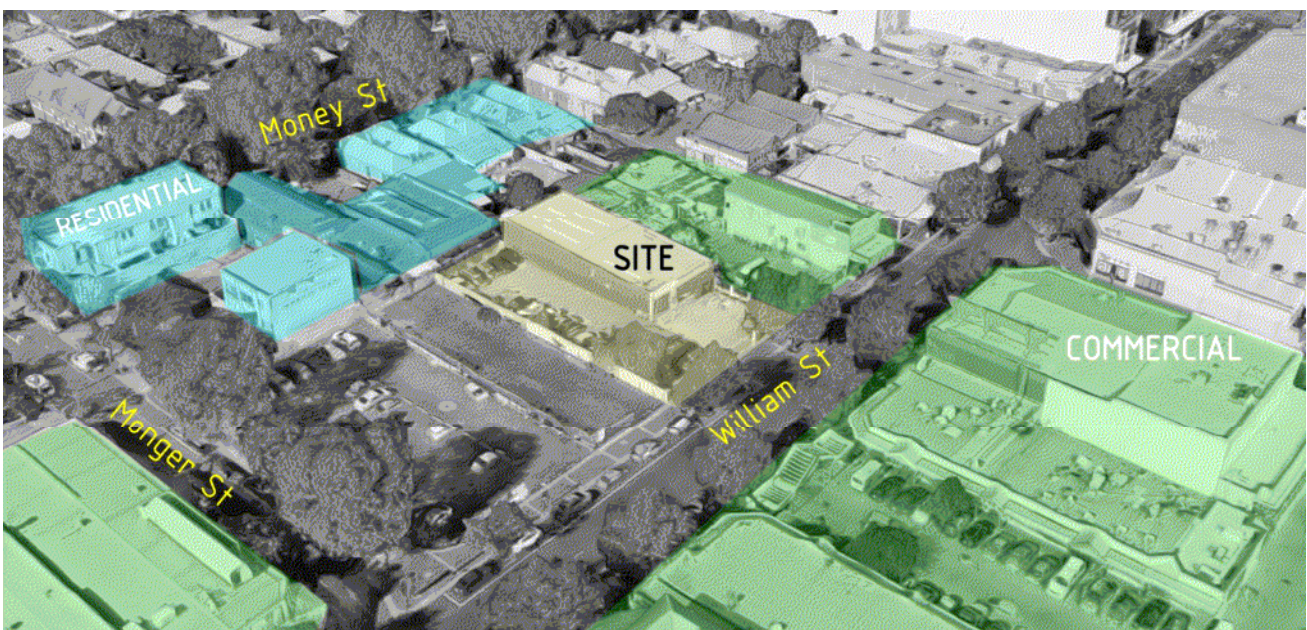
It is understood that the City of Vincent (CoV) would require a review of operations to ensure they are compliant with the Development and Design Policy No. 7.5.21 'Sound Attenuation', which requires compliance with the State Noise Regulations, identification of the potential impacts and mitigation requirements, due to closeness to residential premises.

Noise emitted from the proposed premises and received at the sensitive premises is to be assessed considering highly sensitive receivers, as per the WA Noise Regulations (**Section 3**). Other commercial units are also considered noise sensitive receivers. Further details of the proposed operations are provided in the following sections.

2.1. Location

The site encompasses two lots (67 and 68) on William St (the site), which include a single storey commercial building and carpark. The building is within 'District Centre' area, as per the City of Vincent's Local Planning Scheme No. 2. The rear boundary of the proposed site adjoins commercial zoned land with some residential and highly sensitive use. The identified noise sensitive receivers are a number of lots including residential apartment buildings, single storey dwellings, a hotel and a place of worship, which are all located within either 'Commercial' or 'Mixed Use Area' zones. **Figure 1** depicts an annotated aerial view of the site and its surroundings.

Figure 1 Site location and nearest noise sensitive receivers





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ENTERTAINMENT NOISE ASSESSMENT

The nearest and most exposed highly-sensitive receivers, as defined in Regulations (**Section 3**) have been identified and labelled R1 to R8 in **Figure 2**.

Figure 2 Noise sensitive receivers



The most exposed noise sensitive receivers are:

- R1 – 29 Money St (single storey dwelling)
- R2 – 35 Money St (single storey dwelling)
- R3 – 37 Money St (single storey dwelling)
- R4 – 45 Money St (Place of Worship / Temple)
- R5 – 43 Monger St (three storey mix use building, ground floor is commercial)
- R6 – 39 Monger St (three storey mix use building, ground floor is commercial)
- R7 – 43 Monger St (two storey residential dwelling)
- R8 – Great Southern Hotel 17-39 Robinson Avenue (three storey building)
- R9 – 11 Money St (five storey apartment building)



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Other commercial premises are located adjacent and across to the site on William Street.

It is expected that the noise emissions from the site would be dominated by noise breaking out through the beer garden entrance and from crowd on the beer garden.

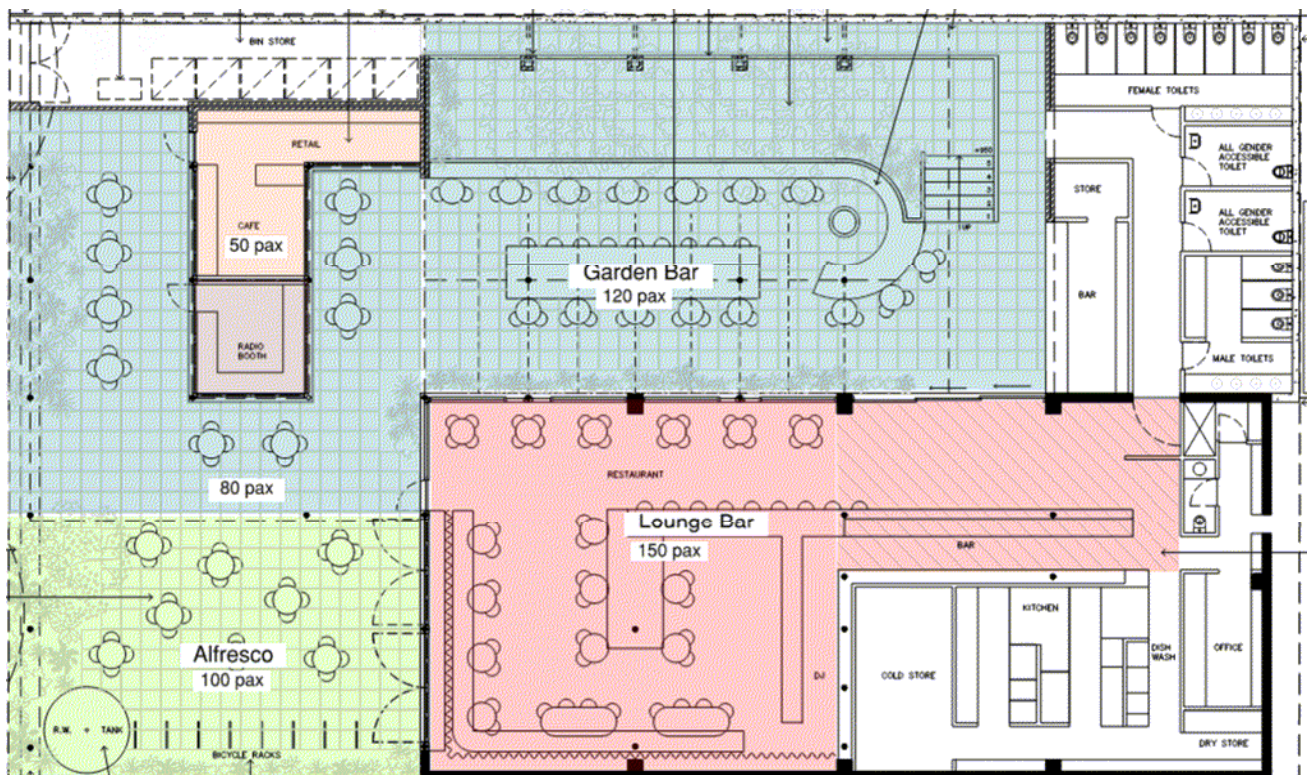
2.2. Operations and Site Description

The proposal is for a hospitality and retail venue, which includes: Indoor bar and dining lounge with dedicated music listening spaces, beer garden, café with annexed music retail facility with alfresco, interactive radio and recording studio. Access to the site is through permeable gates on William Street. The existing commercial/industrial unit will be internally refitted for the proposed bar and dining lounge, the external building envelope is to be essentially preserved and modified to allow for windows, site entry and beer garden access. It is assumed that new air conditioning, kitchen and toilet mechanical plant may be required.

Figure 3 shows the proposed layout of the venue with notes on the crowd capacity and overall distribution of the areas of interest. It is noted that noise barriers have been recommended during conceptual design of the venue and implemented in this proposal. Concrete tilt panels up to 6 metres in height will be built along the North and East boundaries of the site.

Figure 3 Proposed Venue

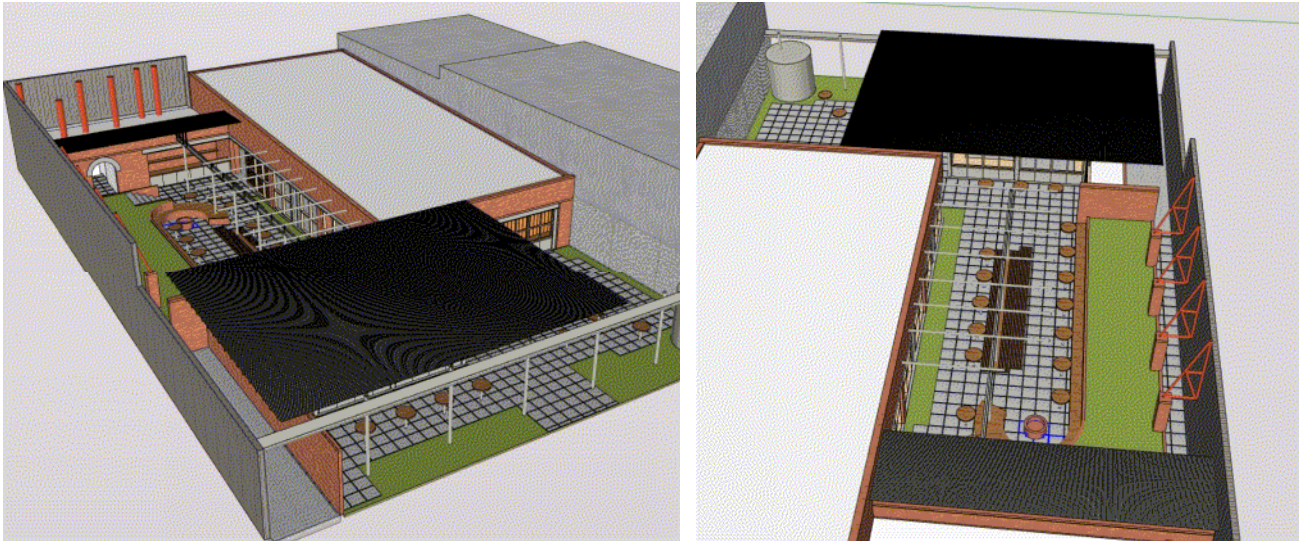
(i) Layout / Crowd Distribution





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(ii) Three dimensional Impressions (Left – View from William St; Right – View from the rear)



Background music (DJ) would be regularly played at the Lounge bar, at a level that would allow for conversational voice. This area will be designed to meet high standards for music listening experience, while permitting for typical casual bar and dining operations. Purpose designed listening areas will be located around the lounge, which will provide quasi-isolated music listening experiences. This suggests that distributed array/network of sound sources (i.e. speakers) will be directionally allocated around the venue with room acoustics treatment, minimising the need for single large amplification systems with elevated sound levels and minimising indirect sound spillage through doors and windows.

It is proposed that music playback, as described above, would take place at the venue on the regular basis. External and internal piped music would be played. It is planned that the radio/studio would be interactive with the surrounding areas and programs may be piped to the Garden Bar and the front Alfresco area. The proposed sound profile, however, is not intended to be dominant of the acoustic environment on site. Thus, it is envisaged that music will be played at the higher range of background music levels, for clarity, yet preserving an overall relaxed but vibrant acoustic amenity.

ACA conducted a site visit on Monday 20th of February 2023 to inspect the current condition of the existing building and surroundings. The existing unit is a brick building sitting on concrete hardstand throughout with steel sheeting roof and fibre insulation beneath. The existing brick façade will be modified to allow for windows and access to the beer garden, while the existing doors of front façade will be used for general entry to the venue. Existing internal partitions may be modified to give way for the proposed layout. The existing boundary is delimited by a 1.8m high colourbond and fibro fence, which would be replaced for noise containment purposes.

There is an existing air conditioning unit on the rooftop; however, it is expected that more mechanical plant will be required to service the various proposed areas. Photos from the site visit are shown in **Figure 4** below.



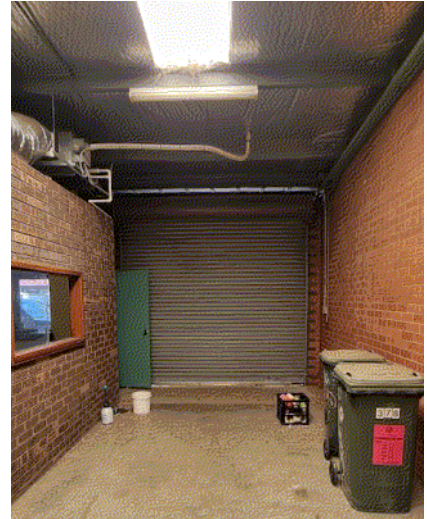
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Figure 4 Site condition during the inspection

i) External condition of the main building



ii) Internal condition of the building





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iii) **Boundary fence**



The proposed trading hours, subject to approvals, are as follows:

For the Lounge (indoor) Bar trading:

- Monday to Thursday, 12pm to midnight.
- Friday and Saturday, 11am to midnight.
- Sunday, 11am to 10pm.

For the Café / Retail space trading:

- Monday to Friday, 7am to 6pm.
- Saturday, 9am to 6pm.
- Sunday, closed.

The identified key noise sources associated with the site are:

- A total of 150 patrons indoors linked to the Lounge Bar and 200 patrons in the Garden Bar.
- A total of 100 patrons in the Alfresco area (even though is not likely all the areas above listed will be operating simultaneously at maximum capacity).
- Music from within the Lounge Bar and the Garden Bar.
- Mechanical plant (assumed):
 - Air conditioning for the internal service areas
 - Air conditioning for the studio and staff room (split units)



- Kitchen extraction fan

Other noise contributors identified from typical operations on site are:

- The café trading; however, this is estimated to have negligible impact due to the estimated noise emissions from the site, its relative location facing William Street (commercial receivers) and the proposed daytime operations (daytime, as defined per the Regulations **Table 1** in **Section 3**).
- Goods' deliveries and waste disposal. Whilst these sources of noise have not been numerically assessed due to the low frequency and duration of these, they are still required to meet the environmental standards. These activities will be restricted to daytime, hence the risk for disturbance at nearby residents will be eliminated.

2.3. Operational Noise Scenarios

The key noise sources identified in the previous section define the following noise generation scenarios:

1. Scenario 1 – Entertainment noise L_{A10} : Lounge Bar doors open.
 - Noise emissions during steady operations at maximum capacity of the site with doors open.
 - All windows to remain closed.
 - DJ music playing at the higher range of 'Background Music'¹.
 - Music piped within the Garden Bar.
 - A noise barrier has been already included for modelling as preliminary assessment determined that this would be required for compliance.
2. Scenario 3 – Mechanical plant noise L_{A10} : Mechanical plant in continuous operation.
 - 3 x air conditioning units.
 - 1 x new kitchen extraction fan.

It is noted that from the noise generating scenarios, a combination of them will provide a resultant noise prediction (**Section 4**), which will be assessed against the applicable noise criteria (**Section 3**).

¹ 'Background Music' as defined in Table 2 of the Association of Australasian Acoustical Consultants *Licensed Premises Noise Assessment Technical Guideline V 2.0*.



3. ACOUSTIC CRITERIA

Acoustic criteria have been determined from a review of the following documents:

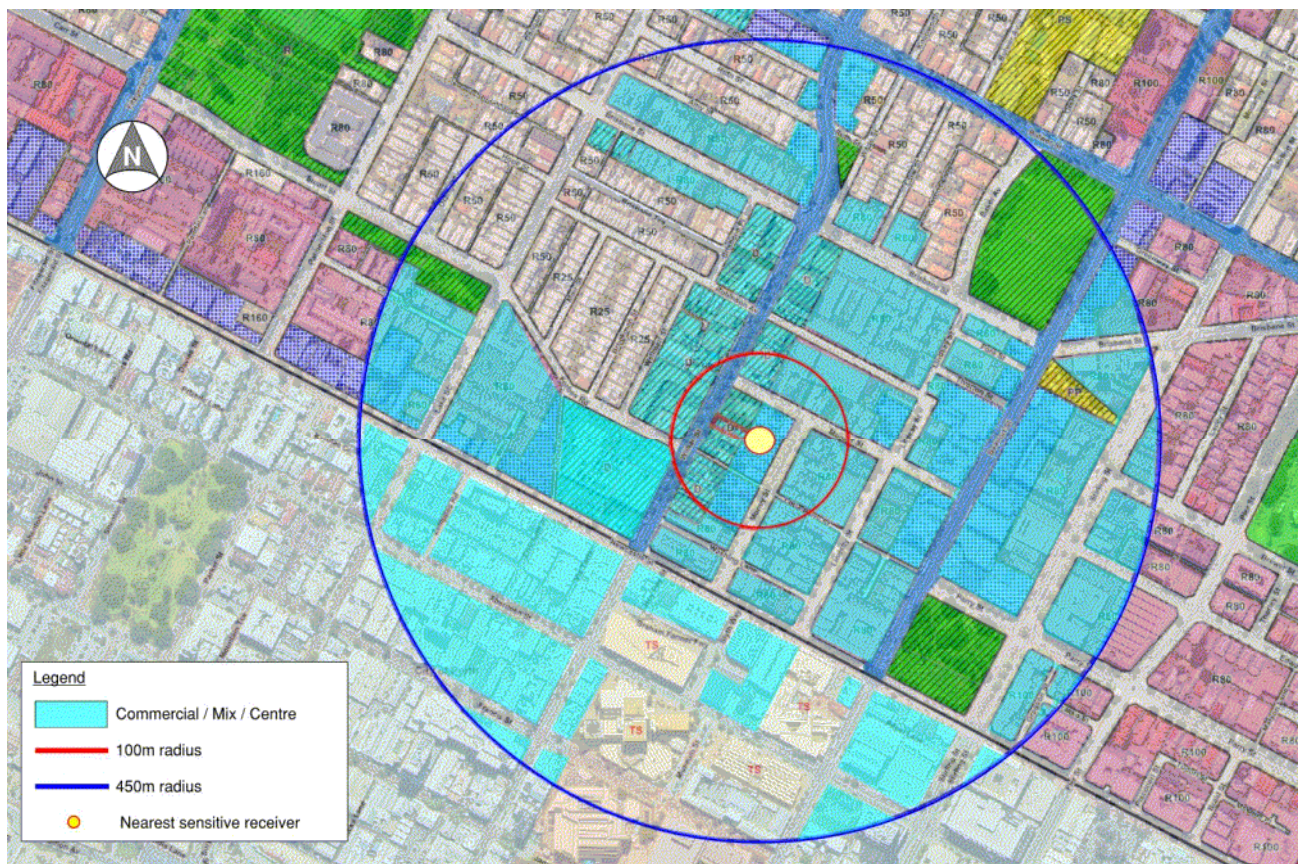
- State requirements: Western Australia *Environmental Protection (Noise) Regulations 1997* (EPNR); and
- The City of Vincent Sound Attenuation Policy No. 7.5.21.

3.1. WA Environmental Protection (Noise) Regulations 1997

Noise emissions from commercial premises are regulated by state noise policy in the form of the Western Australia Environmental Protection (Noise) Regulations of 1997 (EPNR). To achieve compliance with this policy, noise levels at nearby residential areas are not to exceed defined limits. These limits are determined from consideration of prevailing background noise levels and 'influencing factors' that consider the level of commercial and industrial zoning in the locality.

The influencing factor considers zoning and road traffic volumes around the sensitive receiver of interest, within a 100 and 450 m radius (see **Figure 5**).

Figure 5 Influencing factor calculation map





The resulting influencing factor is 10 dB, based on:

- A transport factor of 4 dB due to Beaufort St, a major road (MainRoads site number 1119, 15,222 vehicles/day) within the outer circle, and William St, a secondary road (MainRoads site number 2827, 8,923 vehicles/day) within the inner circle.
- A commercial zoning factor of 6.1 dB due to 73% commercial/mixed use area within the inner circle, and 48% commercial/mixed use area in the outer circle.

A summary of the applicable outdoor noise criteria is provided in the following table.

Table 1 WA EPNR Assigned Noise Levels

Type of premises receiving noise	Time of day	Assigned Level (dB)		
		L _{A10}	L _{A1}	L _{Amax}
Noise sensitive premises: highly sensitive area	0700 to 1900 hours Monday to Saturday	55	65	75
	0900 to 1900 hours Sunday and public holidays	50	60	75
	1900 to 2200 hours All days	50	60	65
	2200 hours on any day to 0700 hours Monday to Saturday and 0900 hours Sunday and public holidays	45	55	65
Noise sensitive premises: any area other than highly sensitive area	All hours	60	75	80
Commercial premises	All hours	60	75	80

For the purposes of this assessment, all periods of the day will be considered as the proposed hours of operation of the bar and lounge include all these periods.

A series of adjustments must be added to the noise source levels if noise received at the sensitive premises cannot reasonably be free of audible characteristics of tonality, modulation and impulsiveness, and the adjusted level must comply with the assigned level. Definition of these terms (tonality, modulation and impulsiveness) are provided by Regulation 9(1) of the EPNR. **Table 2** summarises the adjustments, as defined by the Regulations.

Table 2 Noise character adjustments

Adjustment where noise emission is music		Adjustment where noise emission is not music		
Impulsiveness is not present	Impulsiveness is present	Tonality is present	Modulation is present	Impulsiveness is present
+10 dB	+15 dB	+5 dB	+5 dB	+10 dB



A character noise adjustment of +10 dB would apply to music emissions generated within the bar, but this does not apply to emissions from crowd.

Similarly, a + 5 dB correction would apply to mechanical plant noise emissions, given the nature of rotating equipment giving rise to tonal character.

3.2. Local Government Requirements

The City of Vincent Sound Attenuation Policy No. 7.5.21 requires commercial proposed developments to meet minimum standards for 'Outbound Noise' (i.e. noise expected to be generated within the proposed development). The policy states the following:

4.0 Sound Source Identification

[...]

4.2 Residual Breakout Noise from Lifestyle uses and Entertainment Venues

Developments that accommodate or reside in close proximity to lifestyle uses including cafes, restaurants, shops, hotels and entertainment venues such as nightclubs, are integral to the vitality and enjoyment of mixed-use localities and District Centres. Outdoor Eating Areas associated with hotels, cafés and restaurants can also be a major source of breakout noise.

Outcome 3

The Applicant is to:

a) Identify all breakout noise sources that have a noticeable impact on sound levels received at the subject property, and in particular, those noise sources within a 100m radius;

b) Specify sound levels recorded at the property during time periods that correspond to related breakout noise sources; for example:

- Nightclubs to be monitored during hours of operation after 10.00pm at night on a Friday or Saturday;*
- Cafes and Outdoor Eating Areas to be monitored during peak trade periods; and*
- Locations near sport grounds and stadiums to be monitored during periods of normal use; particularly when used at capacity (where practicable).*

[...]

4.4 Mechanical Plant and Equipment

a) Air-conditioning and related service hardware such as pool pumps, traffic gates and compressors can impact on the amenity of noise sensitive premises, particularly due to the annoying characteristics contained within the noise emitted (modulation and tonality). In a residential setting, the noise emission from a single air conditioner condenser or pool pump can affect a neighbouring property. In mixed use locations, exhaust fans associated with



commercial kitchens, smoke exhaust systems, refrigeration compressors and chillers associated with the air conditioning of larger premises are significant noise sources that impact on the localised environmental noise;

- b) Goods delivery and waste collection vehicles; and*
- c) On-site power generators.*

Outcome 5

The Applicant is to:

- a) provide comment on the likely impact of all existing mechanical plant and equipment noise sources surrounding the proposed development site (air conditioners, traffic gates etc);*
- b) comment on practical building design solutions that could be implemented to minimise the impact of existing mechanical plant and equipment noise sources; and*
- c) comment on the proposed location of all mechanical plant and equipment at the development site; and provide details of how noise emissions from these sources will be contained, so as not to create unwanted sound at nearby properties. The above does not apply to smoke exhaust fans that are only operated in an emergency.*

4.5 Co-existing Land Uses

Whilst it is important to ensure that the proposed development is designed to reasonably negate external/existing noise sources from impacting on noise sensitive areas of the building, it is equally important that the development is designed to ensure that internal noise sources do not impact on existing and planned surrounding land uses also.

Outcome 6

The Applicant is to:

- a) Detail all existing, surrounding noise sensitive land uses within 100m of the applicant's property boundary;*
- b) Plot all surrounding noise sensitive land uses on a site plan detailing the land use type, building construction type of walls and rooves, directional location, height from ground level, and location of windows, ducts or other obvious sound transmission pathways, and distance from the applicant's property boundary;*
- c) Provide comment on how the building should be designed to ensure that sound levels emitted from the building do not exceed the "assigned levels" detailed within the Noise Regulations;*
- d) Identify the most suitable location/s for the placement of noise generating equipment at ground level and above ground level (i.e. air conditioners, traffic gates; sirens; plant rooms; late night/early morning service entry points); and e) Specify associated sound attenuation measures to be implemented to prevent the emission of unreasonable noise.*



e) Specify associated sound attenuation measures to be implemented to prevent the emission of unreasonable noise.'

3.3. Internal Noise Levels

For reference on acoustic amenity at internal spaces, Australian Standard 2107:2016 *Acoustics – Recommended design sound levels and reverberation times for building interiors* (AS/NZS 2107) and the World Health Organisation Guidelines for Community Noise 1999 (The WHO Guidelines) documents provide recommended noise limits for specific room usages.

While AS2107 does not intend to set out environmental impact criteria, in some situations, indoor targets are considered appropriate to noise sensitive activities such as sleep and residential living since they generally occur indoors. Where it can be shown that the *outdoor* Assigned Noise Levels are impracticable to achieve, consideration may be given to appropriate application of guidelines such as Australian Standard 2107:2016.

Further, Regulation 19 of the EPNR provides an alternative to conduct compliance measurements indoors, when measuring outdoors is not a viable option.

The following table presents recommended internal noise levels recommended for residential houses and apartments near major roads in Table 1 of AS/NZS 2107.

Table 3 AS/NZS 2107 Recommended design sound levels

Type of occupancy	Design sound levels ($L_{Aeq,t}$ range) – dB
Houses near major roads	
Living areas	35-45
Sleeping areas (night-time)	35-40
Work areas	35-45

From this table, a referential internal noise target of L_{Aeq} 35 dB is considered reasonable for living areas. The recommended sound levels given are not necessarily appropriate in all circumstances and may not reflect each occupant's expectations of quality; this is particularly the case when noise has considerable low frequency energy or when the levels do not correspond to a quasi-steady noise source (i.e. sound fluctuates by a significant range in a short period of time).

The WHO Guidelines (World Health Organisation) provide internal noise limits recommended to avoid negative health impacts based on sleep disturbance scenarios. The guidelines are not specific to entertainment noise; however, acknowledge that when a significant low frequency component is present, a 10 dB safety factor may be applied.

The recommended limits by the WHO Guidelines are shown in **Table 4**.



Table 4 WHO Guidelines, sleep disturbance recommended noise limits

Noise metric	Recommended indoor levels – dB
Sleep disturbance, inside bedrooms	
L _{Aeq,8hour}	35
L _{Amax}	50

Note: The WHO Guidelines set out outdoor limits based on assumptions of 10dB indoor-outdoor difference. For windows closed, indoor to outdoor level difference may be 5-15 dB higher than with windows open. We summarise the indoor goals, as the façade transmission would vary from resident to resident.

Recommended external noise limits may vary between 10 and 25 dB higher than the figures of **Table 4**, depending on the façade transmission loss specific to each case (i.e. some houses/apartments attenuate sound better than others).



4. ASSESSMENT

4.1. Approach

The assessment has been conducted based on the following steps:

- A site visit to take note of the condition of the existing building;
- A review of the proposed activities and functional spaces to identify the key noise emissions;
- Field measurements conducted by ACA at similar venues to quantify potential noise impacts;
- Noise modelling built and calibrated to the measurements conducted in order to predict noise levels at surrounding noise sensitive receivers; and
- Assessment of predictions against the applicable noise criteria.

Two variables have been assumed under this assessment to design the internal acoustics of the Lounge Bar:

- '*Acoustical Capacity*' of the proposed venue; defined initially for use of restaurants by Rindel²; however, used as a reference to estimate reasonable level of absorption required for this project. The recommended maximum number of patrons for 'sufficient verbal communication' in the internal areas of the venue will indicatively be defined by:

$$N_{max} \approx \frac{\text{Volume of the venue in m}^3}{20 \times (\text{Reverberation time in seconds})}$$

Based on preliminary brief i.e. internal volume of the venue of approximately 850 m³ and reverberation time of approximately 0.6 seconds for mid frequencies assumed, an indicative 'acoustic capacity' (N_{max}) of 75 patrons is calculated. This number is, however, expected to be higher on a ratio of 3-4 for bars. This suggests that internal finishes will require to be highly absorptive for patrons' amenity, to improve comfort and intelligibility within the venue.

- The above assumptions lead to a maximum internal reverberation time of 0.6 seconds required for the bar (when unoccupied) to minimise noise build-up and breakout noise through facades, doors and windows.

Internal crowd noise has been estimated using the Rindel method under the following assumptions:

- One third of the patrons may be talking at any given time with raised vocal effort ($G = 3$).
- Absorption of the room to match 0.6 s reverberation time.

External crowd noise has been estimated using the Haynes formulae.

² Acoustical Capacity as a means of noise control in eating establishments. Jens H. Rindel. Baltic-Nordic Acoustics Meeting, Denmark, 2012.



Mechanical plant noise data have been extracted from standard libraries provided by manufacturers.

4.2. Noise Levels

The source noise levels used in the assessment are summarised in **Table 5**.

Table 5 Noise Source Levels

1/1 Octave Band Sound Level – dB									
	63 Hz	125 Hz	250 Hz	500 Hz	1000 Hz	2000 Hz	4000 Hz	8000 Hz	dBA
Entertainment Noise									
Indoor crowd 150 patrons within the bar – L₁₀ Reverberant Sound Pressure Level									
L ₁₀	n/a	73	79	82	78	72	66	57	82
Internal Music (typical) – L₁₀ Reverberant Sound Pressure Level									
L ₁₀	77	75	74	75	73	65	62	64	77
Outdoor crowd 200 patrons within the Beer Garden area – L₁₀ Sound Power Levels									
L _w	n/a	85	91	94	90	84	78	69	94
External Music (typical) within the Garden Bar area – L₁₀ Sound Pressure Level									
L ₁₀	80	78	78	73	76	71	63	56	77
Mechanical Plant									
Small Air conditioning units (staff room/office and radio/studio) – Sound Pressure Levels @ 1m									
L ₁₀	51	54	54	53	49	45	38	28	54
Large Air conditioning units (Bar / Lounge / Cafe) – Sound Pressure Levels @ 1m									
L _w	73	70	73	73	70	68	57	53	75
Kitchen extraction fan (Typical) – L₁₀ Sound Power Levels									
L _w	76	73	76	76	73	71	60	56	78

These noise source levels in combination with sound transmission data for each of the existing and proposed building materials have been used to estimate the noise levels breaking out of the building.

4.3. Noise Modelling

4.3.1. 3D Model

Geometry from the site and surroundings, surfaces, existing buildings, barriers and sound sources from the site were modelled using internationally recognised noise prediction algorithms. A three-dimensional noise model was developed using a software called SoundPLAN Essential V5.1. An adaptation of the algorithm contained within ISO 9613:1996 *Acoustics – Attenuation of sound during propagation outdoors* was used in this instance.

The following items are considered:

- Three-dimensional location, height and orientation;



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- shielding/reflection effects due to surrounding structures (such as awnings, parapets and roofs); and
- meteorological/thermal effects. However, since propagation distances in this case are smaller than 50 m, such influences are considered insignificant with use of ISO 9613 methodologies.

It is noted that these noise predictions are considered reasonably representative of 'worst case' scenarios and it is expected that actual noise levels would typically be less than that predicted for the majority of adjacent receivers.

Figures 5 a/b show depicted figures with details of the noise model, including the point of assessment (receivers) and the key noise generating sources (i.e. doors, windows, roof, beer garden area) and all features of the site.

Figure 5a Noise model: Entertainment noise setup

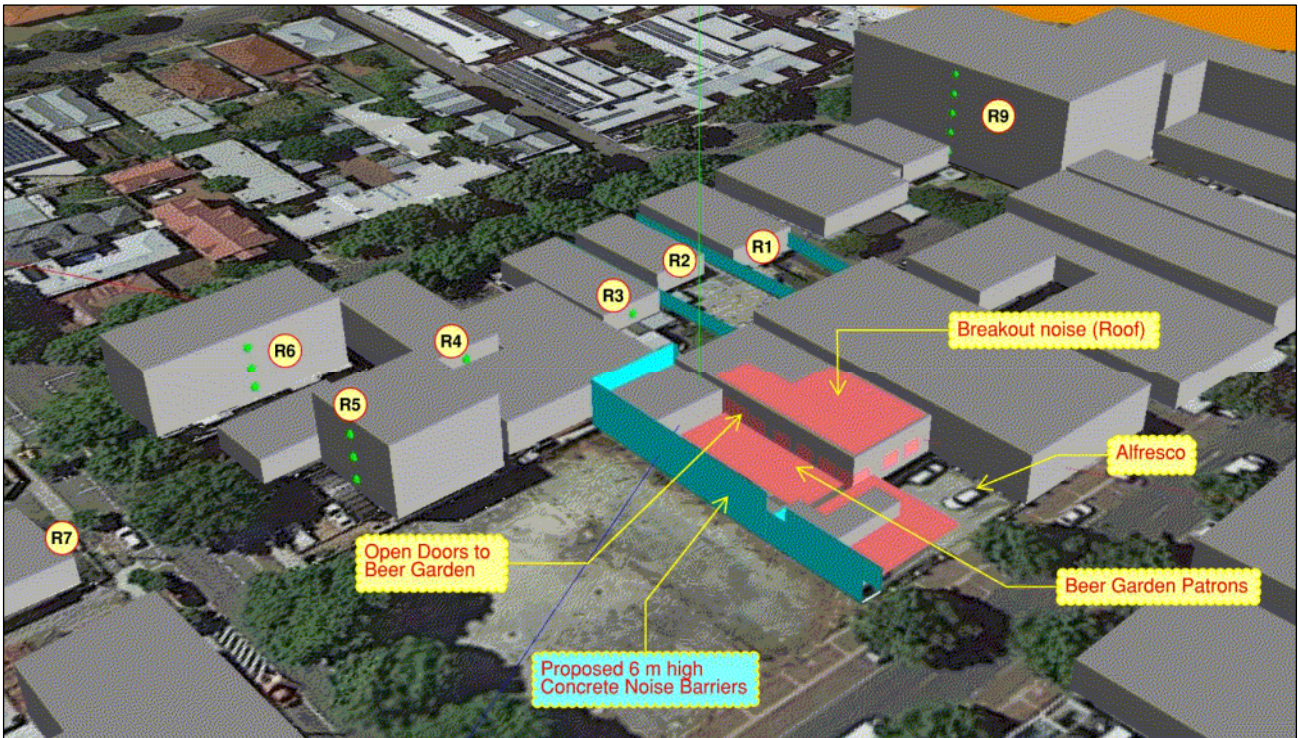
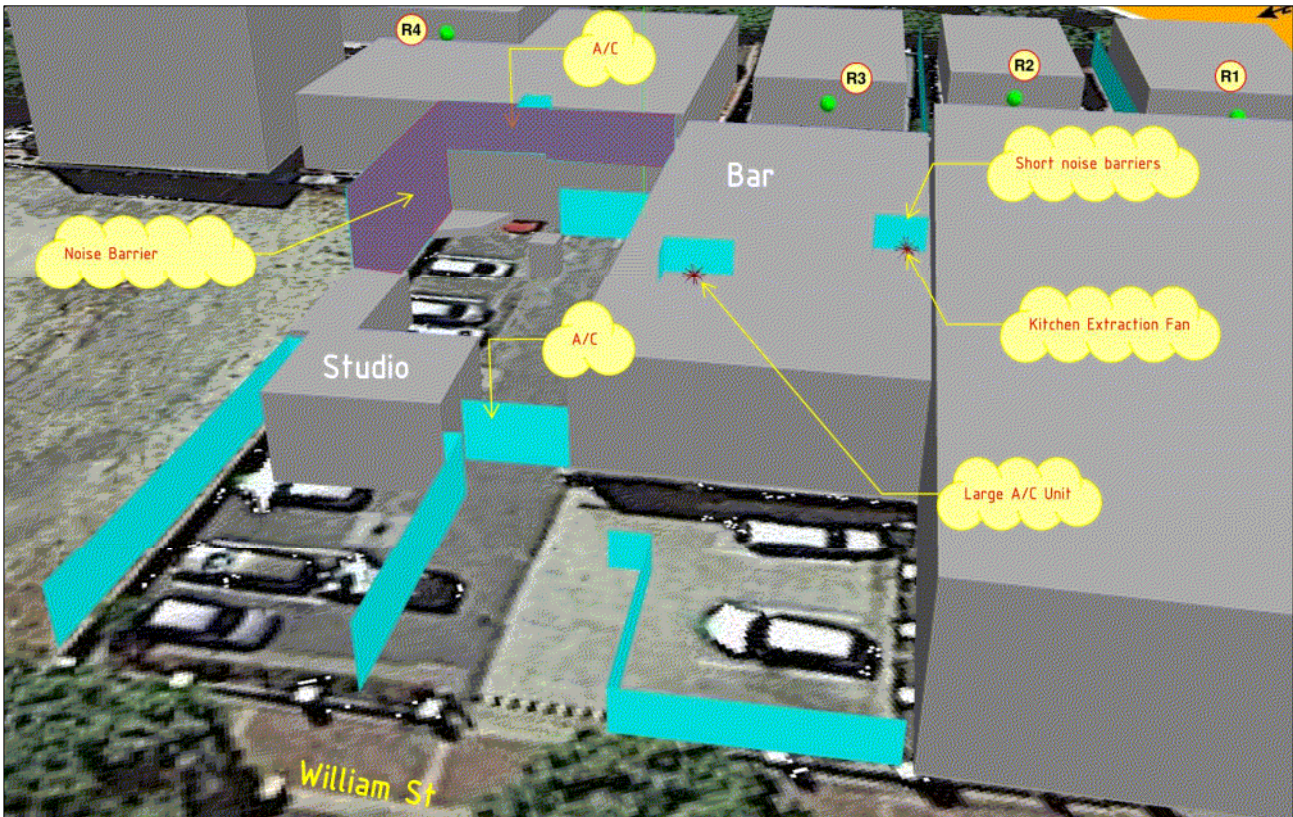




Figure 5b Noise model: Mechanical plant setup



4.3.2. Modelling Scenarios

The noise modelling scenarios described in **Section 2.3** are:

1. Scenario 1 – Entertainment (music and crowd) noise, doors open - L_{A10} noise levels;
2. Scenario 2 – Mechanical plant in continuous operations – L_{A10} noise levels.

4.3.3. Results

Noise contour maps have been generated in SoundPLAN V5.1 (See **Appendix B**). From the results presented obtained in SoundPLAN, a detailed analysis was undertaken to determine individual contributions per noise source and the applicable adjustments applied (See **Table 2** in **Section 2**). That is, noise predictions from music played at the bar/lounge have been penalised with 10 dB and noise predictions from all mechanical plant have been penalised with 5 dB.

These resulting noise character-adjusted predictions are presented in **Table 6**.



Table 6 Outdoor adjusted noise level predictions

Receiver / Location	Predicted Entertainment Noise Levels: Scenario 1 – L _{A10}	Predicted Mechanical Noise Levels: Scenario 2 – L _{A10}
R1 – 29 Money St, ground level	38 dBA	28 dBA
R2 – 35 Money St, ground level	40 dBA	30 dBA
R3 – 37 Money St, ground level	43 dBA	30 dBA
R4 – 45 Money St (Temple)	47 dBA	37 dBA
R5 – 43 Monger St, 2 nd floor Apt.	37 dBA	28 dBA
R6 – 39 Monger St, 2 nd floor Apt.	42 dBA	32 dBA
R7 – 43 Monger St, 1 st floor façade	35 dBA	21 dBA
R8 – Hotel, 2 nd floor room	39 dBA	31 dBA
R9 – 11 Money St, top floor Apt.	39 dBA	31 dBA

The intention is that mechanical noise generated from the site is designed to be kept within 10 dB below the entertainment noise predictions so as the cumulative impact is driven by entertainment noise. This is due to the relatively simple and focused options available to treat mechanical noise. Thus, When the project develops further building services details, these shall be reviewed to ensure noise emissions do not exceed the above baseline.

4.4. Assessment and Discussion

The results presented in the previous section have been combined for potential combined daytime, evening/Sunday and night-time operations and assessed to the noise criteria (per **Section 3**) and the results are presented in **Table 7**.

Table 7 Assessment of results

Receiver	Time Period	Noise Criteria	Noise Prediction	Difference (dB)	Comments
R1	Daytime	L _{A10} 55 dB	L _{A10} 38 dB	-17	Compliant
	Evening, Sunday or public holidays	L _{A10} 50 dB		-12	Compliant
	Night-time	L _{A10} 45 dB		-7	Compliant
R2	Daytime	L _{A10} 55 dB	L _{A10} 40 dB	-15	Compliant
	Evening, Sunday or public holidays	L _{A10} 50 dB		-10	Compliant
	Night-time	L _{A10} 45 dB		-5	Compliant
R3	Daytime	L _{A10} 55 dB	L _{A10} 43 dB	-12	Compliant
	Evening, Sunday or public holidays	L _{A10} 50 dB		-7	Compliant
	Night-time	L _{A10} 45 dB		-2	Compliant



Receiver	Time Period	Noise Criteria	Noise Prediction	Difference (dB)	Comments
R4	Daytime	LA ₁₀ 55 dB	LA ₁₀ 47 dB	-8	Compliant
	Evening, Sunday or public holidays	LA ₁₀ 50 dB		-3	Compliant
	Night-time	LA ₁₀ 45 dB		+2	Exceedance
R5	Daytime	LA ₁₀ 55 dB	LA ₁₀ 37 dB	-18	Compliant
	Evening, Sunday or public holidays	LA ₁₀ 50 dB		-13	Compliant
	Night-time	LA ₁₀ 45 dB		-8	Compliant
R6	Daytime	LA ₁₀ 55 dB	LA ₁₀ 42 dB	-13	Compliant
	Evening, Sunday or public holidays	LA ₁₀ 50 dB		-8	Compliant
	Night-time	LA ₁₀ 45 dB		-3	Compliant
R7	Daytime	LA ₁₀ 55 dB	LA ₁₀ 35 dB	-20	Compliant
	Evening, Sunday or public holidays	LA ₁₀ 50 dB		-15	Compliant
	Night-time	LA ₁₀ 45 dB		-10	Compliant
R8	Daytime	LA ₁₀ 55 dB	LA ₁₀ 40 dB	-15	Compliant
	Evening, Sunday or public holidays	LA ₁₀ 50 dB		-10	Compliant
	Night-time	LA ₁₀ 45 dB		-5	Compliant
R9	Daytime	LA ₁₀ 55 dB	LA ₁₀ 40 dB	-15	Compliant
	Evening, Sunday or public holidays	LA ₁₀ 50 dB		-10	Compliant
	Night-time	LA ₁₀ 45 dB		5	Compliant

The assessment suggests that the proposed venue will be compliant with the EPNR assigned noise levels subject to noise management measures imposed to the venue.

While every effort shall be undertaken to meet the EPNR assigned noise levels, the predicted marginal 2 dB exceedance at receiver R4 (Place of Worship) for night-time is expected to be neither intrusive, nor relevant, considering that typical hours of use of such receiver would not extend past 10pm at any time.

The following compliance requirements must be noted:

- Compliance is predicted for all combined operations during daytime, evening or Sundays.



"PASSENGER" 378-380 WILLIAM ST, NORTHBRIDGE
ENTERTAINMENT NOISE ASSESSMENT

- All calculations for internal noise emissions have been undertaken on the basis that absorptive panels are fitted to the internal surfaces of the bar and lounge to reduce the reverberation time of the venue to 0.6 seconds when occupied.
- All external noise emissions have been predicted on the basis of a 6.0-metre-high boundary barrier built to contain noise from the Garden Bar and Lounge Bar noise breaking out through the entry doors.
- Mechanical plant treated to ensure that noise emissions do not exceed those predicted in this report.

Other considerations set by the assessment basis:

- The new kitchen extraction fan shall be assessed by the acoustic consultant before installation to ensure that the system meets the basis of the assessment (**Table 5** noise levels).
- Music may be played at the higher range of background listening levels at all times and should not be dominant of noise emissions for extended periods of time (minutes at a time rather than hours). Further, background music levels:
 - Should not contain excessive low frequency component;
 - Be played through distributed speakers to avoid larger speakers pointing away from front windows and doors, mounted on rigid structures with vibration isolation pads.
 - The venue will require a purpose design audio visual design to ensure speakers do not generate directional sound emissions to the environment outside the venue's boundary.

The following section summarises the analysis above into a set of recommendations.



5. RECOMMENDATIONS

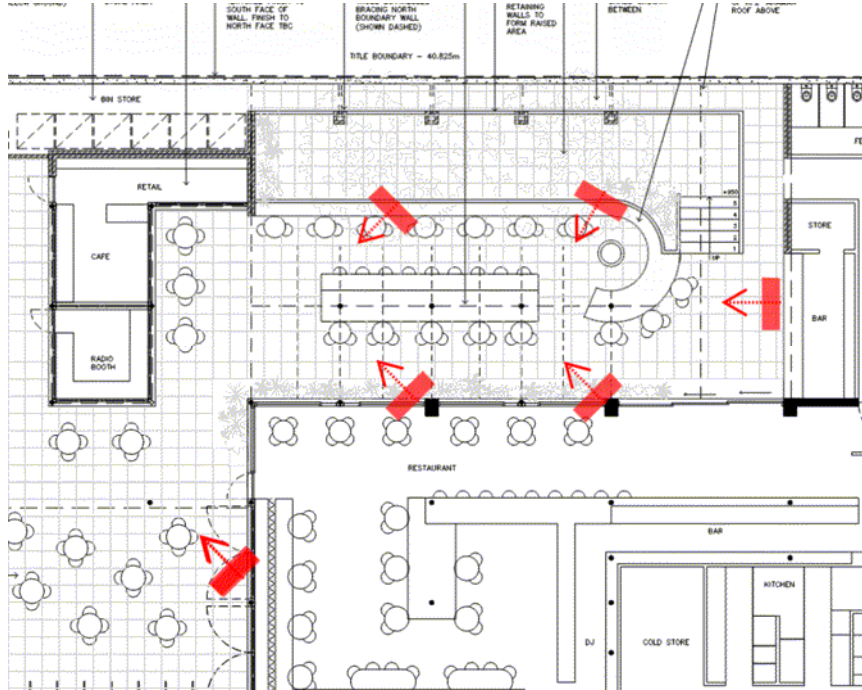
Table 10 outlines the considerations of various noise mitigation options to reduce impact on residents from operations at the proposed venue. The table is divided in 3 sections:

- **Treating the source:** This refers to ways of reducing emissions directly at the source of sound generation (i.e. sound system, speakers, mechanical plant).
- **Treating the path:** This refers to treatment to the medium that is physically in between the source and the receivers (i.e. air paths, buildings, reflective surfaces, supporting structures).
- **Management:** This refers to measures that will be required by the bar management to minimise noise from operations.

Table 8 Noise mitigation options

Item #	Recommendation	Reasoning
Treating the Source		
1	<p>External music may be played through a distributed purpose design audio system with sound levels not to exceed $L_{Aeq,5min}$ 80 dB at listeners' locations.</p> <p>Avoid excessive low frequency emissions. Indicatively, $L_{Ceq} - L_{Aeq}$ should not be more than 15 dB at any time.</p>	To avoid music exceeding the modelled terms.



Item #	Recommendation	Reasoning
2	<p>Loudspeakers shall be installed with appropriate resilient mounts to stop vibration or resonances being transmitted to the building structure and pointing inwards to the centre of the indoor bar.</p> <p>A distributed network of speakers should be sought in place of large stereo system. A custom PA system design shall be discussed with the audio-visual contractor to ensure that both, loudness and directivity of the sound system is design to minimise spillage and projection towards undesirable building façade elements (windows, doors).</p> <p>Speakers shall be located and aimed as close as possible to the patron areas and aimed away to the eastern boundary. Potential locations are shown below:</p> 	
Treating the Path		
3	<p>Bar Fitout</p> <p>Fit sound absorption panelling to the internal bar to achieve reverberation time of 0.6 seconds (occupied).</p> <p>Absorptive panels Class A with minimum NRC 0.95 to the ceiling are recommended. Specific acoustic design is recommended during detailed design of the venue to meet the required reverberation time.</p>	To minimise reverberation and noise build-up indoors.
4	<p>Mechanical Plant</p> <p>Install short barriers on the rooftop next to the mechanical plant.</p> <p><u>Material:</u> The barriers may be built with compressed fibre cement with a minimum surface density of 12 kg/m² (9mm panels) and</p> <p><u>Location and extent:</u> The barriers shall be installed in such a way to break the line of sight between the residential receivers and the mechanical units, as close as possible and at least 0.5 m taller than the top edge of the associated mechanical plant.</p>	To reduce overall noise levels at sensitive receivers.



Item #	Recommendation	Reasoning
5	<p>Noise Barrier</p> <ul style="list-style-type: none"> Noise barriers have already been implemented in the design of the venue. Concrete tilt panels are currently considered. The minimum acoustic performance recommended for barriers is: R_w 30 dB Minimise reverberation within the Garden Bar by lining the barriers internally with absorptive panels (outdoor panels). 	To contain noise from crowd and breakout noise from within the bar / lounge
6	<p>Building and Roof Penetrations: Ensure all penetrations (mechanical, electrical or hydraulic services) are treated to fill any gaps around with fire-rated mastic.</p> <p>Mechanical building services shall be reviewed by the acoustic consultant before commissioning to ensure breakout noise is minimised.</p>	To minimise noise breakout from internal areas of the venue
Management		
7	Set up a noise/measuring management device to provide visual or electronic feedback to management that noise levels recommended in Item 1 are maintained at all times.	To avoid excessive breakout noise
8	Limit waste disposal to daytime hours (i.e. between 9am and 7pm, Monday to Saturday).	To reduce risk of excessive noise at night-time.
9	Limit goods' delivery to daytime hours (i.e. between 9am and 7pm, Monday to Saturday).	



APPENDICES

APPENDIX A: GLOSSARY





"PASSENGER" 378-380 WILLIAM ST, NORTHBRIDGE
ENTERTAINMENT NOISE ASSESSMENT

1 Sound Level or Noise Level

Sound consists of minute fluctuations in atmospheric pressure capable of evoking the sense of hearing. Noise is often used to refer to unwanted sound.

The human ear responds to changes in sound pressure over a very wide range. The loudest sound pressure to which the human ear responds is ten million times greater than the softest. The decibel (abbreviated as dB) scale reduces this ratio to a more manageable range by using logarithms.

The symbols SPL, L or L_p are commonly used to represent Sound Pressure Level.

The symbol L_A represents A-weighted Sound Pressure Level. The standard reference unit for Sound Pressure Levels expressed in decibels is 2×10^{-5} Pa.

2 "A" Weighted Sound Levels

The overall level of a sound is usually expressed in terms of dB(A), which is measured using a sound level meter with an "A-weighting" filter. This is an electronic filter with a frequency response corresponding approximately to that of human hearing.

People's hearing is most sensitive to sounds at mid frequencies (500 Hz to 4000 Hz), and less sensitive at lower and higher frequencies. Thus, the level of a sound in dB(A) is a good measure of the loudness of that sound. Different sources having the same dB(A) level generally sound about equally loud.

A change of 1 dB or 2 dB in the level of a sound is difficult for most people to detect, whilst a 3 dB to 5 dB change corresponds to a small but noticeable change in loudness. A 10 dB(A) change corresponds to an approximate doubling or halving in loudness. The table below lists examples of typical noise levels:

Typical noise levels and subjective scale

Sound Pressure Level dB(A)	Noise Source	Subjective Evaluation
130	Threshold of pain	Intolerable
120	Heavy rock concert	Extremely loud
110	Grinding on steel	
100	Loud car horn at 3 m	Very loud
90	Construction site with pneumatic hammering	Loud
80	Kerbside of busy street	
70	Loud radio or television	
60	Department store	Moderate to quiet
50	General Office	Quiet to very quiet
40	Inside private office	
30	Inside bedroom	
20	Recording studio	Almost silent

Other weightings (e.g. B, C and D) are less commonly used than A-weighting in environmental acoustics. Sound Levels measured without any weighting are referred to as "linear" and the units are expressed as dB(Lin) or dB.



"PASSENGER" 378-380 WILLIAM ST, NORTHBRIDGE
ENTERTAINMENT NOISE ASSESSMENT

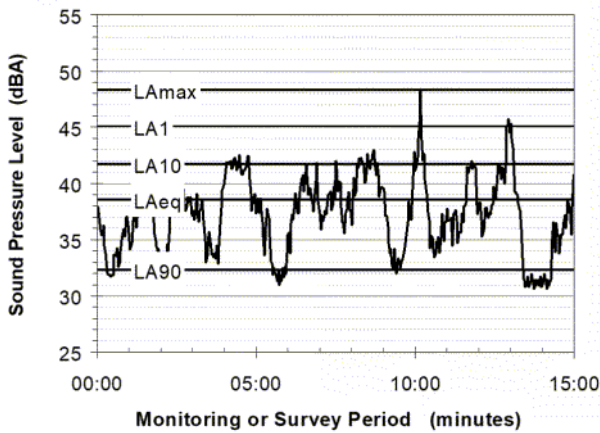
3 Sound Power Level

The Sound Power of a source is the rate at which it emits acoustic energy. As with Sound Pressure Levels, Sound Power Levels are expressed in decibel units, and these may be identified by the symbols SWL or L_w . The Sound Power definitions expressed in dB are typically referenced to the acoustic energy unit 10^{-12} W.

4 Statistical Noise Levels

Sounds that vary in level over time, such as road traffic noise and most community noise, are commonly described in terms of the statistical exceedance levels L_{AN} , where L_{AN} is the A-weighted sound pressure level exceeded for N% of a given measurement period. For example, the L_{A1} is the noise level exceeded for 1% of the time, L_{A10} the noise exceeded for 10% of the time.

The following figure presents a hypothetical 15-minute noise survey, illustrating various common statistical indices of interest.



Of particular relevance, are:

- L_{A1} The noise level exceeded for 1% of the 15 minute interval.
- L_{A10} The noise level exceeded for 10% of the 15 minute interval. This is commonly referred to as the average maximum noise level.
- L_{A90} The noise level exceeded for 90% of the sample period. This noise level is described as the average minimum background sound level (in the absence of the source under consideration), or simply the background level.
- L_{Aeq} The A-weighted equivalent noise level (basically the average noise level). It is defined as the steady sound level that contains the same amount of acoustical energy as the corresponding time-varying sound.

When dealing with numerous days of statistical noise data, it is sometimes necessary to define the typical noise levels at a given monitoring location for a particular time of day. Standardised methods are available for determining these representative levels. Different jurisdictions would choose to define their own preferred Standard.



APPENDIX B: NOISE MODELLING CONTOURS

**Proposed Bar, Cafe, Radio and Retail
378-380 William St, Northbridge
Predicted Environmental
Noise Emissions**

J:\01 PER\02 MODELLING\JOBS SOUNDPLAN\
10.00482 380 William St Northbridge\
10.00482 378-380 William St Northbridge V2S1

Project No: 10.00482
Consultant: MdIM
Date: 13/07/2023




SCENARIO 1: Max Capacity

- Indoor bar/lounge 150 patrons and Music
- Doors Open
- Outdoor Beergarden 200 patrons + piped music
- Alfresco 100 patrons

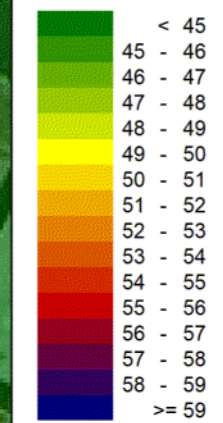
- Reflective ground (Alpha = 0.1)
- Meteorological conditions:
T = 10deg / RH = 50%

- Noise contours @ 1.5m above the ground

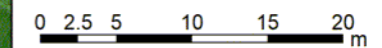
Signs and symbols

-  Line
-  Wall
-  Area source

Levels in dB(A)



1 : 500



**Proposed Bar, Cafe, Radio and Retail
378-380 William St, Northbridge
Predicted Environmental
Noise Emissions**

J:\01 PER\02 MODELLING\JOBS SOUNDPLAN\
10.00482 380 William St Northbridge\
10.00482 378-380 William St Northbridge V1S2

Project No: 10.00482
Consultant: MdIM
Date: 14/04/2023

SCENARIO 2: Mechanical Plant

- 1 x AC Commercial (rooftop)
- 1 x Kitchen Extraction Fan
- 2 x Small AC split units (Studio and Office)

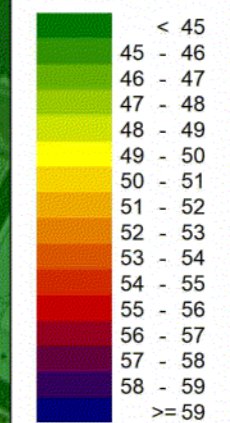
- Reflective ground (Alpha = 0.1)
- Meteorological conditions:
T = 10deg / RH = 50%

- Noise contours @ 1.5m above the ground

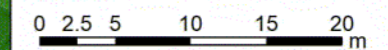
Signs and symbols

- Wall
- * Point source

Levels in dB(A)



1 : 500



PASSENGER



Waste Management Plan

378-380 William street, Northbridge, Boorloo

INTRODUCTION

Sound Hospitality Pty Ltd, trading as Passenger occupies the street-facing, converted warehouse site at 378-380 William Street located at the vibrant heart of the Northbridge retail, dining and hospitality strip. The venue is described as a low-key, high-quality music and community focused hospitality and retail hub with a simple promise at its core:

to provide a local hub and hang-out for music heads and their mates - a safe, homely and hospitable space, where quality, service, music, connection and community are central to everything they do.

The project has been drawn together through the broad experience and vision of two long-standing operators in the Perth hospitality and music scenes, **Iraia Anderson** and **Ben Taaffe**, combining their shared passions for music, hospitality, retail and community radio.

The proposed 350 person (Tavern License) venue will consist of four complementary components, creating a whole that is greater than the sum of its parts, to lead the way in experience-driven, specialist hospitality and retail business in the area:

- (1) An intimate music lounge and bar made for listening; Passenger Bar.
- (2) A spacious, shady and lush courtyard garden bar made for wining, dining: Passenger Backyard.
- (3) An online media streaming facility and internet radio programme - Passenger Radio - broadcasting diverse, progressive and positive signals from Perth to the world while connecting virtual communities with local spaces and real life!
- (4) A world-class music retailer.

LOCATION

378-380 William street, Northbridge (City of Vincent).



COUNCIL OBJECTIVES

- Ensure the long term waste management needs of each development are met in an efficient and sustainable manner and provides for waste minimisation in a manner that protects the environment;
- Minimise the impacts of waste storage and collection facilities on the streetscape, public realm, building entries and the amenity of residents;
- Maximise safety for waste collection staff, residents and the public;
- Minimise traffic and footpath obstruction; and Minimise impacts on the visual or other amenity of the building or neighbourhood.

WASTE MANAGEMENT STRATEGY

Waste management strategies and auditing are a requirement for new developments to provide support for the building design, and promote strong sustainability outcomes for the business. It is Sound Hospitality's belief that a successful waste management strategy contains 3 key objectives:

- Promote responsible source separation to reduce the amount of waste that goes to landfill, by implementing convenient and efficient waste management systems*

ii. Ensure adequate waste provisions and robust procedures that will cater for potential changes during the operational phase of the development

iii. Compliance with all relevant council codes, policies, and guidelines

Some of the key challenges that need to be addressed in waste reduction are: inventory management, recycling, composting and employee culture.

Inventory management

Without regularly scheduled inventory checks to monitor the amount of perishable goods in stock, restaurants will be left with spoiled products. Poor monitoring of buying trends and selling trends also leads to waste.

Employee culture

Creating a green culture involving all members of the staff, from chef to dishwasher, is not always easy. Without the head manager or owner setting goals, leading by example, and enforcing policies, there is no team ambition or incentive for staff members to adopt sustainable practices.

Recycling

Poor or misinformed recycling policies and habits are an issue in many restaurants. Making recycling a priority can be difficult when employees' time is stretched thin and only focused on tasks that directly drive sales and profits.

Composting

Composting also poses a challenge to restaurants. Due to the extra time and effort required to maintain healthy compost, the practice tends to not be a priority for many restaurant owners. However, establishing an efficient and effective composting program is arguably the most important portion of a restaurant's waste management plan.

INVENTORY MANAGEMENT SOLUTIONS TO RESTAURANT WASTE

Sound Hospitality is deeply committed to closely monitoring and routinely recording our inventory. This gives us a detailed look at where food waste comes from and how these inefficiencies can be avoided. Additionally, it should be the responsibility of the staff, both front of house and back of house, to keep running tabs on any food waste that occurs during daily hours of operation. Understanding exactly how and why food waste losses occur will allow us to plan waste reduction initiatives more confidently and lower waste-related costs.

CREATING A SUCCESSFUL GREEN EMPLOYEE CULTURE

Creating an eco-conscious culture within our venue and establishing policies to guide our employees' waste habits will be a key to achieving our green goals. Clearly communicating waste management policies and empowering employees to be good environmental stewards will set the tone of your green culture and drive success.

Further, a successful green employee culture starts at the top and requires engagement from venue managers and owners. This is a critical opportunity for our team leaders to lead the charge and encourage our colleagues and employees to follow.

RECYCLING

Optimizing recycling efforts through proper handling of packaging, single-use containers, and other materials is an important and simple way to reduce our landfill waste and carbon footprint.

The bar and kitchen should have separate bins for items such as cardboard, glass, plastic bottles and organics. Staff will be trained to identify and separate these materials from other waste. The recycling bins should be clearly identified and in a visible location.

Coordinating with our suppliers to have produce delivered in less packaging and switching to eco-friendly packaging alternatives are other ways to reduce our non-recyclable waste.

COMPOSTING SOLUTIONS

The kitchen and bar will generate organic waste that can be composted. Sound Hospitality is committed to starting an in house compost program that will be utilised in its large outdoor space/courtyard gardens or be donated to local community gardens.

PASSENGER WASTE GENERATION

Passenger waste has been calculated using information available on WA Waste Authority's website, the table below is the total waste generated weekly at Passenger from a 3 Bin system:

Table 1.

Venue category	Size (Patron space sqm)	General Waste /L	Recycling /L	FOGO /L
Indoor Bar (Tavern)	150	4200	3150	630
Outdoor Bar (Tavern)	350	9800	7350	1470
Dining Room (Restaurant)	100	3500	2100	840
Retail	80	280	140	

<https://www.wastenet.net.au/waste-calculator/>

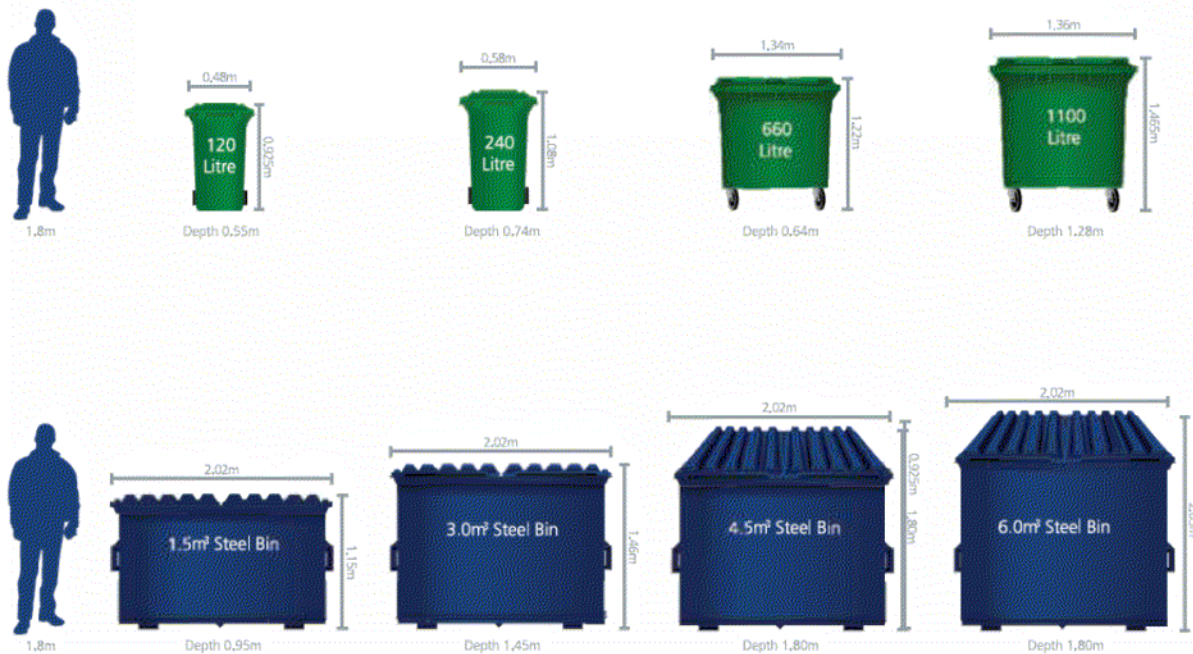
DISPOSAL PROVIDER

Passenger has engaged with Cleanaway for disposal service. The following bins will be provided:

Table 2

Type	Size / L	Quantity	Collection Frequency p/week
General Waste	1100	3	4
Recycling Co mingle	1100	3	4
FOGO	1.5m3	1	2

TYPICAL BIN SPECIFICATIONS



BIN STORE (Architect and directors to complete)

Design details of the bin store included, such as drawings including sizes of bins, aisle and access point widths and wash down facilities (refer 7.2):

- Bin store size
- Bin store layout
- Storage and management of Alternative Wastes (e.g. cooking oil, medical waste, bulky waste and hazardous substances)
- Wash-down area/tap & drain
- Ventilation
- Vermin prevention
- Noise reduction
- Stormwater ingress prevention

Passenger will allow store capacity to have two full weeks' worth of general waste and commingled recycling and one week of FOGO material.

Future proofing our design and allowing for growth and flexibility in the future will ensure our waste management plan will never be compromised by our trading capacity.

DISPOSAL MANAGEMENT (DURING OPERATION)

On completion of each trading day or as required, nominated staff/cleaners will transport their garbage and recycling to the bin store area and place garbage and recycling into the

appropriate collection bins. To ensure the proper management and disposal of waste, tenants must be made aware of the following practices:

- All garbage should be bagged and garbage bins should be plastic lined;
- Bagging of recyclables is not permitted;
- All interim waste storage is located BOH during operations;
- Staff are to adhere to all eco waste strategies as directed by senior management to ensure commingled recycling is correctly separated and that KPI's are being met;
- The operator will organise grease interceptor trap servicing;
- Dry basket arrestors need to be provided to the floor wastes in the food preparation and waste storage areas; and
- All flattened cardboard will be collected and removed to the bin store area recycling.

ELECTRONIC WASTE

Electrical waste (e.g. fluorescent tubing, batteries, laptops etc.) can potentially contaminate soil and surrounding water bodies if not disposed of correctly. These items must not be placed in standard garbage and recycling bins. Disposal or recycling of electronic waste will be organised with the assistance of venue management. These items must not be placed in garbage or recycling bins due to safety and environmental factors.

WASTE OILS

Consideration should be given to the use of cooking oil collection systems. A single service provider may be used to reduce the amount of commercial traffic into the loading bay or around the precinct area. This should be measured against bulk delivery of oils where the same vehicle is used to remove containers of waste cooking oils.

COLLECTION METHOD FREQUENCY

Below are the details of our waste collection method

- Cleanaway will be contracted to remove all waste
- Waste will be collected by Cleanaway waste disposal, rear lift truck fleet
- Cleanaway will be given key access to Passenger bin store area to remove waste
- Passenger will book the disposal service of general waste and comingle recycle after hours to minimise road/traffic disturbance.
- Frequency will vary depending on trade. See table 2 above.

1st August, 2023

To whom it may concern,

I am a sustainability and climate change consultant, experienced in advising West Australian businesses on sustainable business, event and venue practices. I have been engaged by Sound Hospitality to consult on the sustainability of their business venture Passenger, helping to ensure it delivers on its sustainability and low carbon ambitions as a venue. I submit this letter to The City as part of the Applicant's DA Application with the intention to demonstrate the significant efforts the Applicant is applying to ensure best-practice sustainability design principles have been incorporated into the refurbishment plans of the commercial property on 378-380 William St, due to become the Passenger venue, by Sound Hospitality and architect Peter Fredrick Cole.

The design and build of the Passenger venue is a refurbishment consisting of alterations and additions to an existing mixed use and commercial building and, therefore, isn't required to meet the City of Vincent's Environmentally Sustainable Design (ESD) standards as set out in the City's Policy No. 7.1.1 – Built Form (Built Form Policy). However, the Applicant is independently pursuing an ESD approach to the refurbishment, believing this is an important responsibility for any new business or venue, and note and appreciate the City's commitment to supporting and encouraging ESD throughout the City. Working with Sound Hospitality and architect Peter Fredrick Cole, we have used the City's ESD Guidelines to strengthen the sustainability of the construction and operations of the Venue with the intention of pursuing a Life Cycle Assessment and/or Green Building Council of Australia (GBCA) minimum 5 star *Current Design and AsBuilt* rating for the project. As a GBCA Accredited Professional I would be engaged to conduct the accreditation process on behalf of Sound Hospitality.

To date, the following ESD elements have been incorporated into the design of the Venue to produce a higher level of environmental sustainability:

- Over 85% of the existing building and materials are being preserved, with additional construction materials being primarily sourced from recycled materials, recyclable materials, materials that have long life-spans, materials that require minimal maintenance or materials of low embodied carbon. It is estimated that over 70% of the construction waste will be reused and therefore diverted from landfill (e.g.bricks demolished on-site are being used to build a new external wall)

[Carbon emissions reduction-building materials, building orientation and layout; Resource conservation - construction and demolition waste]

- A new external north-facing wall is being constructed to provide external shading and limit heat absorption as well as operating as a sound barrier. This wall will be painted a light colour/white to reduce heat island effect.

[Carbon emissions reduction-building materials, building orientation and layout; Resource conservation - construction and demolition waste]

- The majority of additional windows/glazing are designed into the north side of the building (with a minor amount in the west, none in the east or south), to maximise use of natural light and winter sun for passive heating. Adequate number of windows are openable to allow natural cross-ventilation.

[Carbon emissions reduction - building orientation and layout, windows]

- The building has been designed with accessibility principles and standards, including accessible toilets and a compliant ramp to the raised exterior area). This will ensure equal access to the venue for customers as well as minimise the need for remodelling and retrofitting to accommodate the diverse needs of occupants over time.

[Carbon emissions reduction - building orientation and layout]

- The installation of a significant outdoor green space including two large mature shady trees, a selection of native water-wise shrubs, and a pergola with deciduous vine in the north end of the property is a critical design feature of the Passenger venue. This landscaping feature integrates with the building design to maximise passive heating, cooling, natural light and cross ventilation; provide shade; and minimise heat island effect. Additionally this green infrastructure is watered through a drip irrigation system with rain water captured, stored, and reused from a water tank installed on the premises.

[Carbon emissions reduction - building orientation and layout; Water savings - landscaping, fit-for-purpose water use]

- The use of low emission vehicles and public and active transport by customers and staff is being encouraged at the Venue by providing 32 secure bike, e-bike and e-scooter parking bays; installing an e-bike and e-scooter charging point; providing staff end-of-trip facilities; and the minimum provision of car parking spaces. The venue is also conveniently located near a bus stop and train station.

[Carbon emissions reduction - energy (efficiency and generation)]

In addition to these ESD features that are already central elements of the refurbishment design, Sound Hospitality and architect Peter Fredrick Cole have, upon my advice, committed to ensuring the following ESD elements be incorporated into the next stages of the design and build of the Venue, pending DA Approval:

- Energy efficient lighting (LED) will be used throughout the venue and, where possible, these will be controlled by timers or motion sensors to reduce energy use.
- The roof space is being kept clear of plant and equipment to maximise space for solar PV, which is intended to be installed in the venue to power as much of the energy needs of the multiple spaces as possible (lighting, sound system, kitchen).
- The design of the commercial kitchen will include dedicated kitchen exhaust hoods ducted to outside, new ovens, cooktops and dishwashers with high energy ratings, and specialised cabinetry that provides adequate ventilation for refrigerators to run at maximum efficiency.

- All timber materials being used will be certified as plantation grown and sustainable.
- Low emissivity glass/double glazing will be used in windows to minimise unwanted heat loss/gain, which has the added benefit of insulating sound emissions from the venue.
- A Solar thermal or heat pump system for water heating or pre-heating is being considered.
- An energy monitoring system with real-time data is being considered to easily monitor occupant energy use across the multiple spaces.
- For the Venue's plumbing, it is intended that hot water supply pipes are insulated, the bathrooms and kitchen are located close to the hot water system to minimise wastage of cold water from pipes, and that high WELS rating water-efficient fixtures are used throughout the premises.

As you can see, Sound Hospitality, working alongside architect Peter Fredrick Cole, have taken significant steps to incorporate up-to-date sustainability principles into the design process for this project, and intend to deliver a genuinely environmentally sustainable venue. I look forward to continuing to engage with them on this journey and I believe this business contributes significantly to the environmental, social and economic sustainability of the City of Vincent and the broader West Australian community, not only through its carefully considered built form and green space design, but also through its commitment to locally and sustainably sourced produce, and its contribution to the liveability of the neighbourhood and community through a focus on creative cultural activities that aim to foster social and community connection.

I fully support this venture and hope the City sees it as a valuable and important contribution to the community.

Yours sincerely,



Aimee Smith

Sustainability and Climate Change Consultant

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[Linked-in](#)

Summary of Submissions:

The tables below summarise the comments received during the advertising period of the proposal, together with the Applicant's response to each comment.

Comments Received in Support:	Applicant Comment:
<p><u>Zone Objectives</u></p> <p>The proposal is consistent with the objectives of the zone. The proposed Tavern will improve the vibrancy in the area, and offer additional surveillance to the street in non-peak times.</p>	<p>Noted and agreed</p>
<p><u>Location</u></p> <ul style="list-style-type: none"> • The proposed development is suitably located, is ideal for foot traffic, and is well-serviced by public transport options. • The lack of car parking provided is suitably addressed by the availability of alternative transport options. The development will not rely on on-street car parking within residential streets to satisfy the car parking demand. • The location of the development is suitably separated from residential zones and housing to mitigate noise and crowd impacts. • The walkability of the area is improved by proposed removal of the existing crossovers and the provision of more footpath area. • The venue would be consistent with other venues in the area which do not provide on-site car parking. • The location of the development in an area that contains existing hospitality venues would attract additional patronage to the surrounding businesses. 	<p>Noted and agreed</p>
	<p>Noted and agreed</p>
	<p>Noted and agreed</p>
	<p>Noted and agreed</p>
	<p>Noted and agreed</p>
	<p>Noted and agreed</p>
<p><u>Streetscape</u></p> <ul style="list-style-type: none"> • The street front activation will enhance passive surveillance of the area and will contribute to the vibrancy and atmosphere along William Street, outside of normal business hours. • The proposed street frontage and landscaping will add the area and will attract customers and visitors to the venue. 	<p>Noted and agreed</p>
	<p>Noted and agreed</p>

Summary of Submissions:

Comments Received in Support:	Applicant Comment:
<ul style="list-style-type: none"> The diverse nature of the proposed uses and operating hours would result in a venue that provides for vibrancy at all times. The development would provide an improved streetscape outcome to other venues within Northbridge which typically provide brick walls or glazing built directly on the footpath. 	<p>Noted and agreed</p> <p>Noted and agreed</p>
<p><u>Noise</u></p> <p>The proposed development has provided adequate consideration to address any noise concerns to surrounding residents.</p>	<p>Noted and agreed</p>
<p><u>Activation and Contribution to the Area</u></p> <ul style="list-style-type: none"> The proposed development will further activate and revitalise the northern end of William Street, and the Northbridge and City of Vincent areas. The development would activate an underutilised site that has been empty for an extended period of time and add to the amenity of the area. The retention of the existing building on the site and proposed improvements will bring a sense of community and aesthetic appeal. The proposed development would increase the walkability of the area. The range of services offered by the venue, including dining, bar, retail space, community engagement, art and culture events will be an asset to Northbridge and develop varied late night activities for this pocket of the inner-city. The range of services offered combined the proposed location would attract interest throughout the day from pedestrians and offer a range of people a positive environment to engage with. The location of the proposed venue is a place can be unwelcoming at night. The project will add to the vibrancy, landscape and overall 	<p>Noted and agreed</p> <p>Noted and agreed</p> <p>Noted and agreed</p> <p>Noted and agreed</p> <p>Noted and agreed</p> <p>Noted and agreed</p> <p>Noted and agreed</p>

Summary of Submissions:

Comments Received in Support:	Applicant Comment:
<p>attractiveness of the street.</p> <ul style="list-style-type: none"> • The proposed forward facing, street level activation and place making will assist the City of Vincent to continue to grow into a destination community. • This would be a great addition to the entertainment/hospitality scene on this end of William Street. • The diversity in the proposed operating hours will engage a diverse range of demographic groups and sectors of the community. • The revitalization and activation of the space by the Perth music and arts community will bring vibrancy, and a feeling of safety and community. • The environmental/sustainable focus of the venue within Northbridge will revitalize the area. • The development will encourage more family-friendly venues in the area. • The venue will improve the retention of young adults and creatives in Perth. 	<p></p> <p>Noted and agreed</p> <p>Noted and agreed</p> <p>Noted and agreed</p> <p>Noted and agreed</p> <p>Noted and agreed</p> <p>Noted and agreed</p> <p>Noted and agreed</p>
<p><u>Landscaping and Sustainable Design</u></p> <ul style="list-style-type: none"> • The proposed development would be an example of sustainability and ecofriendly design that would enhance the streetscape. • The location of the proposed development and the provision of bicycle parking on-site would encourage a shift towards active and sustainable transport. • The environmental focus of the development would encourage a focus away from cars to bikes and other models of sustainable transport. • The environmentally conscious design of the proposed development is well considered and would support the existing greenspace in the area. 	<p>Noted and agreed</p> <p>Noted and agreed</p> <p>Noted and agreed</p> <p>Noted and agreed</p>

Summary of Submissions:

Comments Received in Support:	Applicant Comment:
<ul style="list-style-type: none"> The development will repurpose existing structures on-site and proposed the use of sustainable landscaping. The development would provide an improvement to other venues in the area that do not provide sufficient green space. 	Noted and agreed
	Noted and agreed
<u>Management of Venue</u> <ul style="list-style-type: none"> The Venue Management Plan and the proposed fit-out would avoid excess noise. The Code of Conduct outlined in the Venue Management Plan would provide for responsible venue operation. 	Noted and agreed
	Noted and agreed
<u>Community Benefit</u> <ul style="list-style-type: none"> The proposed development would provide social and artistic benefits to the wider community. The proposal for a diversely functioning venue will bring vibrancy and amenity to the neighbourhood. The venue will reinvigorate, activate, and sustain a vibrant and positive culture in the area. The media and radio facility are a defining feature of the proposal which will differentiate it from existing venues and will sustain and represent the community, while fostering new experiences. The proposed bar and social-based internet radio station will positively impact the Perth's local music and cultural scene. The development would foster a genuine sense of community, and would invite people to learn and share the artist merits of Perth's music scene. The innovative approach to engaging the community, supporting emerging musicians, artists and fellow creative practitioners will create a 	Noted and agreed
	Noted and agreed
	Noted and agreed
	Noted and agreed
	Noted and agreed
	Noted and agreed
	Noted and agreed

Summary of Submissions:

Comments Received in Support:	Applicant Comment:
<p>place that serves as a platform for raising awareness and promoting an understanding of diverse cultures and unique artforms.</p> <ul style="list-style-type: none"> • This venue will act as a place to bring together the community with the variety and quality of events that the proposed venue will host. • The development will bolster connection, enhance creative and cultural activities, and foster a sense of local identity. • The development would help foster connection in the community that is currently not possible. • There is a significant need for community focused music spaces, especially toward the northern end of William Street. • The venue has the potential to inject vibrancy and cultural capital into Northbridge. • The online radio concept provides a fresh platform for elevating local voices and promoting local culture and identity through music and art while raising both national and international awareness of Perth's creative scene. • The venue will provide a space to cultivate the local creative scene. The location, facilities and layout will enable the venue to succeed. 	<p></p> <p>Noted and agreed</p> <p>Noted and agreed</p> <p>Noted and agreed</p> <p>Noted and agreed</p> <p>Noted and agreed</p> <p>Noted and agreed</p> <p>Noted and agreed</p>
<p><u>Support for Operator</u></p> <p>The proposed operator is capable of operating a venue that would contribute to the surrounding community.</p>	<p>Noted and agreed</p>

Comments Received in Objection:	Applicant Comment:
<p><u>Character and Amenity of the Area</u></p> <ul style="list-style-type: none"> • Comments regarding the existence of similar land uses in the surrounding area and the lack of public need for the proposed development. 	<p>The comments above and detail in the report clearly illustrates how this venue is not the same or similar to other venues and will harness a sense of community and meet a community need, especially for local musicians and the creative community. It has a</p>

Summary of Submissions:

Comments Received in Objection:	Applicant Comment:
<ul style="list-style-type: none">Concerns regarding the proposed development not being in keeping with the lower density of the area and the resulting impact on the existing amenity of the area.	<p>demonstrated need by virtue of the number of submissions of support received.</p> <p>The area is not a low density area.</p>
<p><u>Car Parking</u></p> <p>Concerns regarding the impact of the proposed development on the availability of car parking in the surrounding area. Further concerns regarding the impact on the existing surrounding businesses due to the proposed car parking shortfall.</p>	<p>Please see detailed survey of car parking availability undertaken in the area, which demonstrates significant public car bays are available during peak times. Notwithstanding the expectation that most people, will walk, ride, scoot or use public transport to the venue and provide for an increase to economic activity in there for the benefit of all businesses. This object is refuted by the applicant, noting that existing businesses do not the right to 'claim' public car bays.</p>

Note: Submissions are considered and assessed by issue rather than by individual submitter.

Summary of Submissions:

The tables below summarise the comments received during the advertising period of the proposal, together with the Administration's response to each comment.

Comments Received in Support:	Administration Comment:
<p><u>Zone Objectives</u></p> <p>The proposal is consistent with the objectives of the zone. The proposed Tavern will improve the vibrancy in the area, and offer additional surveillance to the street in non-peak times.</p>	<ul style="list-style-type: none">• The submissions received in support are noted.
<p><u>Location</u></p> <ul style="list-style-type: none">• The proposed development is suitably located, is ideal for foot traffic, and is well-serviced by public transport options.• The lack of car parking provided is suitably addressed by the availability of alternative transport options. The development will not rely on on-street car parking within residential streets to satisfy the car parking demand.• The location of the development is suitably separated from residential zones and housing to mitigate noise and crowd impacts.• The walkability of the area is improved by proposed removal of the existing crossovers and the provision of more footpath area.• The venue would be consistent with other venues in the area which do not provide on-site car parking.• The location of the development in an area that contains existing hospitality venues would attract additional patronage to the surrounding businesses.	
<p><u>Streetscape</u></p> <ul style="list-style-type: none">• The street front activation will enhance passive surveillance of the area and will contribute to the vibrancy and atmosphere along William Street, outside of normal business hours.• The proposed street frontage and landscaping will add the area and will attract customers and visitors to the venue.	

Summary of Submissions:

Comments Received in Support:	Administration Comment:
<ul style="list-style-type: none">• The diverse nature of the proposed uses and operating hours would result in a venue that provides for vibrancy at all times.• The development would provide an improved streetscape outcome to other venues within Northbridge which typically provide brick walls or glazing built directly on the footpath.	
<p><u>Noise</u></p> <p>The proposed development has provided adequate consideration to address any noise concerns to surrounding residents.</p>	
<p><u>Activation and Contribution to the Area</u></p> <ul style="list-style-type: none">• The proposed development will further activate and revitalise the northern end of William Street, and the Northbridge and City of Vincent areas.• The development would activate an underutilised site that has been empty for an extended period of time and add to the amenity of the area.• The retention of the existing building on the site and proposed improvements will bring a sense of community and aesthetic appeal.• The proposed development would increase the walkability of the area.• The range of services offered by the venue, including dining, bar, retail space, community engagement, art and culture events will be an asset to Northbridge and develop varied late night activities for this pocket of the inner-city.• The range of services offered combined the proposed location would attract interest throughout the day from pedestrians and offer a range of people a positive environment to engage with.• The location of the proposed venue is a place can be unwelcoming at night. The project will add to the vibrancy, landscape and overall attractiveness of the street.	

Summary of Submissions:

Comments Received in Support:	Administration Comment:
<ul style="list-style-type: none">• The proposed forward facing, street level activation and place making will assist the City of Vincent to continue to grow into a destination community.• This would be a great addition to the entertainment/hospitality scene on this end of William Street.• The diversity in the proposed operating hours will engage a diverse range of demographic groups and sectors of the community.• The revitalization and activation of the space by the Perth music and arts community will bring vibrancy, and a feeling of safety and community.• The environmental/sustainable focus of the venue within Northbridge will revitalize the area.• The development will encourage more family-friendly venues in the area.• The venue will improve the retention of young adults and creatives in Perth.	
<p><u>Landscaping and Sustainable Design</u></p> <ul style="list-style-type: none">• The proposed development would be an example of sustainability and ecofriendly design that would enhance the streetscape.• The location of the proposed development and the provision of bicycle parking on-site would encourage a shift towards active and sustainable transport.• The environmental focus of the development would encourage a focus away from cars to bikes and other models of sustainable transport.• The environmentally conscious design of the proposed development is well considered and would support the existing greenspace in the area.	

Summary of Submissions:

Comments Received in Support:	Administration Comment:
<ul style="list-style-type: none">• The development will repurpose existing structures on-site and proposed the use of sustainable landscaping.• The development would provide an improvement to other venues in the area that do not provide sufficient green space.	
<p><u>Management of Venue</u></p> <ul style="list-style-type: none">• The Venue Management Plan and the proposed fit-out would avoid excess noise.• The Code of Conduct outlined in the Venue Management Plan would provide for responsible venue operation.	
<p><u>Community Benefit</u></p> <ul style="list-style-type: none">• The proposed development would provide social and artistic benefits to the wider community.• The proposal for a diversely functioning venue will bring vibrancy and amenity to the neighbourhood.• The venue will reinvigorate, activate, and sustain a vibrant and positive culture in the area.• The media and radio facility are a defining feature of the proposal which will differentiate it from existing venues and will sustain and represent the community, while fostering new experiences.• The proposed bar and social-based internet radio station will positively impact the Perth's local music and cultural scene.• The development would foster a genuine sense of community, and would invite people to learn and share the artist merits of Perth's music scene.• The innovative approach to engaging the community, supporting emerging musicians, artists and fellow creative practitioners will create a place that serves as a platform for raising awareness and promoting an understanding of diverse cultures and unique artforms.	

Summary of Submissions:

Comments Received in Support:	Administration Comment:
<ul style="list-style-type: none"> • This venue will act as a place to bring together the community with the variety and quality of events that the proposed venue will host. • The development will bolster connection, enhance creative and cultural activities, and foster a sense of local identity. • The development would help foster connection in the community that is currently not possible. • There is a significant need for community focused music spaces, especially toward the northern end of William Street. • The venue has the potential to inject vibrancy and cultural capital into Northbridge. • The online radio concept provides a fresh platform for elevating local voices and promoting local culture and identity through music and art while raising both national and international awareness of Perth's creative scene. • The venue will provide a space to cultivate the local creative scene. The location, facilities and layout will enable the venue to succeed. 	
<p><u>Support for Operator</u></p> <p>The proposed operator is capable of operating a venue that would contribute to the surrounding community.</p>	

Comments Received in Objection:	Applicant Comment:
<p><u>Character and Amenity of the Area</u></p> <ul style="list-style-type: none"> • Comments regarding the existence of similar land uses in the surrounding area and the lack of public need for the proposed development. • Concerns regarding the proposed development not being in keeping with the lower density of the area and the resulting impact on the existing amenity of the area. 	<ul style="list-style-type: none"> • The presence of other similar land uses located in proximity to the subject site is not a consideration under the planning framework. The City's Licensed Premises Policy recommends that licensed premises such as this be concentrated within Town Centre areas. The location of the proposed development is considered acceptable against the relevant planning framework. • The area is characterised by a mixture of shop/retail and hospitality businesses with residential development located along side streets. The development focuses patron activity on William Street, with acoustic noise walls and back of house facilities providing suitable separation to residential properties on these side streets, such as Money Street.

Summary of Submissions:

Comments Received in Objection:	Applicant Comment:
<p><u>Car Parking</u></p> <p>Concerns regarding the impact of the proposed development on the availability of car parking in the surrounding area. Further concerns regarding the impact on the existing surrounding businesses due to the proposed car parking shortfall.</p>	<ul style="list-style-type: none">• The proposed parking shortfall is appropriate because there is sufficient capacity within existing parking facilities and availability of high frequency public transport options that are in close proximity to the site.• The applicant has provided a Traffic Impact Assessment and Parking Management Plan in support of the proposed car parking shortfall. The information provided by the applicant demonstrates that the additional demand for car parking generated by the proposed development would be capable of being accommodated by existing public car parking.

Note: Submissions are considered and assessed by issue rather than by individual submitter.

Determination Advice Notes:

1. This is a development approval issued under the City of Vincent Local Planning Scheme No. 2 and the Metropolitan Region Scheme only. It is not a building permit or an approval to commence or carry out development under any other law. It is the responsibility of the applicant/owner to obtain any other necessary approvals and to commence and carry out development in accordance with all other laws.
2. If the development the subject of this approval is not substantially commenced within a period of 2 years, or another period specified in the approval after the date of determination, the approval will lapse and be of no further effect.
3. Where an approval has so lapsed, no development must be carried out without the further approval of the local government having first been sought and obtained.
4. If the applicant or owner is aggrieved by this determination, there is a right of review by the State Administrative Tribunal in accordance with the *Planning and Development Act 2005* Part 14. An application must be made within 28 days of the determination.
5. This approval is not an authority to ignore any constraint to development on the land, which may exist through statute, regulation, contract or on title, such as an easement or restrictive covenant. It is the responsibility of the applicant and not the City to investigate any such constraints before commencing development. This approval will not necessarily have regard to any such constraint to development, regardless of whether or not it has been drawn to the City's attention.
6. The movement of all path users, with or without disabilities, within the road reserve, shall not be impeded in any way during the course of the building works. This area shall be maintained in a safe and trafficable condition and a continuous path of travel (minimum width 1.5 metres) shall be maintained for all users at all times during construction works. Permits are required for placement of any materials within the road reserve.
7. An Infrastructure Protection Bond together with a non-refundable inspection fee shall be lodged with the City by the applicant, prior to the commencement of works, and will be held until all building/development works have been completed and any disturbance of, or damage to the City's infrastructure, including verge trees, has been repaired/reinstated to the satisfaction of the City. An application for the refund of the bond shall be made in writing. The bond is non-transferable.
8. All storm water produced on the subject land shall be retained on site, by suitable means to the full satisfaction of the City. No further consideration shall be given to the disposal of storm water 'off site' without the submission of a geotechnical report from a qualified consultant. Should approval to dispose of storm water 'off site' be subsequently provided, detailed design drainage plans and associated calculations for the proposed storm water disposal shall be lodged together with the building permit application working drawings.
9. The development will be classified as a 'Public Building' and must comply with the *Health (Public Buildings) Regulations 1992*. An application is to be made to the City's Health Services for the assessment of the public building and maximum accommodation numbers prior to occupation of the premises. Please contact Health Services on (08) 9273 6000 upon receipt of this approval to discuss the requirements further with an Environmental Health Officer.
10. The food business must comply with the *Food Act 2008*, *Food Regulations 2009* and the Australia New Zealand Food Standards Code. The applicant must register with the City's Health Services prior to operation of the food business. Please contact Health Services on 9273 6000 to discuss the requirements further with an Environmental Health Officer.

Determination Advice Notes:

11. Food business/es are proposed, mechanical ventilation plans or type of businesses is not yet known. The City recommends that control of cooking odours/smoke are managed on a worst case scenario, by ensuring that vertically discharging mechanical ventilation systems are built in to the building. Mechanical ventilation systems are required to comply with AS1668.2 - *The use of ventilation and air conditioning in buildings (Part 2: Mechanical ventilation in buildings)* in order to prevent odours/smoke causing a nuisance to the residential apartments above.
12. Any external artificial lighting installations, including in carparks and common areas, is to comply with Australian Standard AS 4282-1997 'Control of the obtrusive effects of outdoor lighting' and must not be permitted to shine or reflect into other properties, creating a nuisance.
13. The hours in which the sale, supply and consumption of alcohol may be undertaken are restricted by the *Liquor Control Act 1988*.
14. Any noise created at the premises must ensure compliance with the provisions within the *Environmental Protection (Noise) Regulations 1997*.